



Our Ref: RL127

24 September 2025

Balonne Shire Council
193-199 Grey Street
ST GEORGE QLD 4487

Attention: Sharyn Arnold

By email: sharyn.arnold@balonne.qld.gov.au

Dear Sharyn,

Decision notice approval

(Given under section 63(2) of the *Planning Act 2016*)

The development application described below was properly made to the Balonne Shire Council on 2 September 2024.

Applicant details

Applicant name:	Balonne Shire Council
Applicant contact details:	193-199 Grey Street, St George QLD 4487 Email: sharyn.arnold@balonne.qld.gov.au

Location details

Street address:	130-152 Grey Street, Carnarvon Highway and 54 Commissioners Point Road, St George
Real property description:	Lot 10 on RP882935, Lot 1 on SP340406, Lot 1 on RP55376 and Lots 1 & 2 on RP55377
Local government area:	Balonne Shire Council

Application details

Application number:	RL127
Approval sought:	Development Permit
Description of the development proposed:	Reconfiguring a Lot – Five (5) lots into thirteen (13) lots and dividing land into parts by agreement
Category of assessment:	Code Assessment
Planning scheme:	<i>Balonne Shire Planning Scheme 2024</i>

Decision

I wish to advise that, on 18 September 2025, the above development application was **approved in full** subject to conditions by Council. (Refer to the conditions contained in **Attachment 1**)

Details of the approval

This application is not taken to have been approved (a deemed approval) under section 64(5) of the *Planning Act 2016*.

The following approval is given:

	Planning Regulation 2017 reference	Development Permit	Preliminary Approval
Development assessable under the planning scheme, superseded planning scheme, a temporary local planning instrument, a master plan or a preliminary approval which includes a variation approval - Reconfiguring a Lot	N/A	<input checked="" type="checkbox"/>	N/A

Further development permits

Please be advised that the following development permits are required to be obtained before the development can be carried out:

- Development Permit – Operational Works
- Survey Plan endorsement

Referral agencies for the application

The relevant referral agencies for this application are:

State Assessment and Referral Agency (SARA)	
Address for hand delivery:	128 Margaret Street, Toowoomba QLD 4350
Address for post:	PO Box 825, TOOWOOMBA QLD 4350
Address for electronic submission:	Applications can be prepared and referred to DILGP online by using MyDAS2. MyDAS2 can be accessed at https://prod2.dev-assess.qld.gov.au/suite/ Email: ToowoombaSARA ToowoombaSARA@dsdmip.qld.gov.au
Reason for Referral:	As a <u>Concurrence Agency</u> for an application involving: <i>Development application for an aspect of development stated in schedule 20 that is assessable development under a local categorising instrument or section 21, if—</i> (a) <i>the development is for a purpose stated in schedule 20, column 1 for the aspect; and</i> (b) <i>the development meets or exceeds the threshold—</i> (i) <i>for development in local government area 1—stated in schedule 20, column 2 for the purpose; or</i> (ii) <i>for development in local government area 2—stated in schedule 20, column 3 for the purpose; and</i>

	<p>(c) <i>for development in local government area 1—the development is not for an accommodation activity or an office at premises wholly or partly in the excluded area</i></p> <p><i>However, if the development is for a combination of purposes stated in the same item of schedule 20, the threshold is for the combination of purposes and not for each individual purpose.</i></p> <p>Schedule 10, Part 9, Division 4, Subdivision 1, Table 1, Item 1 of the <i>Planning Regulation 2017</i>.</p>
	<p>As a <u>Concurrence Agency</u> for an application involving:</p> <p>Development application for reconfiguring a lot that is assessable development under section 21, if—</p> <p>(a) <i>all or part of the premises are within 25m of a State transport corridor; and</i></p> <p>(b) <i>1 or more of the following apply—</i></p> <ul style="list-style-type: none"> (i) <i>the total number of lots is increased;</i> (ii) <i>the total number of lots adjacent to the State transport corridor is increased;</i> (iii) <i>there is a new or changed access between the premises and the State transport corridor;</i> (iv) <i>an easement is created adjacent to a railway as defined under the Transport Infrastructure Act, schedule 6; and</i> <p>(c) <i>the reconfiguration does not relate to government supported transport infrastructure</i></p> <p>Schedule 10, Part 9, Division 4, Subdivision 2, Table 1, Item 1 of the <i>Planning Regulation 2017</i></p>
	<p>As a <u>Concurrence Agency</u> for an application involving:</p> <p>Development application for reconfiguring a lot that is assessable development under section 21, if—</p> <p>(a) <i>all or part of the premises are—</i></p> <ul style="list-style-type: none"> (i) <i>adjacent to a road (the relevant road) that intersects with a State-controlled road; and</i> (ii) <i>within 100m of the intersection; and</i> <p>(b) <i>1 or more of the following apply—</i></p> <ul style="list-style-type: none"> (i) <i>the total number of lots is increased;</i> (ii) <i>the total number of lots adjacent to the relevant road is increased;</i> (iii) <i>there is a new or changed access between the premises and the relevant road; and</i> <p>(c) <i>the reconfiguration does not relate to government supported transport infrastructure</i></p> <p>Schedule 10, Part 9, Division 4, Subdivision 2, Table 3, Item 1 of the <i>Planning Regulation 2017</i></p>

Approved plans, specifications and drawings

Copies of the following approved plans are enclosed.

Plan/Document Number:	Plan/Document Name:	Date:
24193-A (Revision 4)	Plan showing proposed lots & road actions at St George Airport. Lot 1 RP55376, Lots 1 & 2 RP55377, Lot 10 RP882935 & Lot 1 SP340406	20/12/24
24193-B (Revision 3)	Plan showing proposed lots and road actions at St George Airport. Lot 1 RP55376, Lots 1 & 2 RP55377, Lot 10 RP882935 & Lot 1 SP340406	20/12/24
22.1003-002 (Revision 6)	St George Airport Industrial Subdivision - Road Geometry Plan	16/10/23
22.1003-003 (Revision 6)	St George Airport Industrial Subdivision - Typical Road Sections	16/10/23
22.1003-007 (Revision 6)	St George Airport Industrial Subdivision - Earthworks Layout Plan - Sheet 1 of 3	16/10/23

Plan/Document Number:	Plan/Document Name:	Date:
22.1003-008 (Revision 6)	St George Airport Industrial Subdivision - Earthworks Layout Plan - Sheet 2 of 3	16/10/23
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22.1003-015 (Revision 6)	St George Airport Industrial Subdivision – MC01 Longitudinal Section	16/10/23
22.1003-016 (Revision 6)	St George Airport Industrial Subdivision - MC02 Longitudinal Section – Sheet 1 of 2	16/10/23
22.1003-017 (Revision 6)	St George Airport Industrial Subdivision - MC02 Longitudinal Section – Sheet 2 of 2	16/10/23
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22.1003-020 (Revision 6)	St George Airport Industrial Subdivision – MC01 Cross Sections – Sheet 1 of 2	16/10/23
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Currency period for the approval (s.85 of the Planning Act)

This approval lapses if a plan for the reconfiguration that, under the Land Title Act, is required to be given to a local government for approval is not given to the local government within 4 years of the approval taking effect.

Appeal Rights

The rights of an applicant to appeal to a tribunal or the Planning and Environment Court against a decision about a development application are set out in chapter 6, part 1 of the *Planning Act 2016*. For certain applications, there may also be a right to make an application for a declaration by a tribunal (see chapter 6, part 2 of the *Planning Act 2016*).

Appeal by an applicant

An applicant for a development application may appeal to the Planning and Environment Court against the following:

- the refusal of all or part of the development application
- a provision of the development approval
- the decision to give a preliminary approval when a development permit was applied for
- a deemed refusal of the development application.

An applicant may also have a right to appeal to the Development tribunal. For more information, see schedule 1 of the *Planning Act 2016*.

The timeframes for starting an appeal in the Planning and Environment Court are set out in section 229 of the *Planning Act 2016*.

Attachment 2 is an extract from the *Planning Act 2016* detailing appeal rights.

To stay informed about any appeal proceedings which may relate to this decision visit:

<https://planning.dsdmip.qld.gov.au/planning/our-planning-system/dispute-resolution/pe-court-database>.

For further information, please contact the Council office on 07 4620 8888 or via email to council@balonne.qld.gov.au.

Yours sincerely



Kate Swepson

Consulting Town Planner

Cc: ToowoombaSARA@dsdilgp.qld.gov.au

- enc. Attachment 1—Assessment Manager Conditions of Approval (Balonne Shire Council)
Attachment 2 – Concurrence Agency Response
Attachment 3—Appeal Provisions
Attachment 4—Statement of Reasons
Attachment 5—Approved Plans and Specifications

ATTACHMENT 1 – ASSESSMENT MANAGER CONDITIONS OF APPROVAL (BALONNE SHIRE COUNCIL)

GENERAL ADVICE

- i. The relevant planning scheme for this development is *Balonne Shire Planning Scheme 2024*. All references to the 'Planning Scheme' and 'Planning Scheme Schedules' within these conditions refer to the above Planning Scheme.
- ii. A development permit for a Material Change of Use will be required for any activity or development on the approved lot(s) that does not comply with the accepted development criteria in the *Balonne Shire Planning Scheme 2024*.
- iii. All persons involved in the development have an obligation to take all reasonable and practical measures to prevent or minimise any biosecurity risk under the *Biosecurity Act 2014*.
- iv. This approval lapses if a plan for the reconfiguration is not given to the Council within four (4) years of the approval taking effect.
- v. The plan for the reconfiguration must be duly signed by the registered proprietor of the land and the surveyor, and submitted to Council for approval in a form acceptable to Council within the relevant period.

Unless otherwise stated all conditions shall be completed prior to the Council endorsing the relevant plan of survey.

- vi. The *Environmental Protection Act 1994* states that a person must not carry out any activity that causes, or is likely to cause, environmental harm unless the person takes all reasonable and practicable measures to prevent or minimise the harm. Environmental harm includes environmental nuisance. In this regard, persons and entities involved in the establishment of the approved development are to adhere to their 'general environmental duty' to minimise the risk of causing environmental harm to adjoining premises.
- vii. It is the responsibility of the developer to obtain all necessary permits and submit all necessary plans to the relevant authorities that are associated with the approved development, including any permits/approvals required by any State Agencies.
- viii. In completing an assessment of the proposed development, Council has relied on the information submitted in support of the development application as true and correct. Any change to the approved plans and documents may require a new or changed development approval. Council should be contacted for advice in the event of any potential change in circumstances.
- ix. All Aboriginal Cultural Heritage in Queensland is protected under the *Aboriginal Cultural Heritage Act 2003* and penalty provisions apply for any unauthorised harm. Under the legislation a person carrying out an activity must take all reasonable and practicable measures to ensure the activity does not harm Aboriginal Cultural Heritage. This applies whether or not such places are recorded in an official register and whether or not they are located in, on or under private land. The developer is responsible for implementing reasonable and practical measures to ensure Cultural Heritage Duty of Care Guidelines are met and for obtaining any clearances required from the responsible entity.

CONDITIONS

Approved development

1. The approved development is for Reconfiguring a Lot:
 - a. Subdivision – Five (5) lots into thirteen (13) lots
 - b. Dividing land into parts by agreement (Lease A)as defined in the *Planning Act 2016* and as shown on the approved plans.
2. Lease lots may be registered individually upon entering a lease for the relevant lot, subject to all applicable approval conditions being met.
3. The lease of any approved lot must reflect the extent of infrastructure and utility services (i.e. water, sewerage, electricity supply) available to the lot at the time of registering the relevant Survey Plan.
4. Complete and maintain the approved development as follows:
 - a. in accordance with development approval documents; and
 - b. strictly in accordance with those parts of the approved development that have been specified in detail by the Council unless the Council agrees in writing that those parts will be adequately complied with by amended specifications.

Compliance

5. Unless otherwise stated, all conditions must be complied with prior to the Council endorsing the relevant Survey Plan.

Approved documents

6. The approved development is to be carried out in accordance with following approved plans and documents and subject to the approval conditions. Where there is any conflict between the approval conditions and the details shown on the approved plans, the approval conditions prevail.

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7. The approved lot layout is in accordance with approved plans 24193-A (Revision 4) and 24193-B (Revision 3) as listed in the above table. Where there is any conflict between the approved lot layout and approved civil drawings, the approved lot layout prevails.

Existing buildings and structures

8. Existing buildings, structures, infrastructure and services located on the development site are not to encroach on the proposed allotment boundaries.

Development works

9. During construction of the development, the developer shall ensure that all works are carried out by appropriately qualified persons and the developer and the persons carrying out and supervising the work shall be responsible for all aspects of the works, including public and worker safety, and shall ensure adequate barricades, signage and other warning devices are always in place.
10. Any civil engineering and related work shall be designed and supervised by Registered Professional Engineers of Queensland (RPEQ) who are competent in completing the works.
11. All works on or near roadways shall be adequately signed in accordance with the “Manual for Uniform Traffic Control Devices – Part 3, Works on Roads”.
12. Construction activities must be undertaken in consultation with the Airport Manager so as to ensure there is no encroachment of the Obstacle Limitation Surface or impediment to airport operations.

Services provision

13. Proposed Lots 1-11 must be connected to Council’s reticulated water supply system in accordance with the approved Water & Sewerage Plan, applicable Water Services Association of Australia (WSAA) publication and relevant engineering standards, ensuring an adequate supply is available to meet the operational needs of future land uses.

14. Proposed Lots 1-11 must be connected to Council's reticulated sewerage disposal system in accordance with the approved Water & Sewerage Plan, applicable Water Services Association of Australia (WSAA) publication and relevant engineering standards, ensuring network capacity is adequate to meet the operational needs of future land uses.
15. Proposed Lots 1-11 must be connected to an electricity reticulation service in accordance with the relevant service provider's requirements and specifications along with relevant building standards, requirements and specifications (as relevant).
16. If the lots are connected to a telecommunications service, then such works shall be undertaken in accordance with the relevant service provider's requirements and specifications along with relevant building standards, requirements and specifications (as relevant).
17. All works must comply with:
 - a. the development approval conditions;
 - b. any relevant provisions in the Planning Scheme;
 - c. any relevant standards, drawings or specifications by the Institute of Public Works Engineering Australasia Queensland Division;
 - d. any relevant Australian Standard that applies to that type of work; andany alternative specifications that Council has agreed to in writing and which the developer must ensure do not conflict with any requirements imposed by any applicable laws and standards.
18. Any conflicts associated with proposed and existing services shall be forwarded by the developer to the appropriate controlling authority for approval of any proposed changes.

Stormwater and drainage

19. Stormwater is to be managed in accordance with:
 - a. Approved stormwater plans;
 - b. Queensland Urban Drainage Manual; and
 - c. Pilgrim, DH, (ed)., Australian Rainfall & Runoff – A Guide to Flood Estimation, Institution of Engineers, Australia, Barton, ACT, 1987.
20. Post-development stormwater runoff flows, the characteristics of which include volume, concentration and velocities from the development site, must not exceed pre-development stormwater runoff flows to adjoining properties. Stormwater runoff from the site must not adversely impact on flooding or drainage of properties or roads that are upstream, downstream or adjacent to the site.
21. Discharge of stormwater runoff from the development shall drain freely in all cases, and no nuisance of ponding is to be created as a result of the development.
22. There must be no increases in any silt loads or contaminants in any overland flow from the property being developed during the development process and after the development has been completed. The stormwater disposal system must be designed to include appropriate pollution control devices or methods to ensure no contamination or silting of creeks or other waterways.

Earthworks

23. All earthworks for the development shall be undertaken in accordance with the Institute of Public Works Engineering Australasia Queensland Division.
24. All earthworks, including batters, must be wholly contained within the development site and must not impact on properties, land uses or roads adjoining the site.
25. No contaminated material is to be excavated or disturbed and no contaminated material or waste materials are to be used as fill.
26. Fill material is:
 - a. contained wholly within the site;
 - b. located in a single manageable area;
 - c. located a minimum distance of 10 metres from any property boundary;
 - d. limited to the quantity necessary to establish the proposed works;
 - e. ensures no ponding develops on adjacent land at any time;
 - f. restores all surfaces exposed or damaged by the operations to their original standard immediately on conclusion of the works.

Erosion Control

27. All construction works on site are to be undertaken in accordance with:
 - a. Approved erosion and sediment control plans
 - b. Soil Erosion and Sediment Control Engineering Guidelines for Queensland Construction sites (IE Aust, Queensland Division, Brisbane – or later versions); and
 - c. The Institute of Public Works Engineering Australasia Queensland Division.
28. Erosion and sedimentation controls shall be implemented and maintained at all times during the course of the works. To the extent practicable, erosion and sediment controls are to be established prior to the commencement of works, and be monitored, maintained and adjusted as necessary to ensure ongoing effectiveness.
29. Measures shall be applied to prevent site vehicles tracking sediment and other pollutants onto adjoining roads during the course of the construction period and to prevent dust nuisance during construction. It is the developer's responsibility to ensure that all reasonable measures are taken to protect nearby properties and roads from dust pollution, erosion, siltation or sediment transport.
30. Stockpiles of material capable of being moved by the action of running water shall be stored clear of drainage paths and be prevented from entering the road and/or drainage system.
31. Should it be necessary for the road and/or drainage system to be reinstated or cleaned up due to erosion and/or sedimentation from the site, then such works shall be undertaken immediately.

Roadworks

32. Design and construct the new road, from the Grey Street/Carnarvon Highway intersections as an Industrial Road in accordance with:
 - (a) the approved Road Geometry Plan (Reference 22.1003-002 [Revision 6]) and Typical Road Sections Plan (Reference 22.1003-03 [Revision 6]);
 - (b) provision for stormwater drainage, line marking, signage, street lighting associated with the required road works and road reserve transitions between existing and proposed roads;
 - (c) the developer shall contact Council's Infrastructure Services Department to ensure the design for all civil works is approved prior to commencement of any works onsite.
33. Intersections with Grey Street/Carnarvon Highway and the new road are to be upgraded in accordance with the Department of Transport and Main Roads standards.

Access

34. Any future vehicle access to the proposed new lots is to be constructed from the edge of bitumen seal to the property boundary. Any future crossover is to be constructed to a sealed standard, generally in accordance with IPWEA Drawing – RSD-102, and must be designed to cater for the maximum vehicle size exiting the site, ensuring no damage to the roadway or kerb.

Each of the proposed new lots must ensure that any future driveways or access paths are constructed in accordance with Councils standard designs for residential driveways.

35. The landowner is responsible for the construction and maintenance of crossovers from the road carriageway to the property boundary and all internal vehicle access ways, and for obtaining any approvals that may be required and for complying with the applicable designs and standards.
36. Vehicle crossovers must be located a minimum distance of one metre from any power poles, road signage, stormwater gully pits or other Council assets, unless otherwise specified in the applicable development standards and specifications.

Protection of infrastructure

37. The developer is responsible for locating and protecting any Council and public utility services, infrastructure and assets. Any damage to existing infrastructure (road pavement, existing underground assets, etc.) attributable to the development, shall be immediately rectified in accordance with the asset owners' requirements and specifications and to the satisfaction of the asset owners' representative(s) and at no cost to Council.

Land Dedication

38. Land shown on the Approved Plan as New Road must be dedicated as Road Reserve.

Latest versions

39. Where another condition refers to a specific published standard, manual or guideline, including specifications, drawings, provisions and criteria within those documents, that condition shall be deemed as referring to the latest versions of those publications that are publicly available at the time the first operational works or compliance approval is lodged with the assessment manager or approval agency for those types of works to be performed or approved, unless a regulation or law requires otherwise.

Avoiding nuisance

40. During the establishment of the approved development, no nuisance is to be caused to adjoining properties and occupiers, including by way of smoke, dust, rubbish, contaminant, stormwater discharge or siltation at any time.
41. Dust emanating as a result of any development works onsite must be continually monitored and suppressed in order to prevent any dust impacting on airport operations, drifting onto road networks or nearby properties.
42. Parking areas for vehicles associated with the construction works must be provided within the development site.
43. The transportation of materials, equipment and machinery to and from the site during the course of the construction activities, is to be undertaken in a manner that ensures public access ways and roads are kept free of dust and spoil.
44. Construction activities must not prevent or obstruct access to adjoining properties, land uses or roads.
45. Any waste associated with the development works must be contained and disposed of from the site to prevent release to adjoining properties and roads.
46. The area and its surrounds shall be kept in an orderly fashion, free of rubbish and clear of weeds and long grasses (during construction and post construction). The premises is to be maintained in a clean and tidy condition and not to pose any health and safety risks to the community.

ATTACHMENT 2 – CONCURRENCE AGENCY RESPONSE



SARA reference: 2503-45237 SRA
Council reference: RL127

11 August 2025

Chief Executive Officer
Balonne Shire Council
PO Box 201
ST GEORGE QLD 4487
council@balonne.qld.gov.au

Attention: Kate Swepson

Dear Kate

SARA referral agency response—130-152 Grey Street, St George

(Referral agency response given under section 56 of the *Planning Act 2016*)

The development application described below was confirmed as properly referred by the State Assessment and Referral Agency (SARA) on 1 April 2025.

Response

Outcome:	Referral agency response – with conditions
Date of response:	11 August 2025
Conditions:	The conditions in Attachment 1 must be attached to any development approval
Advice:	Advice to the applicant is in Attachment 2
Reasons:	The reasons for the referral agency response are in Attachment 3

Development details

Description:	Development permit	Reconfiguring a Lot – Subdivision (Five (5) lots into fourteen (14) lots and road reserve)
SARA role:	Referral agency	
SARA trigger:	<ul style="list-style-type: none">Schedule 10, part 9, division 4, subdivision 1, table 1, item 1 (10.9.4.1.1.1) - Development impacting on state transport infrastructureSchedule 10, part 9, division 4, subdivision 2, table 1, item 1 (10.9.4.2.1.1) - Reconfiguring a lot near a state transport	

corridor.

- Schedule 10, part 9, division 4, subdivision 2, table 3, item 1 (10.9.4.2.3.1) - Reconfiguring a lot near a state-controlled road intersection (Planning Regulation 2017)

SARA reference: 2503-45237 SRA

Assessment manager: Balonne Shire Council

Street address: 54 Carnarvon Highway, St George; 130-152 Grey Street, St George

Real property description: Lot 9 & 10 on RP882935 & Lot 1 on SP340406, Lots 1 & 2 on RP55377

Applicant name: Balonne Shire Council

Applicant contact details: 193-199 Grey Street
St George QLD 4487
sharyn.arnold@balonne.qld.gov.au

Human Rights Act 2019 considerations: A consideration of the 23 fundamental human rights protected under the *Human Rights Act 2019* has been undertaken as part of this decision. It has been determined that this decision does not limit human rights.

Representations

An applicant may make representations to a concurrence agency, at any time before the application is decided, about changing a matter in the referral agency response (s.30 Development Assessment Rules). Copies of the relevant provisions are in **Attachment 4**.

A copy of this response has been sent to the applicant for their information.

For further information please contact Danica Clark, Senior Planner, on 4616 7305 or via email ToowoombaSARA@dsdilgp.qld.gov.au who will be pleased to assist.

Yours sincerely



Paul Gleeson
A/Manager

cc Ms Sharyn Arnold, sharyn.arnold@balonne.qld.gov.au

enc Attachment 1 - Referral agency conditions
Attachment 2 - Advice to the applicant
Attachment 3 - Reasons for referral agency response
Attachment 4 - Representations about a referral agency response provisions

Attachment 1—Referral agency conditions

(Under section 56(1)(b)(i) of the *Planning Act 2016* the following conditions must be attached to any development approval relating to this application)

No.	Conditions	Condition timing
Reconfiguring a Lot – Subdivision (Five (5) lots into fourteen (14) lots and road reserve)		
Schedule 10, part 9, division 4, subdivision 1, table 1, item 1 (10.9.4.1.1.1) - Development impacting on state transport infrastructure and Schedule 10, part 9, division 4, subdivision 2, table 1, item 1 - Reconfiguring a lot near a state transport corridor & Schedule 10, part 9, division 4, subdivision 2, table 3, item 1 - Reconfiguring a lot near a state-controlled road intersection (Planning Regulation 2017)— The chief executive administering the <i>Planning Act 2016</i> nominates the Director-General of the Department of Transport and Main Roads to be the enforcement authority for the development to which this development approval relates for the administration and enforcement of any matter relating to the following conditions:		
1.	<u>Carnarvon Highway / Cliff Parsons Drive Intersection</u> (a) Road works comprising CHR and AUL turn treatments are to be provided at the intersection of the Carnarvon Highway and the Cliff Parsons Drive, designed and constructed to accommodate the largest design vehicle legally permitted on the state-controlled road (i.e. Type 1 Road train). (b) The road works must be designed and constructed in accordance with the Department of Transport and Main Roads' Road Planning and Design Manual and any external material referenced therein.	Prior to submitting the Plan of Survey to the local government for approval.
2.	<u>Carnarvon Highway / Buchan Bypass / Proposed Airport Road Intersection</u> (a) Road works comprising CHR and AUL turn treatments are to be provided at the four-way intersection of Carnarvon Highway, Buchan Bypass and Airport Road designed and constructed to accommodate the largest design vehicle legally permitted on the state-controlled road (i.e. Type 1 Road train). (b) The road works must be designed and constructed in accordance with the Department of Transport and Main Roads' Road Planning and Design Manual and any external material referenced therein.	Prior to submitting the Plan of Survey to the local government for approval.
3.	(a) Stormwater management of the development must not cause worsening to the operating performance of the state-controlled road corridor, such that any works on the land must not: <ul style="list-style-type: none"> (i) create any new discharge points for stormwater runoff onto the state-controlled road corridor. (ii) concentrate or increase the velocity of flows to the state-controlled road corridor. (iii) interfere with and/or cause damage to the existing stormwater drainage on the state-controlled road corridor. (iv) surcharge any existing culvert or drain on the state-controlled 	(a) At all times. (b) Prior to obtaining development approval for operational work (c) Within 20 business days of the completion of works.

	<p>road corridor.</p> <p>(v) reduce the quality of stormwater discharge onto the state-controlled road corridor.</p> <p>(vi) impede or interfere with any overland flow or hydraulic conveyance from the state-controlled road corridor.</p> <p>(vii) reduce the floodplain immunity of the state-controlled road corridor.</p> <p>(b) Submit RPEQ certification with supporting documentation to Downs.South.West.IDAS@tmr.qld.gov.au within the Department of Transport and Main Roads, confirming that the development has been designed in accordance with part (a) of this condition.</p> <p>(c) Submit RPEQ certification supporting documentation to Downs.South.West.IDAS@tmr.qld.gov.au within the Department of Transport and Main Roads, confirming that the development has been constructed in accordance with parts (a) and (b) of this condition.</p>	
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Attachment 2—Advice to the applicant

General advice	
1.	Terms and phrases used in this document are defined in the <i>Planning Act 2016</i> , its regulation or the State Development Assessment Provisions (SDAP) (version 3.0). If a word remains undefined it has its ordinary meaning.
2.	<p>Road works approval: Under section 33 of the <i>Transport Infrastructure Act 1994</i>, written approval is required from the Department of Transport and Main Roads to carry out road works on a state-controlled road. Please contact the Department of Transport and Main Roads on 4639 0743 to make an application for road works approval.</p> <p>This approval must be obtained prior to commencing any works on the state-controlled road reserve. The approval process may require the approval of engineering designs of the proposed works, certified by a Registered Professional Engineer of Queensland (RPEQ). The road works approval process takes time – please contact Transport and Main Roads as soon as possible to ensure that gaining approval does not delay construction.</p>

Attachment 3—Reasons for referral agency response

(Given under section 56(7) of the *Planning Act 2016*)

The reasons for the SARA's decision are:

With conditions, the development complies with State code 1 and 6. Specifically, the development:

- does not increase the likelihood or frequency of accidents, fatalities, or serious injury for users of a state-controlled road
- does not adversely impact the structural integrity or physical condition of state-controlled roads, road transport infrastructure, public passenger transport infrastructure or active transport infrastructure
- does not adversely impact the function efficiency of state-controlled roads or future state-controlled roads
- does not adversely impact the state's ability to plan, construct, maintain, upgrade or operate state-controlled roads, future state-controlled roads or road transport infrastructure
- does not significantly increase the cost to plan, construct, upgrade or maintain state-controlled roads, future state-controlled roads or road transport infrastructure.

Material used in the assessment of the application:

- the development application material and submitted plans
- *Planning Act 2016*
- Planning Regulation 2017
- the SDAP (version 3.0), as published by SARA
- the Development Assessment Rules
- SARA DA Mapping system
- section 58 of the *Human Rights Act 2019*

Attachment 4—Representations about a referral agency response provisions

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Development Assessment Rules—Representations about a referral agency response

The following provisions are those set out in sections 28 and 30 of the Development Assessment Rules¹ regarding **representations about a referral agency response**

Part 6: Changes to the application and referral agency responses

28 Concurrence agency changes its response or gives a late response

- 28.1. Despite part 2, a concurrence agency may, after its referral agency assessment period and any further period agreed ends, change its referral agency response or give a late referral agency response before the application is decided, subject to section 28.2 and 28.3.
- 28.2. A concurrence agency may change its referral agency response at any time before the application is decided if—
- (a) the change is in response to a change which the assessment manager is satisfied is a change under section 26.1; or
 - (b) the Minister has given the concurrence agency a direction under section 99 of the Act; or
 - (c) the applicant has given written agreement to the change to the referral agency response.²
- 28.3. A concurrence agency may give a late referral agency response before the application is decided, if the applicant has given written agreement to the late referral agency response.
- 28.4. If a concurrence agency proposes to change its referral agency response under section 28.2(a), the concurrence agency must—
- (a) give notice of its intention to change its referral agency response to the assessment manager and a copy to the applicant within 5 days of receiving notice of the change under section 25.1; and
 - (b) the concurrence agency has 10 days from the day of giving notice under paragraph (a), or a further period agreed between the applicant and the concurrence agency, to give an amended referral agency response to the assessment manager and a copy to the applicant.

¹ Pursuant to Section 68 of the *Planning Act 2016*

² In the instance an applicant has made representations to the concurrence agency under section 30, and the concurrence agency agrees to make the change included in the representations, section 28.2(c) is taken to have been satisfied.

Part 7: Miscellaneous

30 Representations about a referral agency response

30.1. An applicant may make representations to a concurrence agency at any time before the application is decided, about changing a matter in the referral agency response.³

³ An applicant may elect, under section 32, to stop the assessment manager's decision period in which to take this action. If a concurrence agency wishes to amend their response in relation to representations made under this section, they must do so in accordance with section 28.

ATTACHMENT 3 – PLANNING ACT EXTRACT APPEAL RIGHTS

Chapter 6 Dispute resolution Part 1 Appeal rights

228 Appeals to tribunal or P&E Court

(1) Schedule 1 states—

- (a) matters that may be appealed to—
 - (i) either a tribunal or the P&E Court; or
 - (ii) only a tribunal; or
 - (iii) only the P&E Court; and
- (b) the person—
 - (i) who may appeal a matter (the **appellant**); and
 - (ii) who is a respondent in an appeal of the matter; and
 - (iii) who is a co-respondent in an appeal of the matter; and
 - (iv) who may elect to be a co-respondent in an appeal of the matter.

(2) An appellant may start an appeal within the appeal period.

(3) The **appeal period** is—

- (a) for an appeal by a building advisory agency—10 business days after a decision notice for the decision is given to the agency; or
- (b) for an appeal against a deemed refusal—at any time after the deemed refusal happens; or
- (c) for an appeal against a decision of the Minister, under chapter 7, part 4, to register premises or to renew the registration of premises—20 business days after a notice is published under section 269(3)(a) or (4); or
- (d) for an appeal against an infrastructure charges notice—20 business days after the infrastructure charges notice is given to the person; or
- (e) for an appeal about a deemed approval of a development application for which a decision notice has not been given—30 business days after the applicant gives the deemed approval notice to the assessment manager; or
- (f) for any other appeal—20 business days after a notice of the decision for the matter, including an enforcement notice, is given to the person.

Note—

See the P&E Court Act for the court's power to extend the appeal period.

(4) Each respondent and co-respondent for an appeal may be heard in the appeal.

(5) If an appeal is only about a referral agency's response, the assessment manager may apply to the tribunal or P&E Court to withdraw from the appeal.

(6) To remove any doubt, it is declared that an appeal against an infrastructure charges notice must not be about—

- (a) the adopted charge itself; or
- (b) for a decision about an offset or refund—
 - (i) the establishment cost of trunk infrastructure identified in a LGIP; or

- (ii) the cost of infrastructure decided using the method included in the local government's charges resolution.

229 Notice of appeal

- (1) An appellant starts an appeal by lodging, with the registrar of the tribunal or P&E Court, a notice of appeal that—
 - (a) is in the approved form; and
 - (b) succinctly states the grounds of the appeal.
- (2) The notice of appeal must be accompanied by the required fee.
- (3) The appellant or, for an appeal to a tribunal, the registrar must, within the service period, give a copy of the notice of appeal to—
 - (a) the respondent for the appeal; and
 - (b) each co-respondent for the appeal; and
 - (c) for an appeal about a development application under
 - (d) schedule 1, table 1, item 1—each principal submitter for
 - (e) the development application; and
 - (f) for an appeal about a change application under
 - (g) schedule 1, table 1, item 2—each principal submitter for
 - (h) the change application; and
 - (i) each person who may elect to become a co-respondent
 - (j) for the appeal, other than an eligible submitter who is not a principal submitter in an appeal under paragraph (c) or (d); and
 - (k) for an appeal to the P&E Court—the chief executive; and
 - (l) for an appeal to a tribunal under another Act—any other person who the registrar considers appropriate.
- (4) The **service period** is—
 - (a) if a submitter or advice agency started the appeal in the P&E Court—2 business days after the appeal is started; or
 - (b) otherwise—10 business days after the appeal is started.
- (5) A notice of appeal given to a person who may elect to be a co-respondent must state the effect of subsection (6).
- (6) A person elects to be a co-respondent by filing a notice of election, in the approved form, within 10 business days after the notice of appeal is given to the person.

230 Other appeals

- (1) Subject to this chapter, schedule 1 and the P&E Court Act, unless the Supreme Court decides a decision or other matter under this Act is affected by jurisdictional error, the decision or matter is non-appealable.
- (2) The Judicial Review Act 1991, part 5 applies to the decision or matter to the extent it is affected by jurisdictional error.

(3) A person who, but for subsection (1) could have made an application under the Judicial Review Act 1991 in relation to the decision or matter, may apply under part 4 of that Act for a statement of reasons in relation to the decision or matter.

(4) In this section—

decision includes—

- (a) conduct engaged in for the purpose of making a decision; and
- (b) other conduct that relates to the making of a decision; and
- (c) the making of a decision or the failure to make a decision; and
- (d) a purported decision; and
- (e) a deemed refusal.

non-appealable, for a decision or matter, means the decision or matter—

- (a) is final and conclusive; and
- (b) may not be challenged, appealed against, reviewed, quashed, set aside or called into question in any other way under the Judicial Review Act 1991 or otherwise, whether by the Supreme Court, another court, a tribunal or another entity; and
- (c) is not subject to any declaratory, injunctive or other order of the Supreme Court, another court, a tribunal or another entity on any ground.

231 Rules of the P&E Court

- (1) A person who is appealing to the P&E Court must comply with the rules of the court that apply to the appeal.
- (2) However, the P&E Court may hear and decide an appeal even if the person has not complied with rules of the P&E Court.

ATTACHMENT 4 — STATEMENT OF REASONS

The following information is provided in accordance with section 63 of the *Planning Act 2016*.

Description of development	Development Application for Reconfiguring a Lot – Five (5) lots into thirteen (13) lots and dividing land into parts by agreement
Assessment benchmarks	<p>The assessment manager has assessed the application against the following—</p> <ul style="list-style-type: none">• the Darling Downs Regional Plan;• the Maranoa-Balonne Regional Plan;• the State Planning Policy;• the Planning Scheme <p>Reconfiguring a lot code</p>
Relevant matters	N/A – there are no relevant matters for a code assessable application.
Matters raised in submissions	N/A – there are no submissions for a code assessable application.
Reasons for the decision	<ul style="list-style-type: none">• The proposed reconfiguration facilitates the creation of a range of lot sizes to support a range of industrial land uses or aviation related businesses.• The proposed lot layout removes conflicts with boundaries over existing airport infrastructure and constructed roads.• All lots will be serviced by reticulated infrastructure services.• The proposal will not adversely impact mapped areas of regulated vegetation on the site.• The proposal will not impact the viability of the stock route network <p>On balance, the proposal presents no significant inconsistencies with assessment benchmarks. Development conditions have been imposed to ensure compliance to the greatest extent possible.</p>

ATTACHMENT 5 — APPROVED PLANS AND SPECIFICATIONS



Note:

This plan was prepared for BALLONE S.C. as a proposed subdivision to accompany a subdivision application to the Ballone S.C. Council and should not be used for any other purpose. The dimensions, areas and total number of lots shown hereon are subject to field survey and also to the requirements of Council and any other relevant legislation. In particular, no reliance should be placed on this plan for any financial dealings involving the land. This note is an integral part of this plan.

BALONNE SHIRE COUNCIL
Planning Act 2016
This document comprises part
of
Development Permit No.

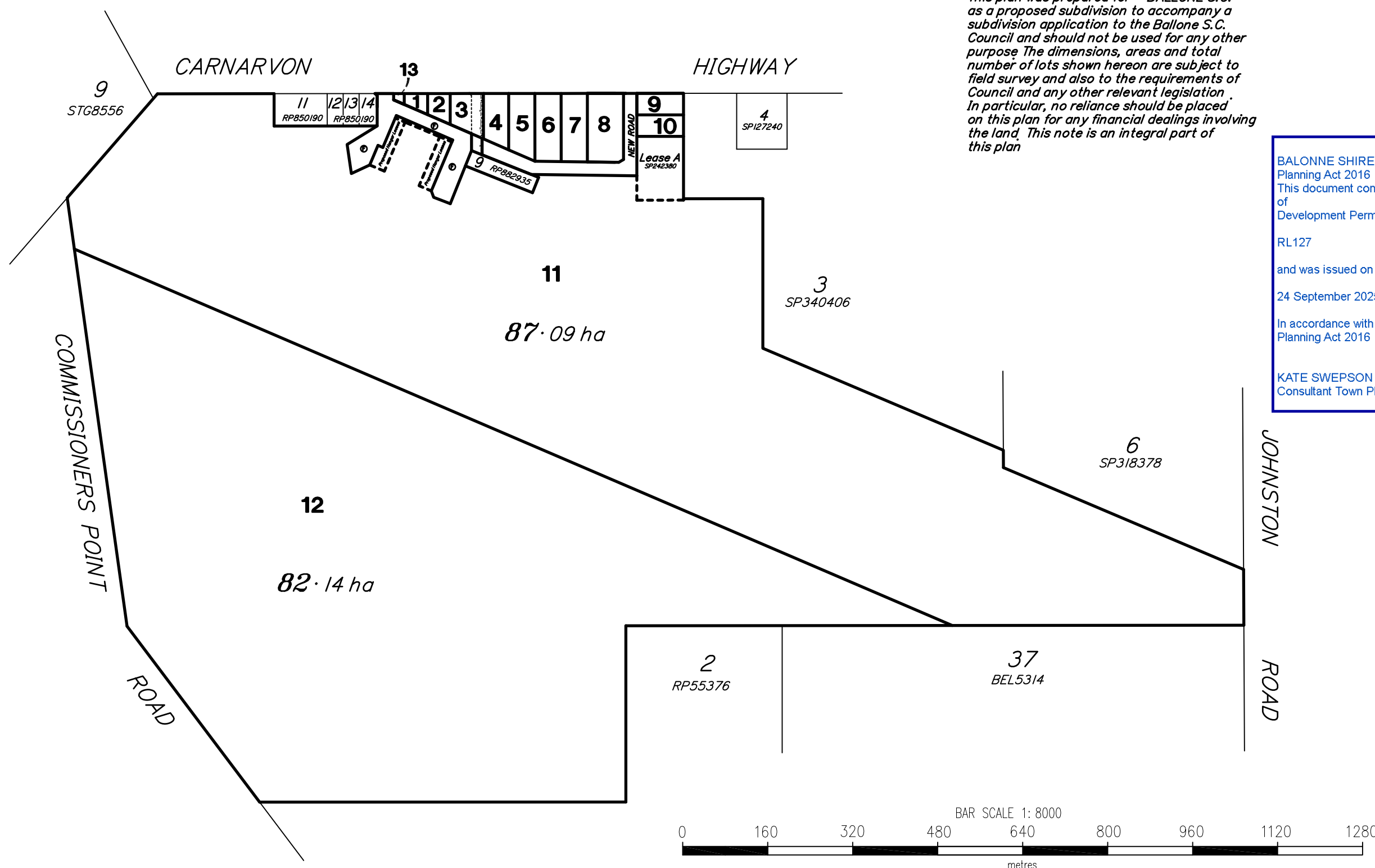
RL127

and was issued on

24 September 2025

In accordance with the :-
Planning Act 2016

KATE SWEPSON
Consultant Town Planner



BALONNE SHIRE COUNCIL

SMK QLD PTY. LTD.

Goondiwindi | Brisbane | Gold Coast | Toowoomba | Gatton
Ph (07)4671 2445 Email admin@smkqld.com.au

PLAN SHOWING PROPOSED LOTS & ROAD ACTIONS
AT ST GEORGE AIRPORT. LOT 1 RP55376,
LOTS 1 & 2 RP55377, LOT 10 RP882935
& LOT 1 SP340406.

24193 REV 4

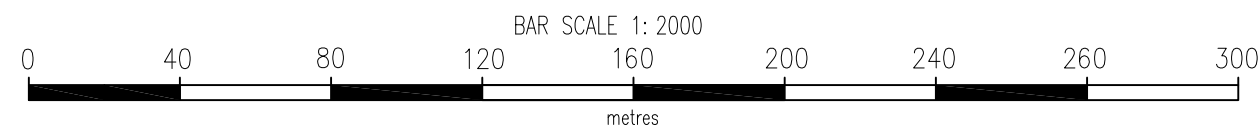
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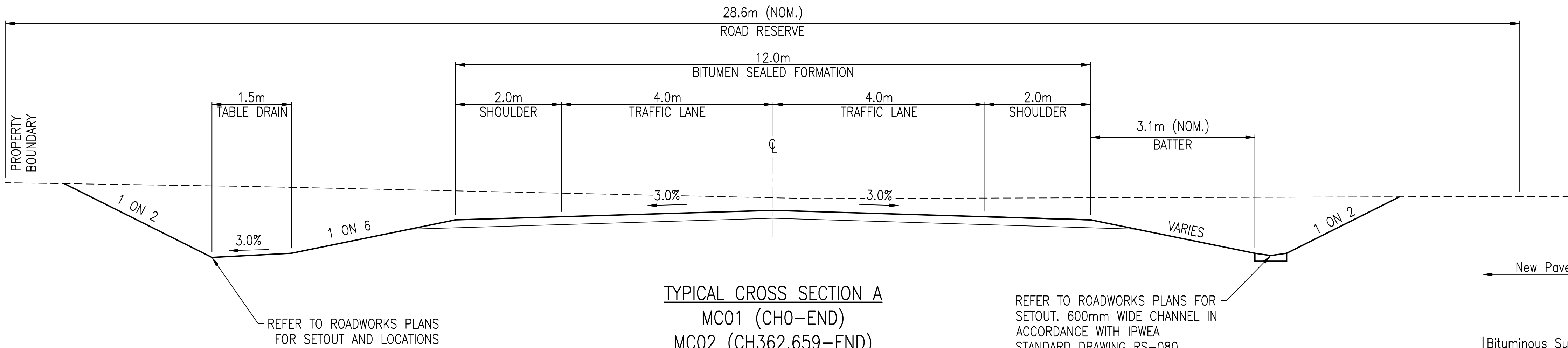
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Drawn RPK 20/12/24 Checked JFJ 20/12/24

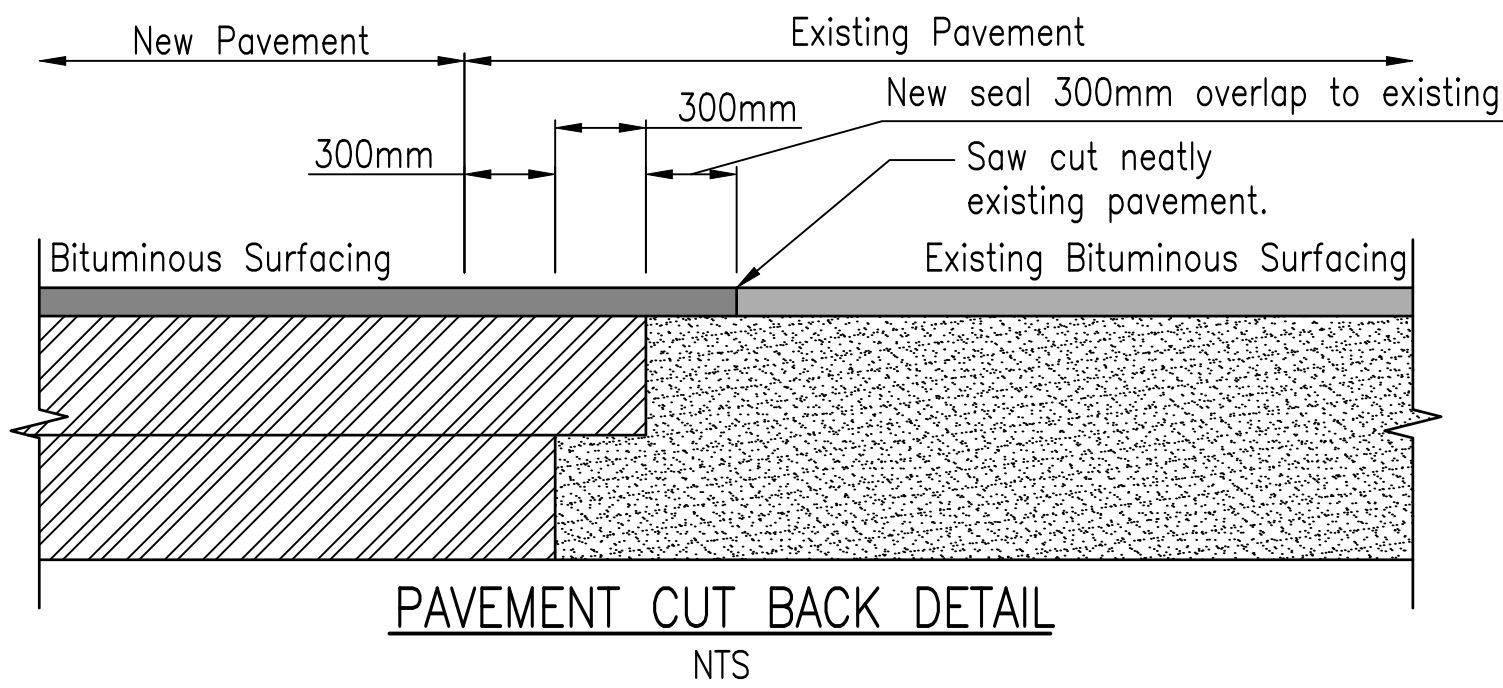
KATE SWEPSON
Consultant Town Planner



Drawn	RPK	20/12/24	Checked	JFJ	20/12/24
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THE CONTRACTOR IS TO NOTIFY THE SUPERVISING ENGINEER OF ANY DISCREPENCIES BETWEEN THE DESIGN PLANS AND THE CONDITIONS ON SITE PRIOR TO COMMENCEMENT OF ANY WORK.



PAVEMENT DETAILS

- DOUBLE/DOUBLE SEAL REFER TO C3 SPRAY SEAL NOTES FOR DETAILS.
- 150mm THICK CBR 60 STABILIZED ROAD BASE CONTRACTOR TO CONFIRM STABILISATION
- SUBGRADE LEVEL CBR 40

SPRAY SEAL SURFACING NOTES

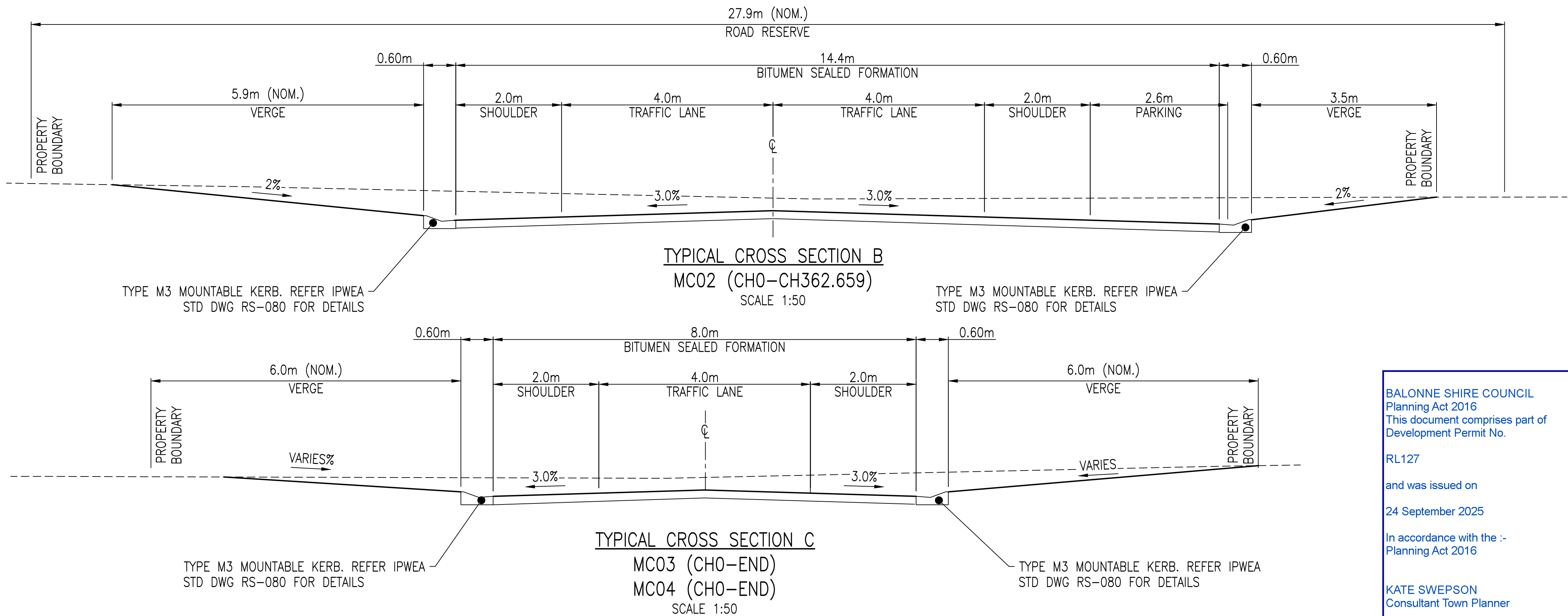
- Spray seal surfacing of the road pavement is to be carried out in accordance with DTMR specification MRTS11.
- Inspection and Testing of Spray Seal Surfacing is to be carried out in accordance with DTMR requirements.
- Double/double seal design to be determined by contractor and to be provided to supervising engineer prior to construction for Approval.

SEAL DESIGN

- 2nd Coat seal – Seal S10E/S35E @ 1.2 L/m² & 10mm Precoated Aggregate @ 140m²/m³
- 1st Coat Seal – Primerseal AMC7 @ 1.6L/m² & 14mm Precoated Aggregate @ 100m²/m³

ROAD SUBGRADE AND GRAVEL PAVEMENT NOTES

- Subgrade is to be trimmed to an even surface, free from loose material, organic matter and graded to be free-draining
- Compact subgrade to 97% characteristic Standard Density Ratio as defined by Queensland main roads test method Q020, based on test methods AS 1289.5.1.1, AS 1289.5.7.1, and AS 1289.5.4.1
- Proof roll subgrade to detect any soft spots, soft spots shall be removed and backfilled with approved sub-base material.
- subgrade affected by rainfall after final trimming is not to be accepted until appropriate drying out treatment has been affected.
- Each pavement course is not to be commenced until the previous course has been inspected and approved by the supervising engineer.
- Select Fill to be minimum CBR15 material, placed in maximum 150mm layers and placed at optimum moisture content and compacted to 97% characteristic standard density Ratio as defined by Queensland Main Roads Test method Q020, based on test methods AS 1289.5.1.1, AS 1289.5.7.1, and AS 1289.5.4.1.
- Pavement courses to be placed as optimum moisture content in 100mm (minimum) – 150mm (maximum) thick layers and compacted to 100% Characteristic standard Dry density Ratio as defined by Queensland Main Roads Test Method Q020, based on test methods AS 1289.5.1.1, and AS1289.5.4.1.
- Compaction and preparation is to be carried out in accordance with Balonne Shire Council Planning Scheme and the contracto shall ensure construction results are provided in accordance with this document.
- Inspections and Testing of compacted material is to be carried out in accordance with Balonne Shire Council Planning Scheme and the contractor shall ensure results are provided in accordance with this document.
- Pavement depths are to be verified by the provision of "as Constructed" Survey information. The contractor shall ensure survey information of each pavement course is collected and submitted in accordance with Balonne Shire Council requirements.



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Planning Act 2016
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Consultant Town Planner

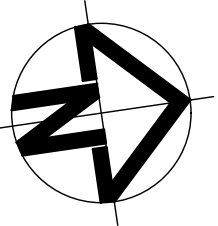
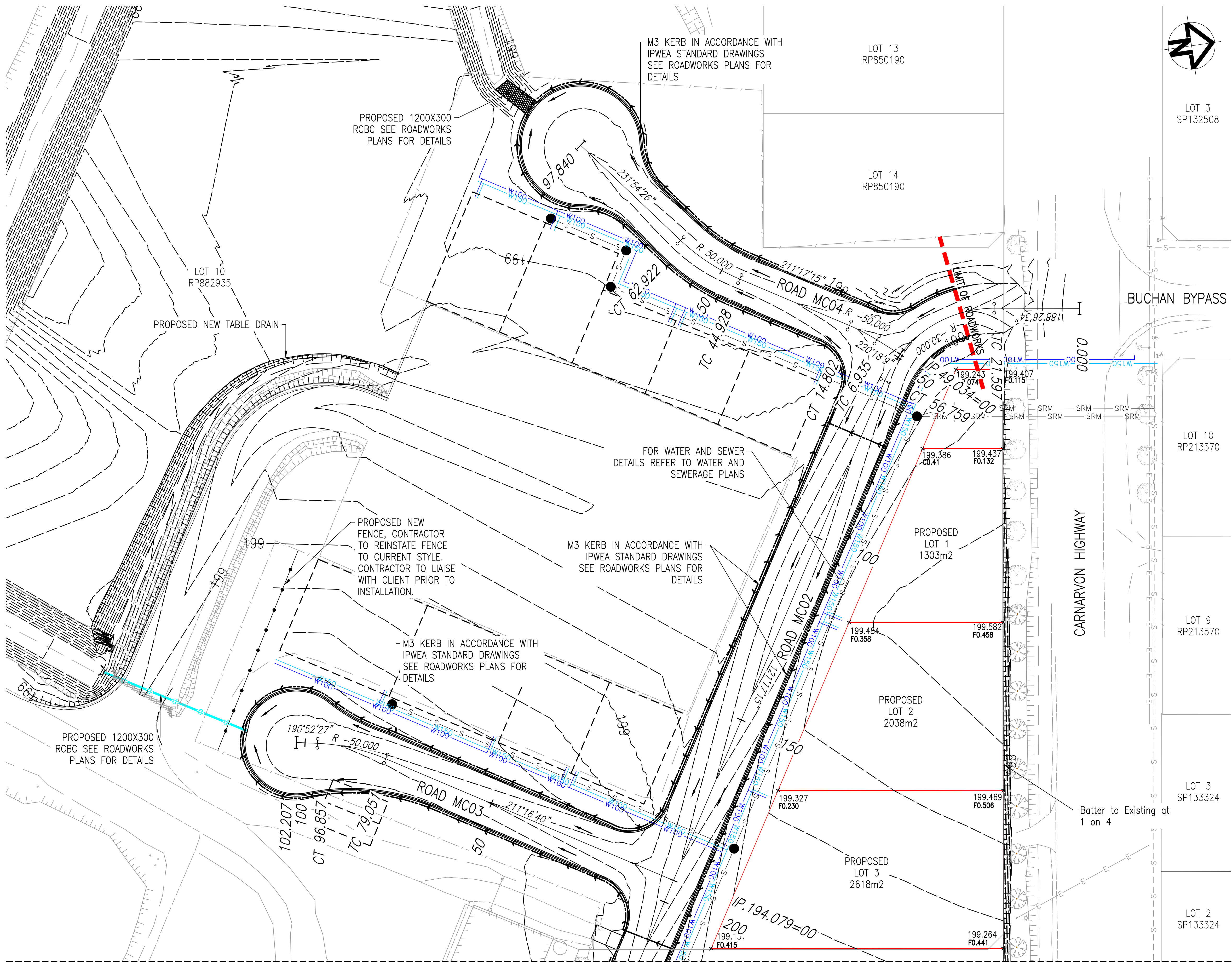
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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23
1	BJS	ISSUE FOR TENDER	LOD	01/06/23
Rev	By	Description	Approved	Date

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<p>Scales:</p> <div><div><div>0m</div><div>0.5</div><div>1.0</div><div>1.5</div><div>2.0</div></div><div>1:50 AT A1</div></div>		Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION TYPICAL ROAD SECTIONS	Drawing No. 22.1003-003	
		Drafter	GCC				Sheet 3 of 42	
		Check	LOD					
		Approved				Client	BALONNE SHIRE COUNCIL	Revision. 6
		A1 ORIGINAL SIZE						



EARTHWORKS LEGEND

- DESIGN SURFACE CONTOURS
- FINISHED ALLOTMENT LEVEL
- CUT/FILL VALUE
- DENOTES EXISTING ALLOTMENT LEVEL
- TOP OF BATTER
- BOTTOM OF BATTER
- INVERT OF DRAIN
- DIRECTION OF KERB & CHANNEL FLOW

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Consultant Town Planner

EXISTING SERVICES

ALL EXISTING SERVICES ARE TO BE
LOCATED BY THE CONTRACTOR
THROUGH CONTACTING THE RELEVANT
SERVICE AUTHORITY PRIOR TO THE
COMMENCEMENT OF ANY WORK

EXISTING LEVELS

THE CONTRACTOR IS TO VERIFY ALL
LEVELS PRIOR TO CONSTRUCTION.
ANY DISCREPANCIES ARE TO BE
REPORTED TO THE SUPERINTENDENT
FOR DIRECTION.

NOTES

- Strip all topsoil, vegetation and deleterious material from eathworks area.
- Proof roll subgrade to detect soft spots. soft spots shall be removed and backfilled with approved fill material.
- All earthworks filling to be in accordance with AS 3798.
- All fill material within the road corridor must be a minimum of CBR15 and fill under proposed structures are to be a minimum of CBR30.
- Filling and testing of proposed dilling shall be carried out in accordance with AS 3798 and Balonne Shire council requirements.
- Test results are to be made available to the superintendent upon request.
- all imported fill material to be minimum CBR15 material, placed in maximum 150mm layers and placed at optimum moisture content and compacted moisture content and compacted in accordance with AS3798 table 5.1
- All fill material won from site must be a minimum of CBR15 and placed maximum 150mm layers and placed at optimum moisture content and compacted in accordance with AS3798 table 5.1
- all fill material must be free of rubbish and organic material.
- Materials unsuitable for use as fill include organic soils, silts, contaminated soils and materials prone to dissolving (discursive soils)
- A topsoil depth of 75mm has been used to determine the earthwork quantities. The contractor is to satisfy himself of the accuracy of these quantities and make any necessary allowance if he disagrees with them.

JOINS SHEET 2
EARTHWORKS LAYOUT
SCALE 1:500

6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23
5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23
1	BJS	ISSUE FOR TENDER	LOD	01/06/23
Rev	By	Description	Approved	Date

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Designed	BJS
Drafter	GCC
Check	LOD
Approved	

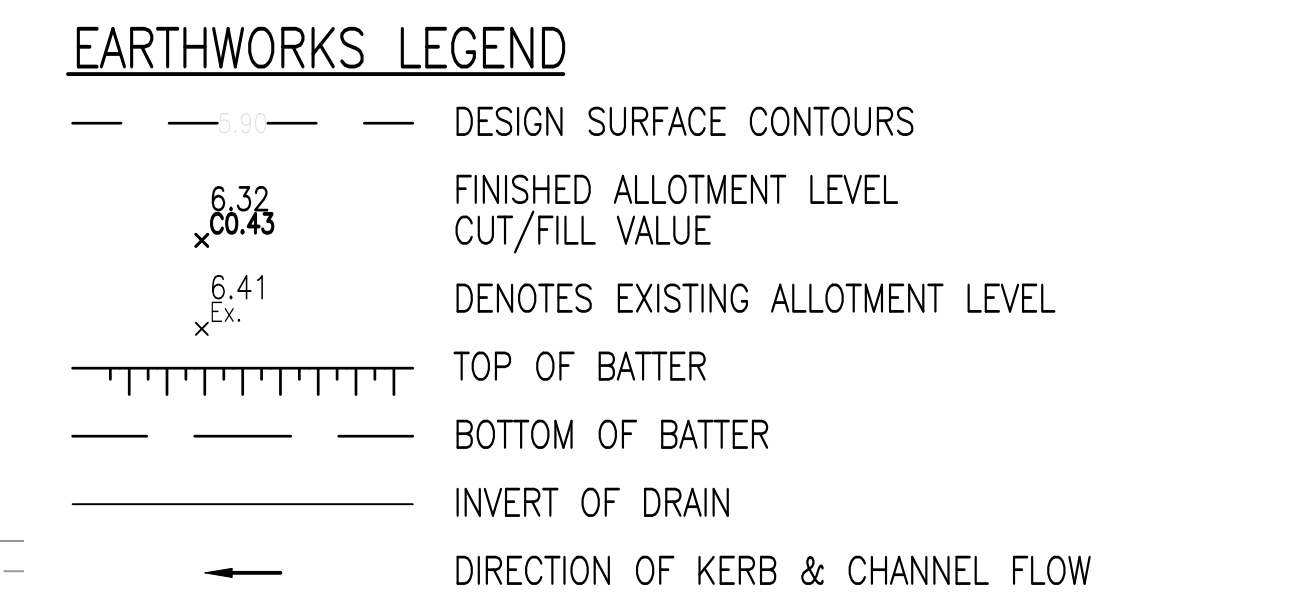
**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
EARTHWORKS LAYOUT PLAN
SHEET 1 OF 3**

Client
BALONNE SHIRE COUNCIL

Drawing No.
22.1003-007

Sheet 7 of 42

Revision. **6**



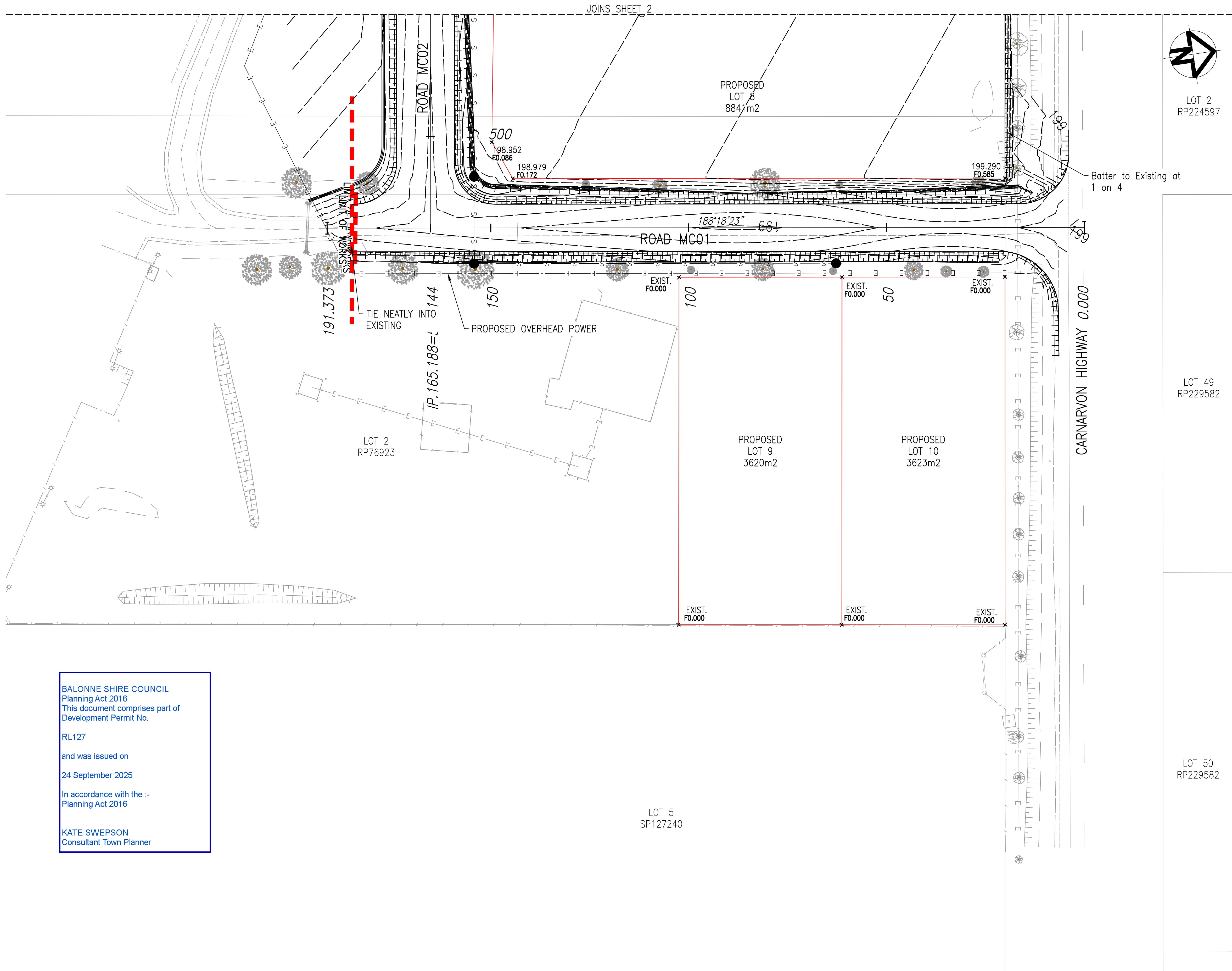
KATE SWEPSON
Consultant Town Planner

ALL EXISTING SERVICES ARE TO BE
LOCATED BY THE CONTRACTOR
THROUGH CONTACTING THE RELEVANT
SERVICE AUTHORITY PRIOR TO THE
COMMENCEMENT OF ANY WORK

THE CONTRACTOR IS TO VERIFY ALL
LEVELS PRIOR TO CONSTRUCTION.
ANY DISCREPANCIES ARE TO BE
REPORTED TO THE SUPERINTENDENT
FOR DIRECTION.

1. Strip all topsoil, vegetation and deleterious material from earthworks area.
2. Proof roll subgrade to detect soft spots. soft spots shall be removed and backfilled with approved fill material.
3. All earthworks filling to be in accordance with AS 3798.
4. All fill material within the road corridor must be a minimum of CBR15 and fill under proposed structures are to be a minimum of CBR30.
5. Filling and testing of proposed dilling shall be carried out in accordance with AS 3798 and Balonne Shire council requirements.
6. Test results are to be made available to the superintendent upon request.
7. all imported fill material to be minimum CBR15 material, placed in maximum 150mm layers and placed at optimum moisture content and compacted moisture content and compacted in accordance with AS3798 table 5.1
8. All fill material won from site must be a minimum of CBR15 and placed maximum 150mm layers and placed at optimum moisture content and compacted in accordance with AS3798 table 5.1
9. all fill material must be free of rubbish and organic material.
10. Materials unsuitable for use as fill include organic soils, silts, contaminated soils and materials prone to dissolving (discursive soils)
11. A topsoil depth of 75mm has been used to determine the earthwork quantities. The contractor is to satisfy himself of the accuracy of these quantities and make any necessary allowance if he disagrees with them.

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5	CDH ISSUE FOR 100% REVIEW	LOD	26/09/23			Scales: <div> 0m 5.0 10 15 20 1:500 AT A1</div>	Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION EARTHWORKS LAYOUT PLAN SHEET 2 OF 3	Drawing No.	22.1003-008	
4	BJS AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	GCC				Sheet		8 of 42
3	BJS AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD						
2	BJS AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved				Client	BALONNE SHIRE COUNCIL	Revision.	6
1	BJS ISSUE FOR TENDER	LOD	01/06/23											
Rev	By	Description	Approved	Date		A1 ORIGINAL SIZE								



EARTHWORKS LEGEND

- DESIGN SURFACE CONTOURS
- FINISHED ALLOTMENT LEVEL
- CUT/FILL VALUE
- DENOTES EXISTING ALLOTMENT LEVEL
- TOP OF BATTER
- BOTTOM OF BATTER
- INVERT OF DRAIN
- DIRECTION OF KERB & CHANNEL FLOW

EXISTING SERVICES

ALL EXISTING SERVICES ARE TO BE LOCATED BY THE CONTRACTOR THROUGH CONTACTING THE RELEVANT SERVICE AUTHORITY PRIOR TO THE COMMENCEMENT OF ANY WORK

EXISTING LEVELS

THE CONTRACTOR IS TO VERIFY ALL LEVELS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES ARE TO BE REPORTED TO THE SUPERINTENDENT FOR DIRECTION.

- NOTES**
- Strip all topsoil, vegetation and deleterious material from earthworks area.
 - Proof roll subgrade to detect soft spots. soft spots shall be removed and backfilled with approved fill material.
 - All earthworks filling to be in accordance with AS 3798.
 - All fill material within the road corridor must be a minimum of CBR15 and fill under proposed structures are to be a minimum of CBR30.
 - Filling and testing of proposed dilling shall be carried out in accordance with AS 3798 and Balonne Shire council requirements.
 - Test results are to be made available to the superintendent upon request.
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 - Materials unsuitable for use as fill include organic soils, silts, contaminated soils and materials prone to dissolving (discursive soils)
 - A topsoil depth of 75mm has been used to determine the earthwork quantities. The contractor is to satisfy himself of the accuracy of these quantities and make any necessary allowance if he disagrees with them.

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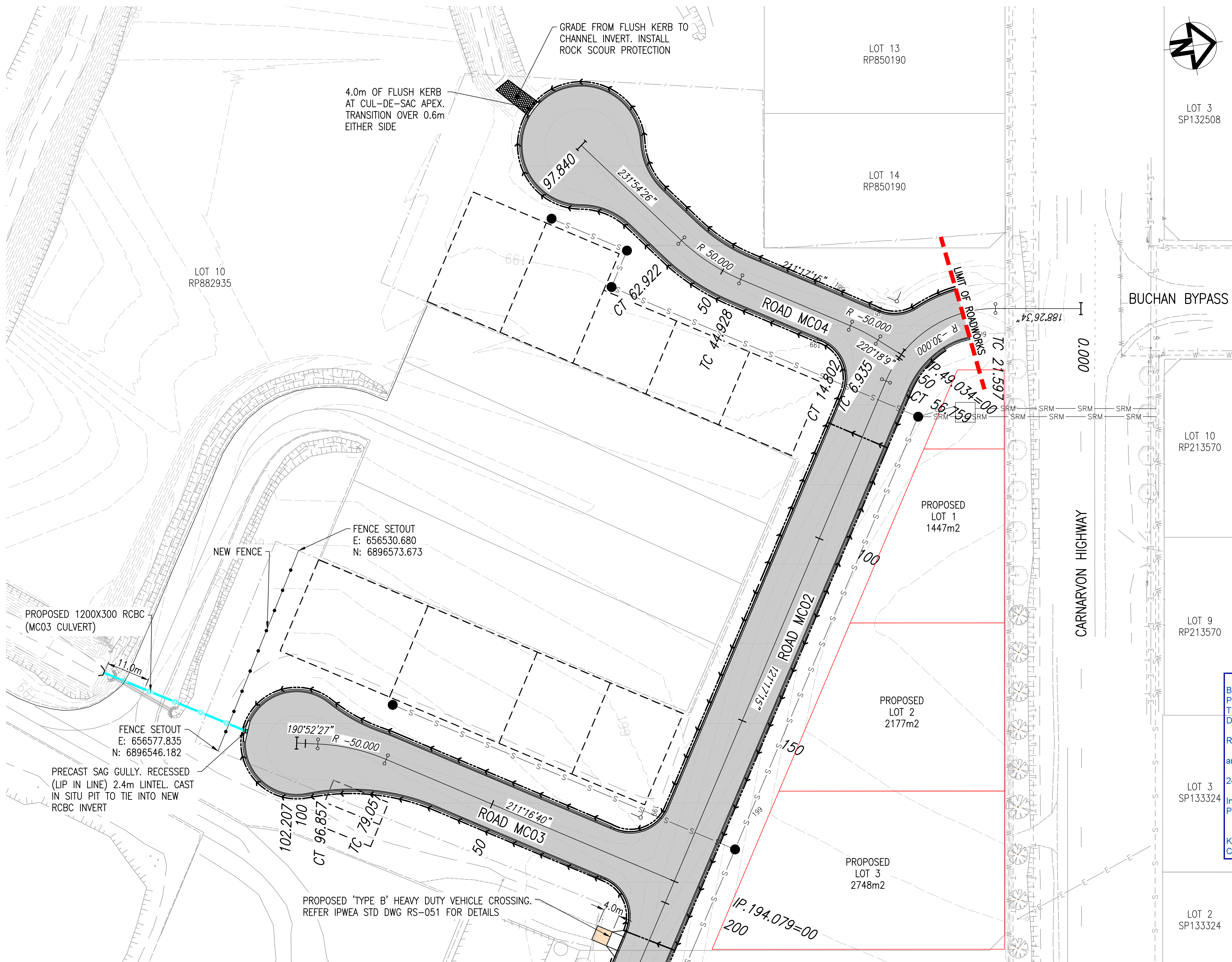
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24 September 2025

In accordance with the :-
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KATE SWEPSON
Consultant Town Planner

EARTHWORKS LAYOUT
SCALE 1:500

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3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23						Check	LOD			
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23			A1 ORIGINAL SIZE			Approved			Client BALONNE SHIRE COUNCIL	Sheet 9 of 42
1	BJS	ISSUE FOR TENDER	LOD	01/06/23										
Rev	By	Description	Approved	Date									Revision.	6



ROADWORKS LEGEND

- TYPE M3 MOUNTABLE KERB IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-080
- DOUBLE STREET NAME PLATE
- DIRECTION OF KERB & CHANNEL FLOW
- PROPOSED PAVEMENT
REFER PAVEMENT DESIGN FOR DETAILS
- PROPOSED CONCRETE DRIVEWAY
REFER PAVEMENT DESIGN FOR DETAILS
- SUBSOIL DRAIN IN ACCORDANCE WITH RS-140 (INDICATIVE LOCATION ONLY)

ROADWORKS NOTES

- ALL WORKS TO BE CONSTRUCTED IN ACCORDANCE WITH RELEVANT LATEST ISSUE COUNCIL STANDARD DRAWINGS AND SPECIFICATIONS, UNLESS NOTED OTHERWISE.
- FOR ROAD GRADING, LEVELS AND TYPICAL SECTIONS, REFER ROAD LONGITUDINAL/CROSS SECTION AND ROADWORKS DETAILS DRAWINGS.
- ALL SUBSOIL DRAINS TO BE GRADED AT MINIMUM 0.5% FROM HIGH POINT TO OUTLET, UNLESS NOTED OTHERWISE.
- SUBSOIL DRAIN TO BE REVERSE GRADED AT MINIMUM 0.5% TO ENSURE POSITIVE GRADE.
- SUBSOIL IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-140.

STORMWATER LEGEND:

- SEWER MAIN
- STORMWATER DRAINAGE LINE
- STORMWATER MANHOLE (MANHOLE NO./LINE NO.)
- KERB INLET PIT (PIT NO./LINE NO.)
- EXISTING STORMWATER DRAINAGE LINE
- EXISTING STORMWATER MANHOLE (MANHOLE NO./LINE NO.)
- EXISTING KERB INLET PIT (PIT NO./LINE NO.)

STORMWATER NOTES

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- FOR STORMWATER SETOUT, LEVELS, PIPE LENGTHS, PIPE CLASS AND STRUCTURE TYPES, REFER STORMWATER LONGITUDINAL DRAWINGS.
- ALL PIPE AND BOX CULVERT CUTS TO BE MADE USING A MASONRY SAW.
- ALL TRENCHES UNDER ROAD PAVEMENT (INCLUDING FUTURE) TO BE BACKFILLED WITH CRUSHER DUST TO SUBGRADE LEVEL. REFER PROJECT SPECIFICATION FOR COMPACTION AND COMPLIANCE TESTING REQUIREMENTS.

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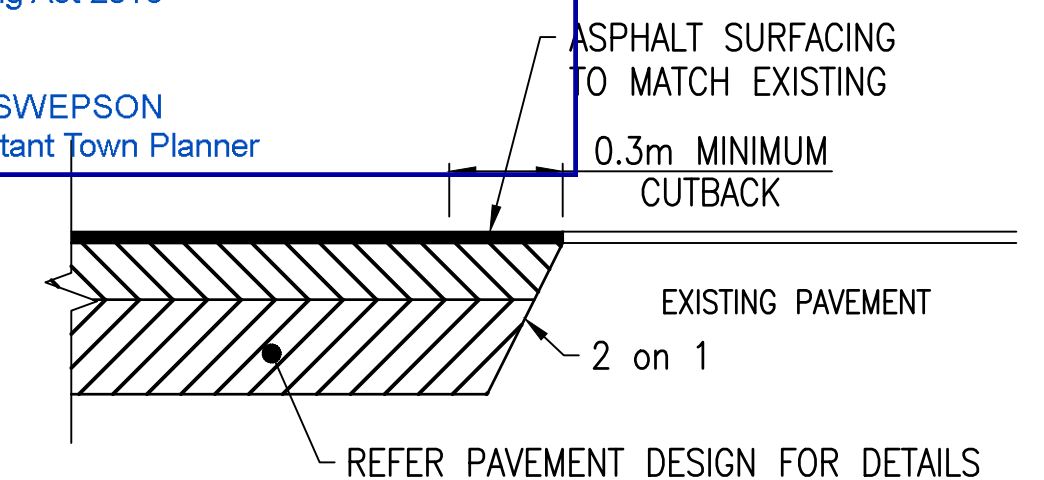
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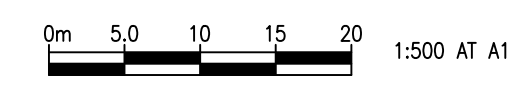
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SCALE 1:20

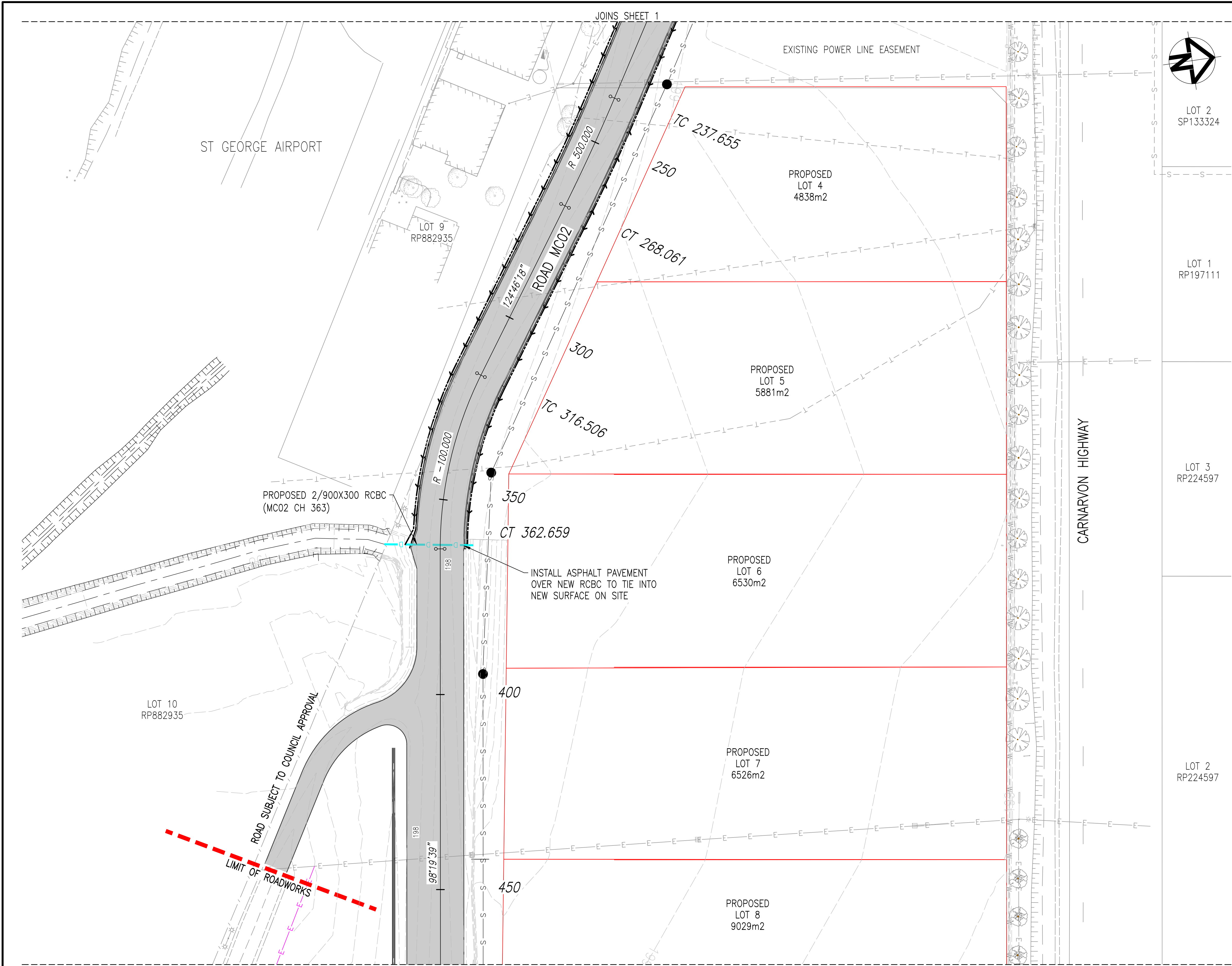
Rev	By	Description	Approved	Date
6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23
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Scales:		Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS AND STORMWATER PLAN SHEET 1 OF 3	
		Drafter	GCC				
A1 ORIGINAL SIZE		Check	LOD				
		Approved		Client		BALONNE SHIRE COUNCIL	Drawing No. 22.1003-010
							Sheet 10 of 42
							Revision. 6



ROADWORKS LEGEND

- TYPE M3 MOUNTABLE KERB IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-080
- DOUBLE STREET NAME PLATE
- DIRECTION OF KERB & CHANNEL FLOW
- PROPOSED PAVEMENT
REFER PAVEMENT DESIGN FOR DETAILS
- PROPOSED CONCRETE DRIVEWAY
REFER PAVEMENT DESIGN FOR DETAILS
- SUBSOIL DRAIN IN ACCORDANCE WITH RS-140 (INDICATIVE LOCATION ONLY)

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STORMWATER LEGEND:

- SEWER MAIN
- STORMWATER DRAINAGE LINE
- STORMWATER MANHOLE (MANHOLE NO./LINE NO.)
- KERB INLET PIT (PIT NO./LINE NO.)
- EXISTING STORMWATER DRAINAGE LINE
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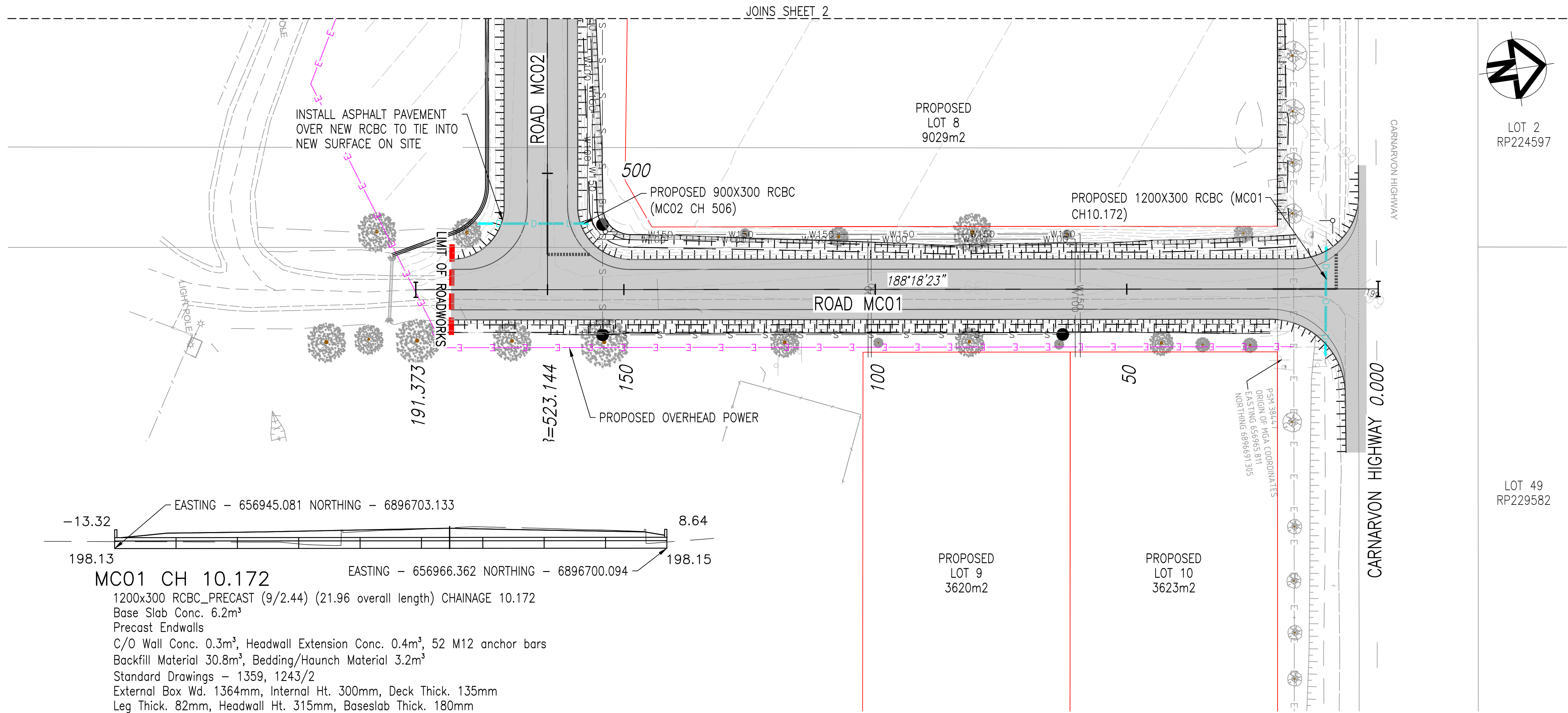
Rev	By	Description	Approved	Date
6	GCC	PUMP STATION DETAILS ADDED		16/10/23
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<div>Scale: 1:500 AT A1</div>		Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS AND STORMWATER PLAN SHEET 2 OF 3	Drawing No. 22.1003-011
		Drafter	GCC				
		Check	LOD				Sheet 11 of 42
		Approved					Revision. 6
A1 ORIGINAL SIZE				Client		BALONNE SHIRE COUNCIL	



ROADWORKS LEGEND

- TYPE M3 MOUNTABLE KERB IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-080
- DOUBLE STREET NAME PLATE
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STORMWATER LEGEND:

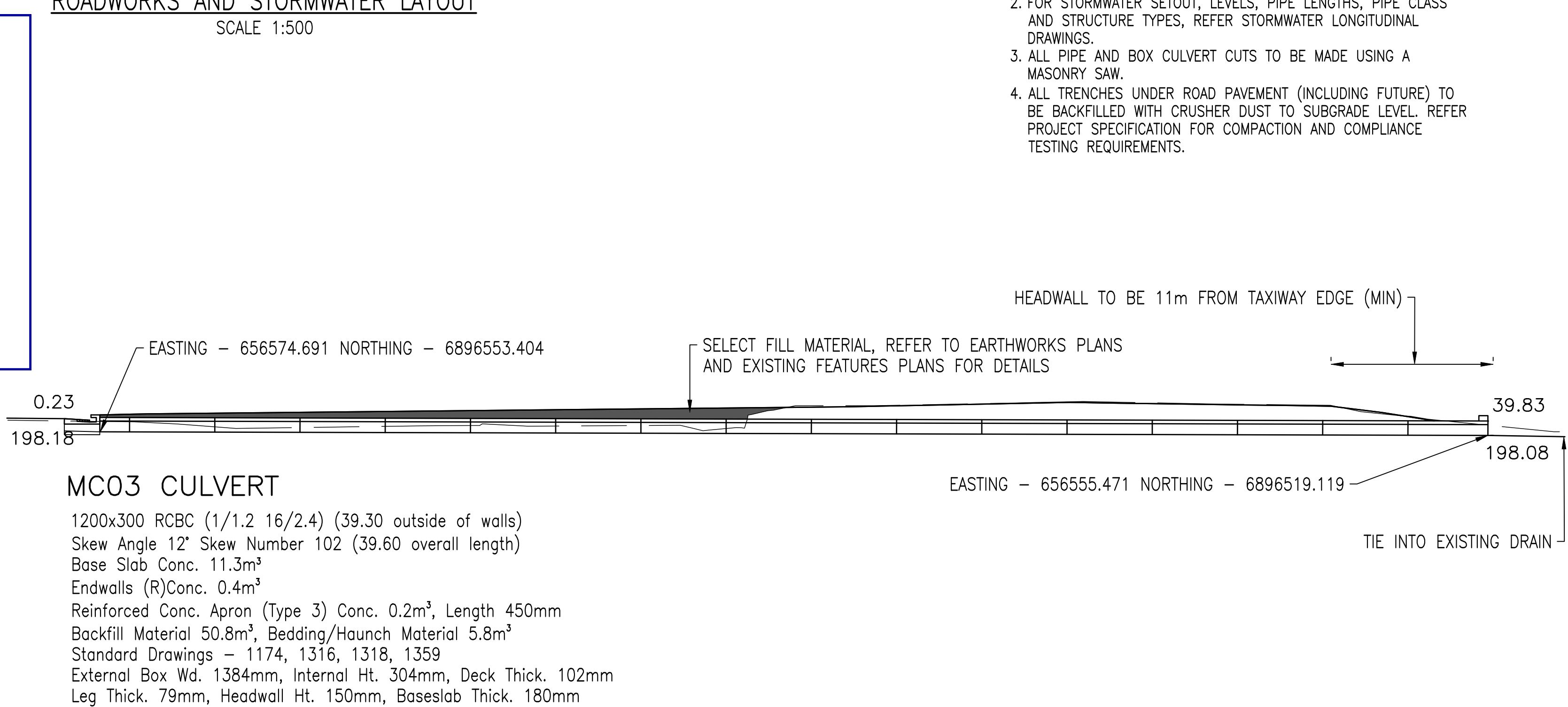
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ROADWORKS AND STORMWATER LAYOUT

SCALE 1:500



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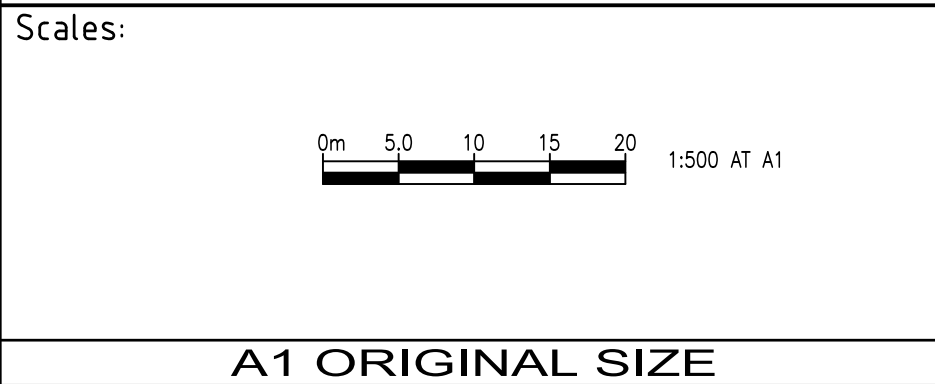
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Approved			

**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
ROADWORKS AND STORMWATER PLAN
SHEET 3 OF 3**

Client
BALONNE SHIRE COUNCIL

Drawing No.
22.1003-012
Sheet 12 of 42
Revision.
6

STORMWATER DESIGN - MINOR: 39%AEP, MAJOR 1%AEP																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																
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STRUCTURE No.	DRAIN SECTION	Sub-Catchment Time of Conc.	I Rainfall Intensity	Sub-Catchment Area	Equivalent Area	Sub-Catchment Discharge	Qa Flow in K&C (Inc. Bypass)	Half Road Capacity	Flow Width	Flow Depth	Flow D/V	Road Grade % at Inlet	Road Xfall % at Inlet	Inlet Type	Inlet Curve	Qg Flow into Inlet	Qb Bypass Flow	Bypass Structure No.	Critical Time of Conc.	I Rainfall Intensity	CA Total (C x A)	Qrat Peak Flow	Q Pipe Flow	L Reach Length	S Pipe Grade	Pipe Size	Pipe Class	Vf=Q/A Full Pipe Velocity	Qcap Capacity w/ Flow	Vcap Capacity Velocity	Vt Travel Velocity	Chart(s) Used	Vf²/2g Velocity w/ Head	Ku U/S Head Loss Coefficient	hu U/S Head Loss	Kw W.S.E Coefficient	hw Change in W.S.E	Sf Pipe Friction Slope	hf Pipe Friction Head Loss	Normal Depth	Normal Depth Vel.	Pipe U/S LL	Pipe D/S LL	Pipe U/S HGL	Pipe D/S HGL	W.S.E	Grate Level	Structure No.																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																
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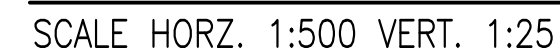
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4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	WF				Sheet 14 of 42					
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD									
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved						Client BALONNE SHIRE COUNCIL	Revision. 6			
1	BJS	ISSUE FOR TENDER	LOD	01/06/23														
Rev	By	Description	Approved	Date	A1 ORIGINAL SIZE													

DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

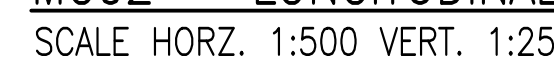
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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23			Scales:  1:500 AT A1  1:25 AT A1	Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC01 LONGITUDINAL SECTION	Drawing No. 22.1003-015	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	GCC				Sheet 15 of 42	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD					
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved				Client BALONNE SHIRE COUNCIL	Revision. 6	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23										
Rev	By	Description	Approved	Date		A1 ORIGINAL SIZE								

DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23			 0m 5.0 10 15 20 1:500 AT A1 0m 0.2 0.4 0.6 0.8 1.0 1:25 AT A1	Scales:		Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC02 LONGITUDINAL SECTION SHEET 1 OF 2	Drawing No. 22.1003-016
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	GCC			Sheet 16 of 42			
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD						
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved				Client BALONNE SHIRE COUNCIL	Revision. 6		
1	BJS	ISSUE FOR TENDER	LOD	01/06/23				A1 ORIGINAL SIZE							
Rev	By	Description	Approved	Date											

PAVEMENT DESIGN
ROAD MC02
DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

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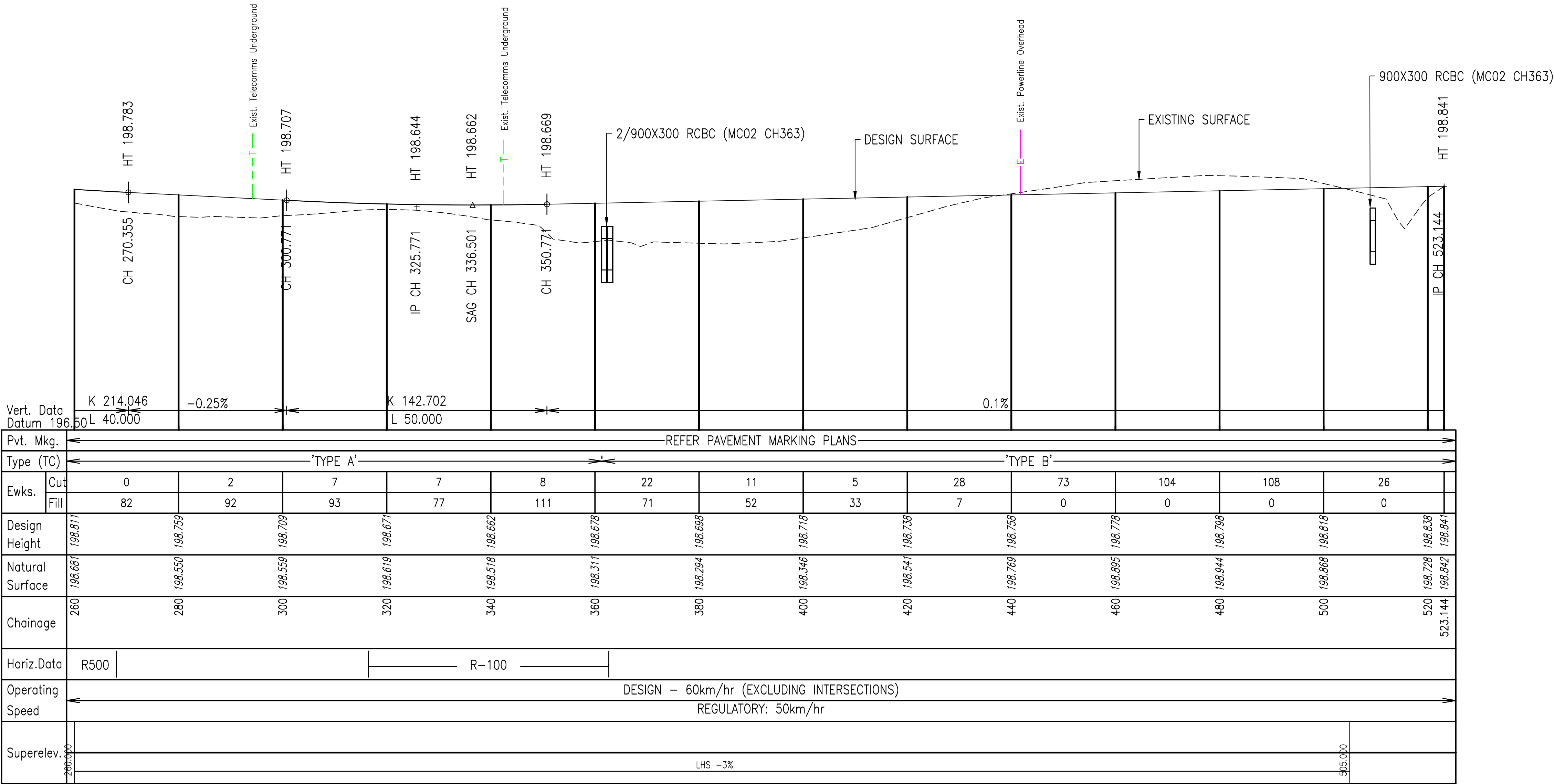
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MC02 – LONGITUDINAL SECTION
SCALE HORZ. 1:500 VERT. 1:25

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23	
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23	
Rev	By	Description	Approved	Date	

PAVEMENT DESIGN
ROAD MC03
DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

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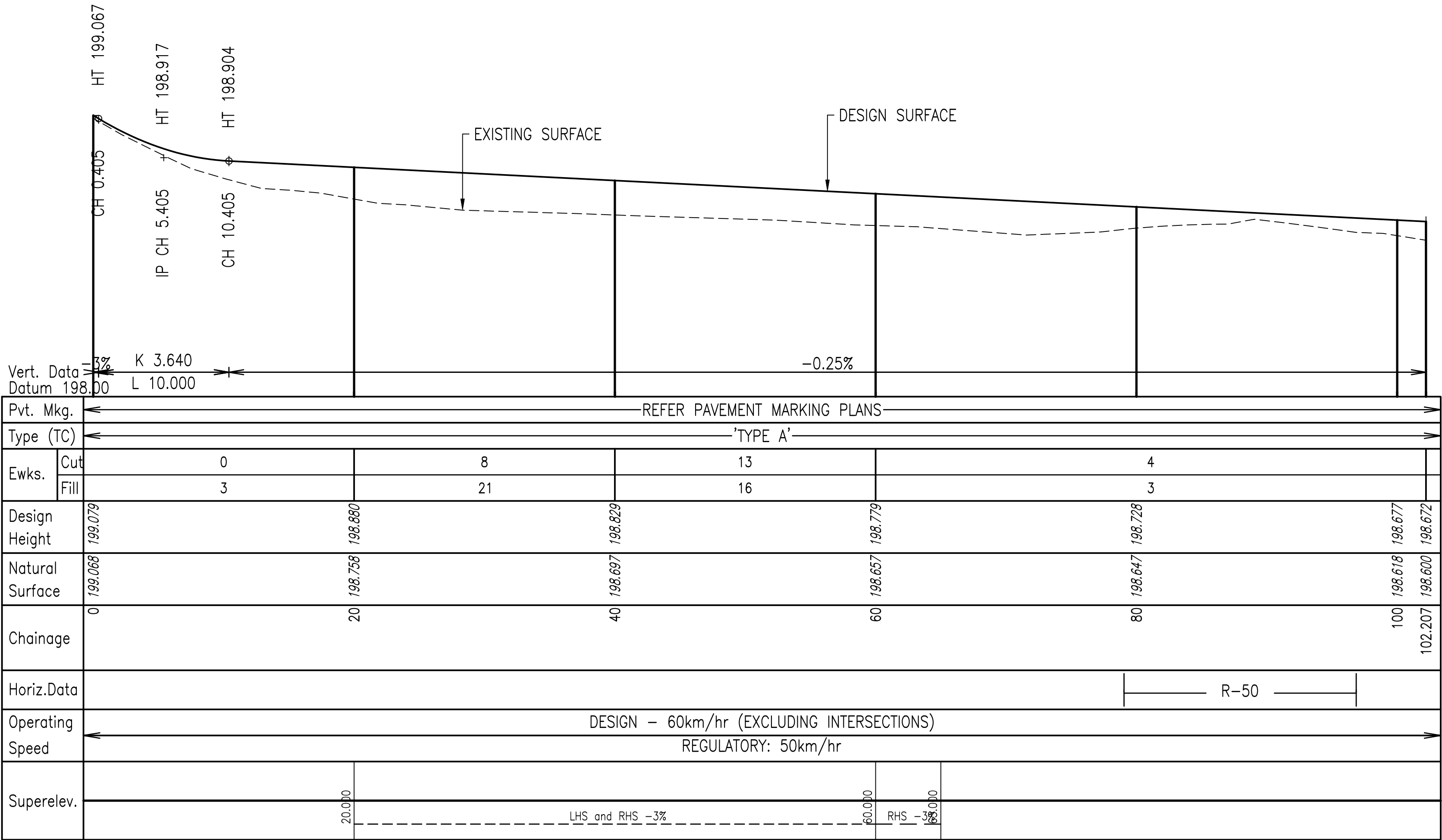
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MC03 – LONGITUDINAL SECTION
SCALE HORZ. 1:500 VERT. 1:25

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23	
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2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23	
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PAVEMENT DESIGN
ROAD MC04
DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

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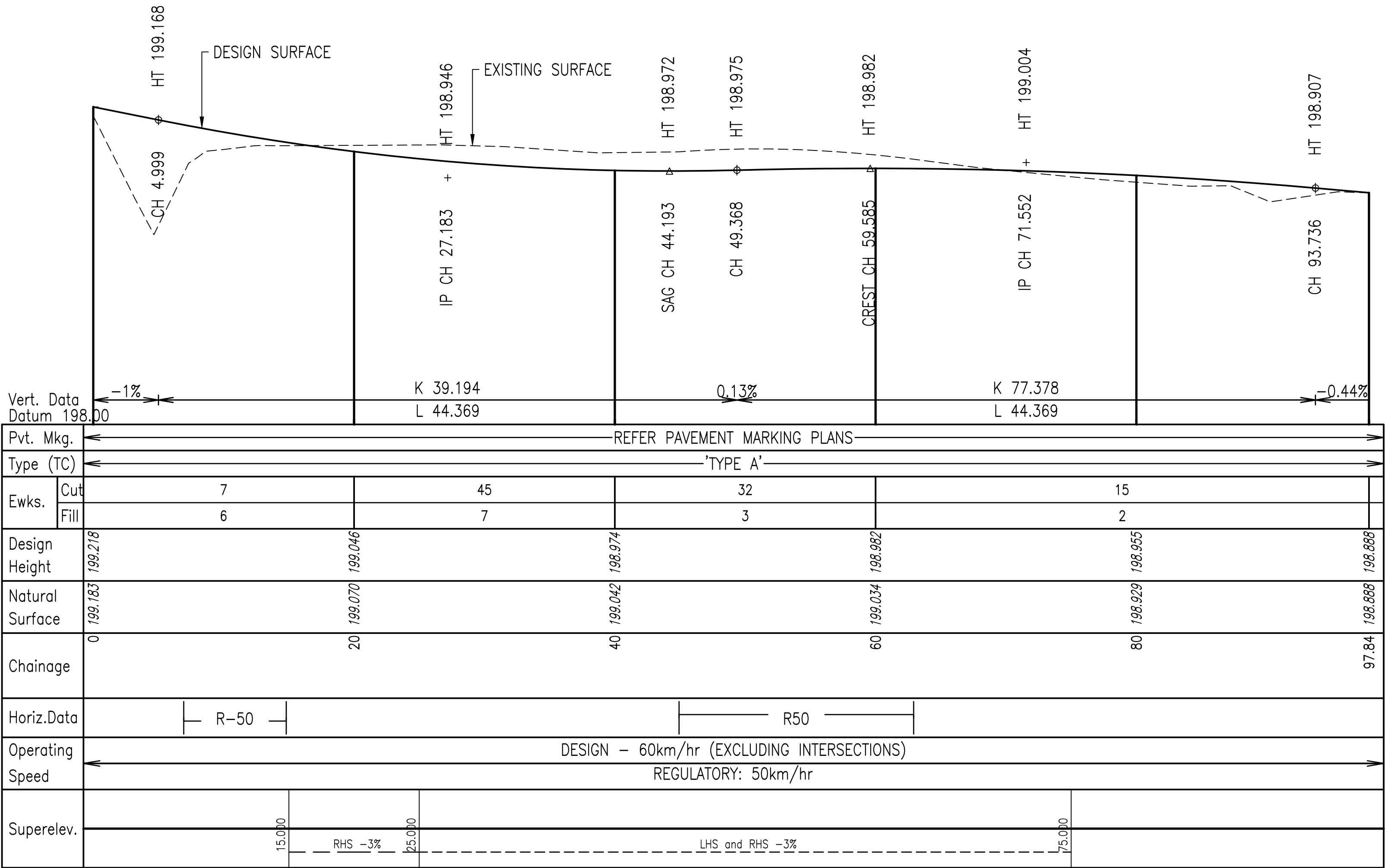
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MC04 – LONGITUDINAL SECTION
SCALE HORZ. 1:500 VERT. 1:25

6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23	<div><div><div><div><div></div><div></div></div><div><div></div><div></div></div></div><div><div><div>PROTERRA GROUP</div><div>31 GLASSER STREET GOONDIWINDI QLD 4390</div><div>ABN: 82 626 886 771</div><div>TEL: (07) 4671 5701</div><div>WWW.PROTERRAGROUP.COM.AU</div></div></div><div><div>DO NOT SCALE FROM THIS DRAWING. USE DIMENSIONS WHERE MARKED</div><div><div>Scales:</div><div><div><div><div>0m</div><div>5.0</div><div>10</div><div>15</div><div>20</div></div><div>1:500 AT A1</div></div><div><div><div>0m</div><div>0.2</div><div>0.4</div><div>0.6</div><div>0.8</div><div>1.0</div></div><div>1:25 AT A1</div></div></div><div>A1 ORIGINAL SIZE</div></div></div><div><div>REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION</div><div><div><div>Designed</div><div>BJS</div></div><div><div>Drafter</div><div>GCC</div></div><div><div>Check</div><div>LOD</div></div><div><div>Approved</div><div></div></div></div><div><div>ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC04 LONGITUDINAL SECTION</div><div>Client BALONNE SHIRE COUNCIL</div></div><div><div>Drawing No. 22.1003-019</div><div>Sheet 19 of 42</div><div>Revision. 6</div></div></div></div></div>
5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23	
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23	
Rev	By	Description	Approved	Date	

Z = 198.945															
Datum 198.00															
DESIGN HEIGHT		198.613	198.613	198.554	198.599	198.765		198.825		198.945	198.825	198.765	198.535	198.490	198.654
EXISTING SURFACE		198.613	198.611	198.564	198.538	198.715		199.013		198.749		198.576	198.488	198.624	198.654
OFFSETS		-8.619	-8.500	-7.000	-6.000	-4.000		0.000		4.000		6.000	7.384	8.884	9.213

CONTROL LINE MC01					
X = 656949.104					
Y = 6896672.486					
Z = 198.849					
Datum 198.00					
	-8.645	198.530	198.530	198.457	
	-8.500	198.530	198.527	198.502	
	-7.000	198.527	198.548	198.669	
	-6.000	198.548		198.729	
	-4.000	198.740	198.729		
	0.000	199.018	198.849		
	4.000	198.900	198.729		
	6.000	198.800	198.669		
	7.452	198.787	198.427		
	8.952	198.807	198.382		
	9.805	198.809	198.809		

CONTROL LINE MC01		X = 656951.993		Y = 6896692.276		Z = 198.878	
Datum 198.00							
DESIGN HEIGHT		199.049	198.758	198.758	198.698	198.319	198.274
EXISTING SURFACE		199.049	198.767	198.773	198.772	198.695	198.671
OFFSETS		0.000	4.000	6.000	8.270	9.770	10.564

CONTROL LINE MC01		X = 656952.514		Y = 6896695.845		Z = 198.898	
Datum 198.00							
DESIGN HEIGHT		198.898	198.778	198.718	198.300	198.255	198.644
EXISTING SURFACE		199.022	198.834	198.801	198.672	198.630	198.644
OFFSETS		0.000	4.000	6.000	8.506	10.006	10.784

Z = 198.906		-3.00% 1 in -6		-3.00%		-3.00%		1 in -6		-3.00% 1 in -2			
Datum 198.00													
DESIGN HEIGHT		198.632	198.632	198.615	198.560	198.726	198.786	198.906	198.786	198.726	198.350	198.305	198.821
EXISTING SURFACE		198.632	198.626	198.515	198.592	198.569	198.677	198.899	198.733	198.643	198.653	198.772	198.821
OFFSETS		-8.734	-8.500	-7.000	-6.000	-4.000	0.000	4.000	6.000	8.256	9.756	10.787	

CONTROL LINE MC01
X = 656937.546
Y = 6896593.325
Z = 198.953

The diagram shows a cross-section of a road profile. A dashed line represents the datum at 198.00. The profile is defined by a solid line with a central peak. The profile is divided into sections by vertical lines. The elevations at the boundaries of these sections are: 198.607, 198.562, 198.607, 198.773, 198.833, 198.953, 198.833, 198.773, 198.665, 198.623, 198.421, and 198.766. The slopes are indicated as -3.00% on the left, -3.00% on the right, and 1 in -6 on the far right. The offsets from the centerline are: -8.591, -8.500, -7.000, -6.000, -4.000, 0.000, 4.000, 6.000, 7.842, 9.342, and 10.032.

		198.607	198.562	198.607	198.773	198.833	198.953	198.833	198.773	198.665	198.623	198.421	198.766
DESIGN HEIGHT		198.607	198.562	198.607	198.773	198.833	198.953	198.833	198.773	198.665	198.623	198.421	198.766
EXISTING SURFACE		198.607	198.606	198.592	198.593	198.734	198.936	198.773	198.665	198.623	198.721	198.421	198.766
OFFSETS		-8.591	-8.500	-7.000	-6.000	-4.000	0.000	4.000	6.000	7.842	9.342	10.032	

CONTROL LINE MC01	
X = 656940.436	
Y = 6896613.115	
Z = 199.000	
Datum 198.00	
DESIGN HEIGHT	198.696 198.608 198.653 198.820 198.880 199.000 198.880 198.820 198.582 198.537 198.750
EXISTING SURFACE	198.696 198.694 198.677 198.670 198.776 198.984 198.785 198.668 198.620 198.722 198.750
OFFSETS	-8.675 -8.500 -7.000 -6.000 -4.000 0.000 4.000 6.000 7.428 8.928 9.354

CONTROL LINE MC01													
X = 656943.325													
Y = 6896632.905													
Z = 199.010													
Datum 198.00													
DESIGN HEIGHT		198.702	198.702	198.618	198.663	198.830	198.890	199.010	198.890	198.830	198.642	198.597	198.749
EXISTING SURFACE		198.702	198.701	198.685	198.677	198.809	199.021	198.783	198.675	198.614	198.726	198.749	
OFFSETS		-8.668	-8.500	-7.000	-6.000	-4.000	0.000	4.000	6.000	7.126	8.626	8.930	

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4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	GCC				Sheet 20 of 42	22.1003-020
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD					
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved				Client BALONNE SHIRE COUNCIL	Revision. 6	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23										
Rev	By	Description	Approved	Date										

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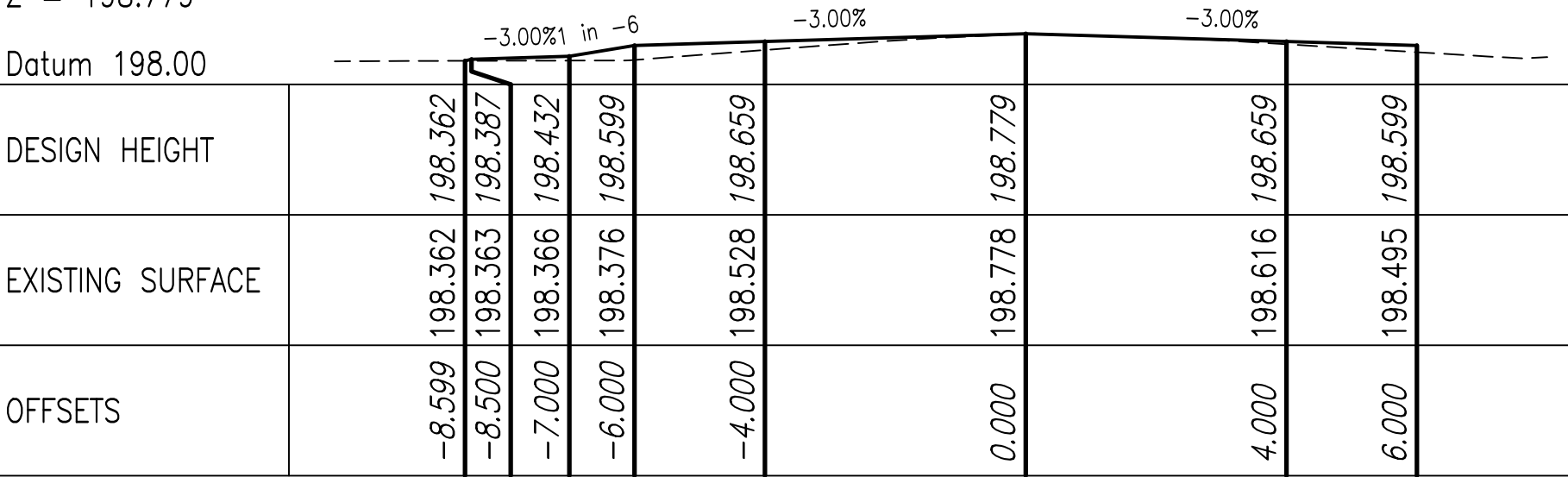
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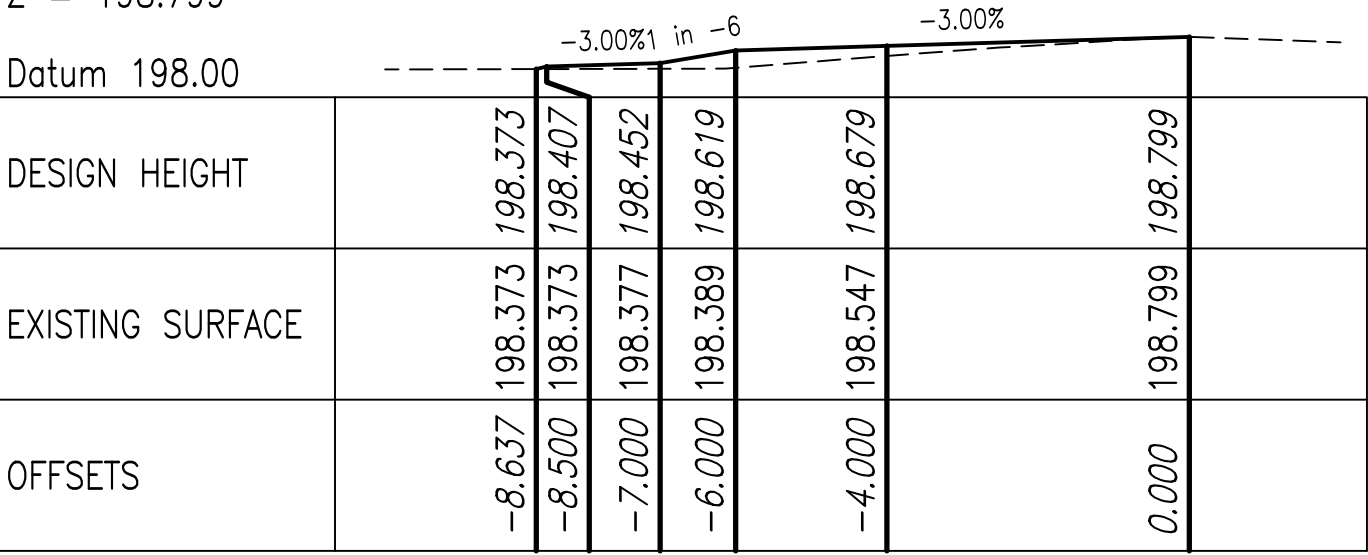
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CONTROL LINE MC01
X = 656928.156
Y = 6896529.007
Z = 198.779



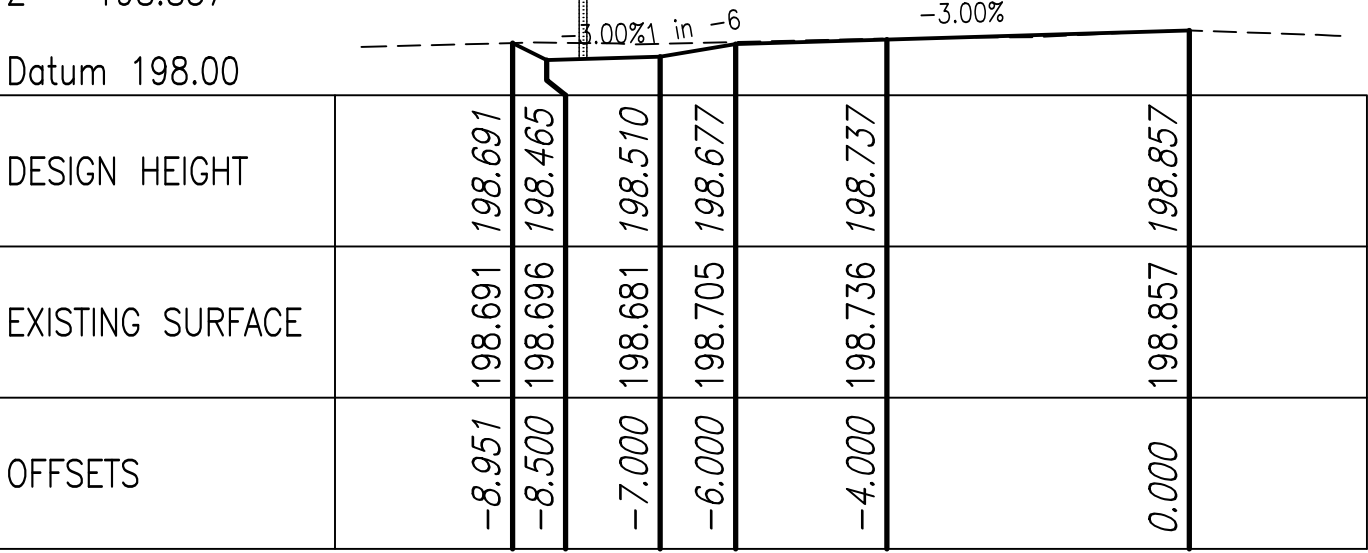
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CONTROL LINE MC01
X = 656928.879
Y = 6896533.954
Z = 198.799



CHAINAGE 180.000

CONTROL LINE MC01
X = 656931.768
Y = 6896553.745
Z = 198.857



CHAINAGE 160.000

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3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23
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Rev	By	Description	Approved	Date

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Scales:		Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC01 CROSS SECTIONS SHEET 2 OF 2	
		Drafter	GCC				
A1 ORIGINAL SIZE		Check	LOD				
Approved				Client		BALONNE SHIRE COUNCIL	Revision. 6

Datum 198.00

CHAINAGE 80.000

Datum 198.00

CHAINAGE 60.000

Datum 198.00

CHAINAGE 40.000

Datum 198.00

CHAINAGE 23.429

Datum 198.00

CHAINAGE 160.000

Datum 198.00

CHAINAGE 140.000

Datum 198.00

CHAINAGE 120.000

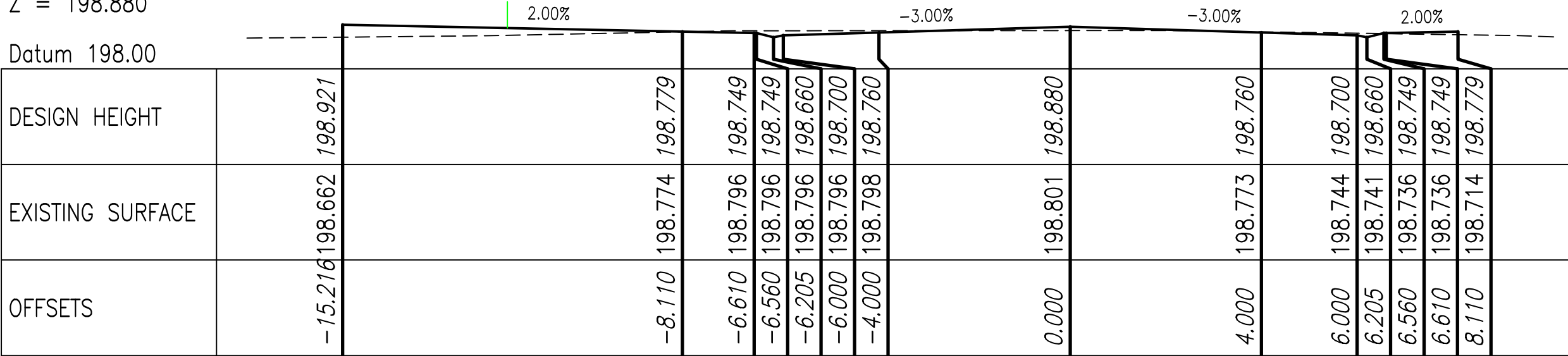
Datum 198.00

CHAINAGE 100.000

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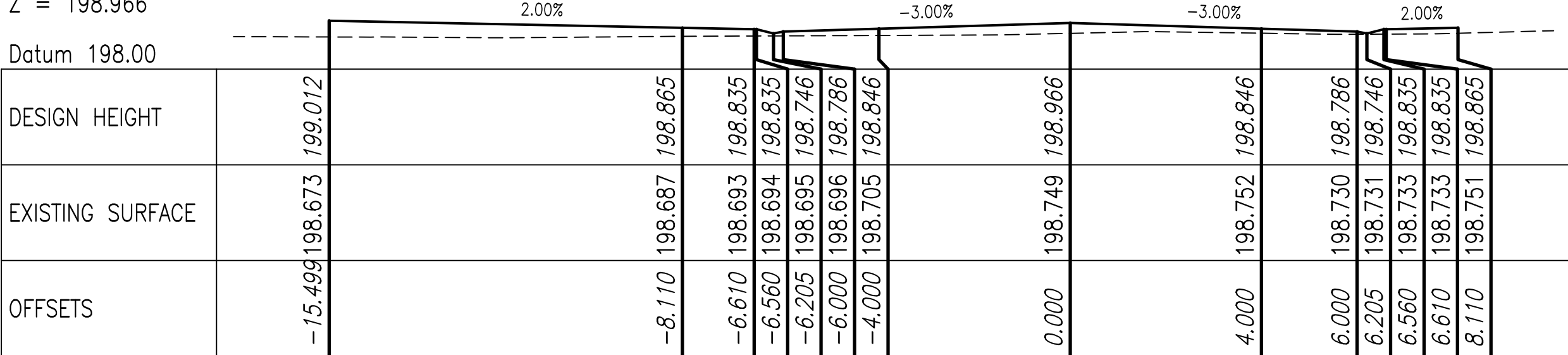
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CONTROL LINE MC02
X = 656666.397
Y = 6896631.634
Z = 198.880



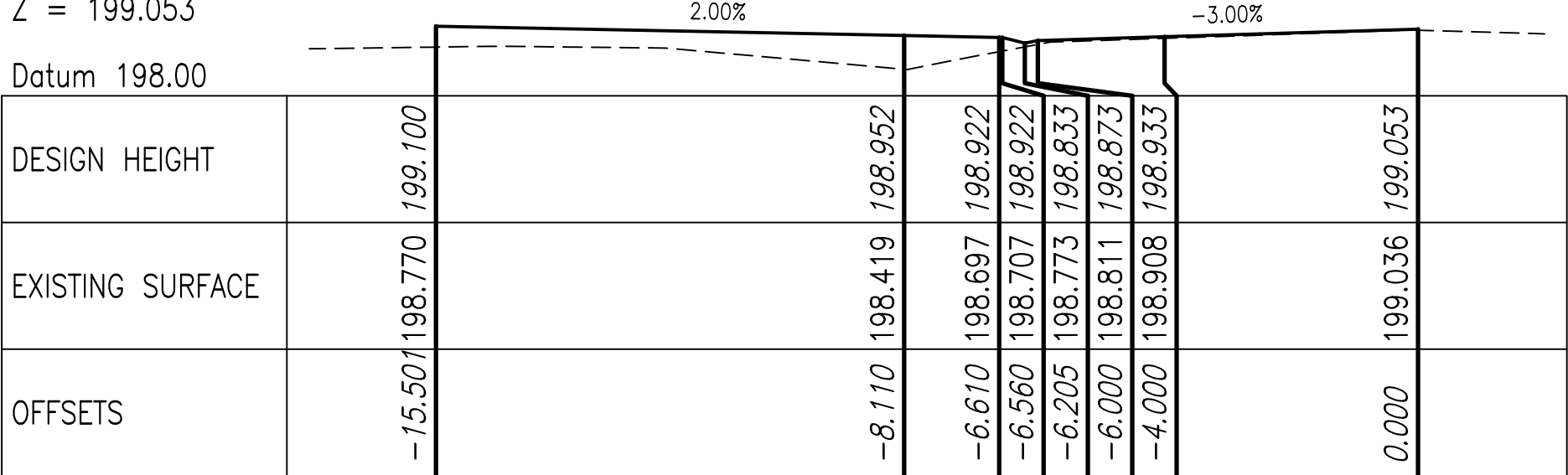
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CONTROL LINE MC02
X = 656649.309
Y = 6896642.025
Z = 198.966



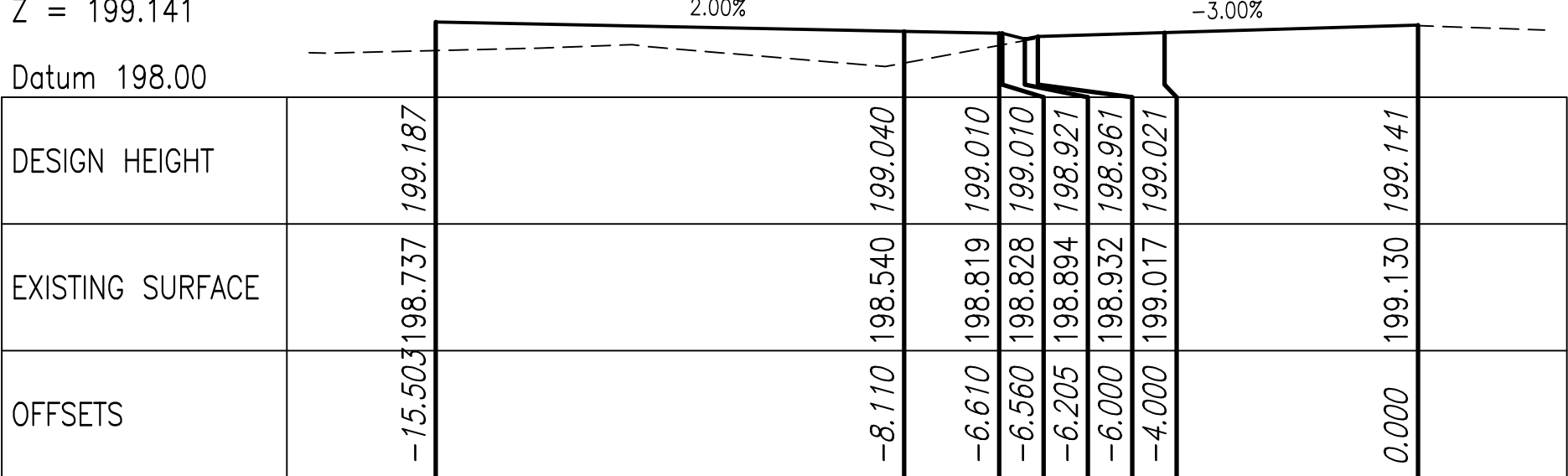
CHAINAGE 220.000

CONTROL LINE MC02
X = 656632.217
Y = 6896652.412
Z = 199.053



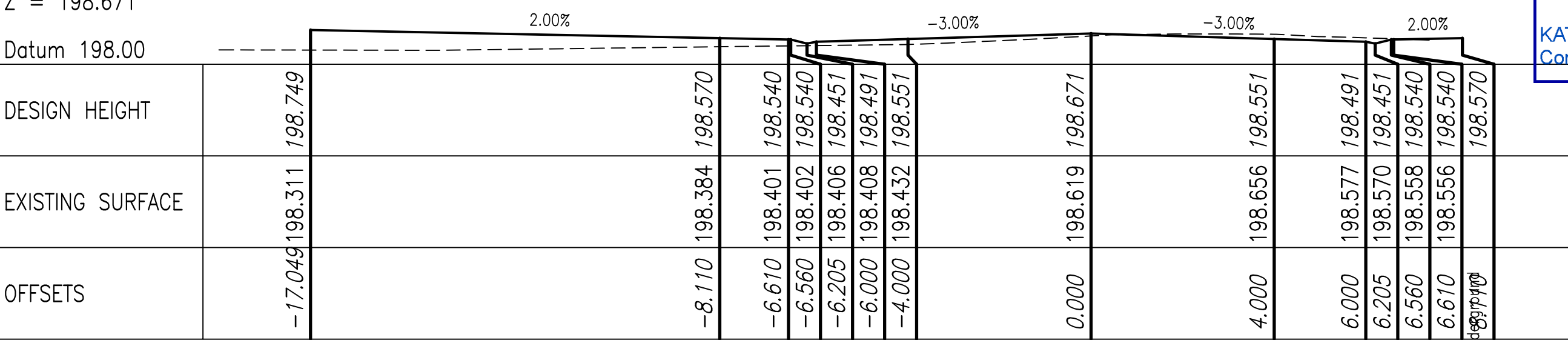
CHAINAGE 200.000

CONTROL LINE MC02
X = 656615.126
Y = 6896662.798
Z = 199.141



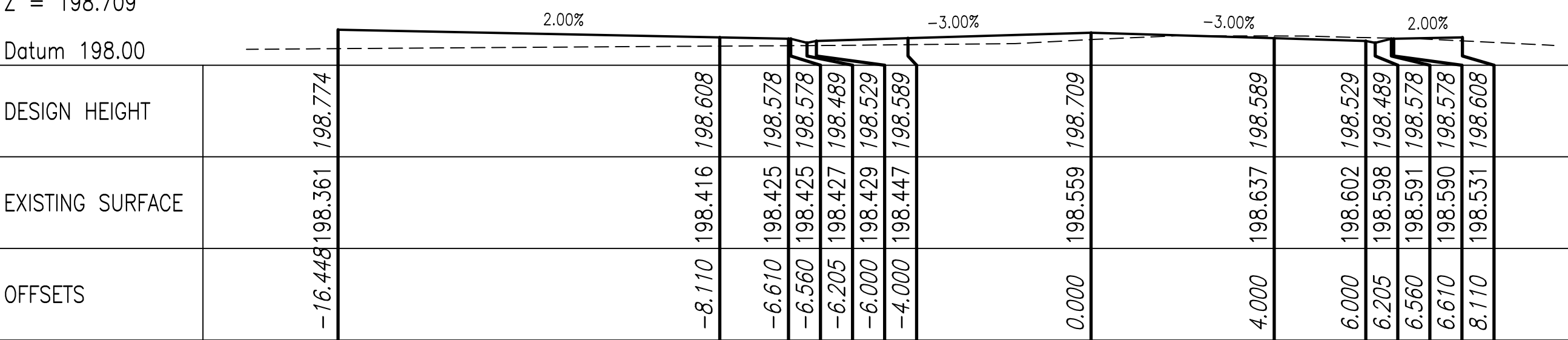
CHAINAGE 180.000

CONTROL LINE MC02
X = 656732.583
Y = 6896586.714
Z = 198.671



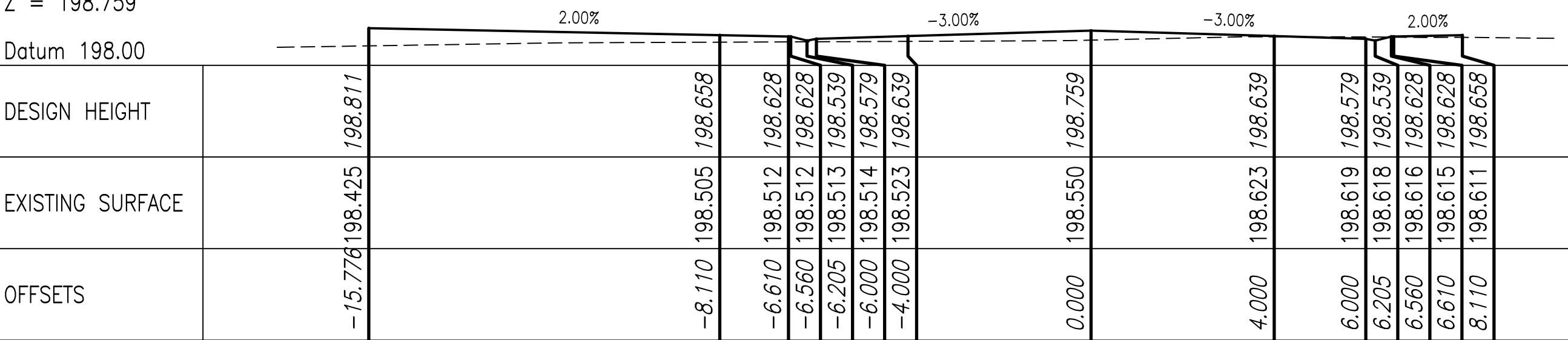
CHAINAGE 320.000

CONTROL LINE MC02
X = 656716.120
Y = 6896598.070
Z = 198.709



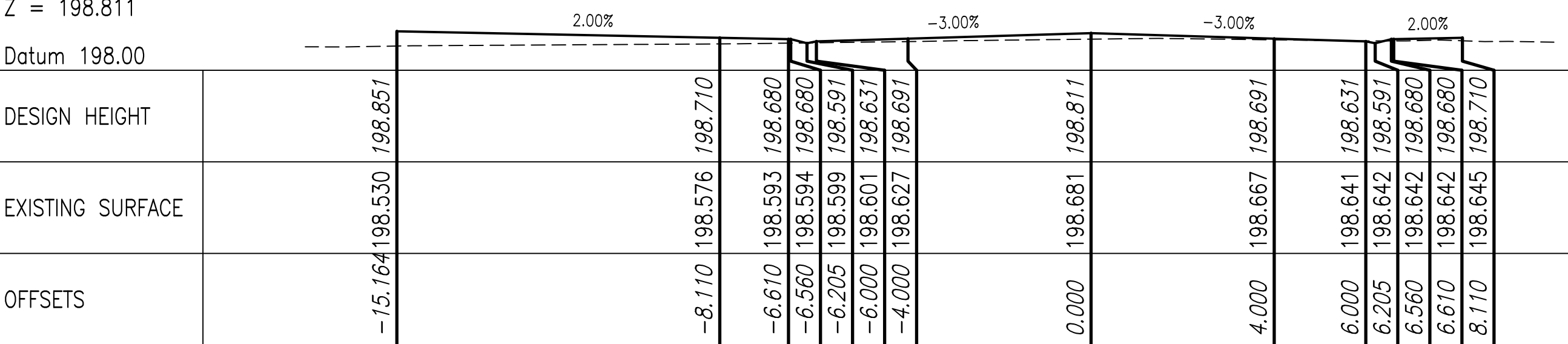
CHAINAGE 300.000

CONTROL LINE MC02
X = 656699.691
Y = 6896609.476
Z = 198.759



CHAINAGE 280.000

CONTROL LINE MC02
X = 656683.226
Y = 6896620.829
Z = 198.811



CHAINAGE 260.000

BALONNE SHIRE COUNCIL
Planning Act 2016
This document comprises part of
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RL127

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24 September 2025

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4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23	
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23	
Rev	By	Description	Approved	Date	



PROTERRA GROUP
31 GLASSER STREET
GOONDIWINDI QLD 4390
ABN: 82 626 886 771
TEL: (07) 4671 5701
WWW.PROTERRAGROUP.COM.AU

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<div>Scales:</div> <div><div><div><div>0m</div><div>1,0</div><div>2,0</div><div>3,0</div><div>4,0</div></div><div>1:100 AT A1</div></div><div><div><div>0m</div><div>0,4</div><div>0,8</div><div>1,2</div><div>1,6</div></div><div>1:40 AT A1</div></div></div> <div>A1 ORIGINAL SIZE</div>				Designed	BJS			<div>ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC02 CROSS SECTIONS SHEET 2 OF 4</div>	Drawing No. 22.1003-023
				Drafter	GCC				Sheet 23 of 42
				Check	LOD				Revision. 6
				Approved		Client BALONNE SHIRE COUNCIL			

Datum 198.00		-3.00% 1 m	
DESIGN HEIGHT		198.370	198.237
EXISTING SURFACE		198.370	198.365
OFFSETS		-9.301	-6.000

Datum 198.00		-3.00% 1 in -6		1 in -6		-3.00%	
DESIGN HEIGHT		198.306	198.137				
EXISTING SURFACE		198.306	198.304				
OFFSETS		-9.855	-8.018				
		-9.518	198.300	198.294	198.518		
				-4.000	198.288	198.578	
				0.000	198.294	198.698	
				4.000	198.381	198.578	
				6.000	198.424	198.518	
				8.051	198.469	198.177	
				9.551	198.501	198.132	
				10.318	198.515	198.515	

Datum	198.00
DESIGN HEIGHT	198.762
EXISTING SURFACE	198.282
OFFSETS	-17.322

Datum 198.00																									
DESIGN HEIGHT		198.708			198.561		198.531		198.442		198.531		198.542		198.662		198.542		198.482		198.531		198.561		
EXISTING SURFACE		198.296			198.356		198.370		198.373		198.375		198.393		198.518		198.623		198.598		198.590		198.576		198.574
OFFSETS		-15.416			-8.110		-6.610		-6.205		-6.000		-4.000		0.000		4.000		6.000		6.205		6.560		8.110

[illegible][illegible]

Datum 198.00									
DESIGN HEIGHT		198.851	198.851	198.438	198.483	198.578	198.638	198.758	198.851
EXISTING SURFACE		198.851	198.849	198.833	198.827	198.802	198.769	198.744	198.717
OFFSETS		-8.900	-8.073	-6.573	-6.000	-4.000	0.000	4.000	8.600

Datum 198.00		-3.00%		-3.00%		-3.00%		-3.00%	
DESIGN HEIGHT		198.554	198.554	198.553	198.553	198.549	198.549	198.558	198.558
EXISTING SURFACE		198.554	198.554	198.553	198.553	198.549	198.549	198.558	198.558
OFFSETS		-8.988	-8.988	-8.555	-8.555	-7.055	-7.055	-6.000	-6.000

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23			Scales:  1:100 AT A1  1:40 AT A1	Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC02 CROSS SECTIONS SHEET 3 OF 4	Drawing No.	22.1003-024
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	GCC				Sheet 24 of 42	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD					
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved					Client BALONNE SHIRE COUNCIL	Revision. 6
1	BJS	ISSUE FOR TENDER	LOD	01/06/23										
Rev	By	Description	Approved	Date	A1 ORIGINAL SIZE									

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Consultant Town Planner

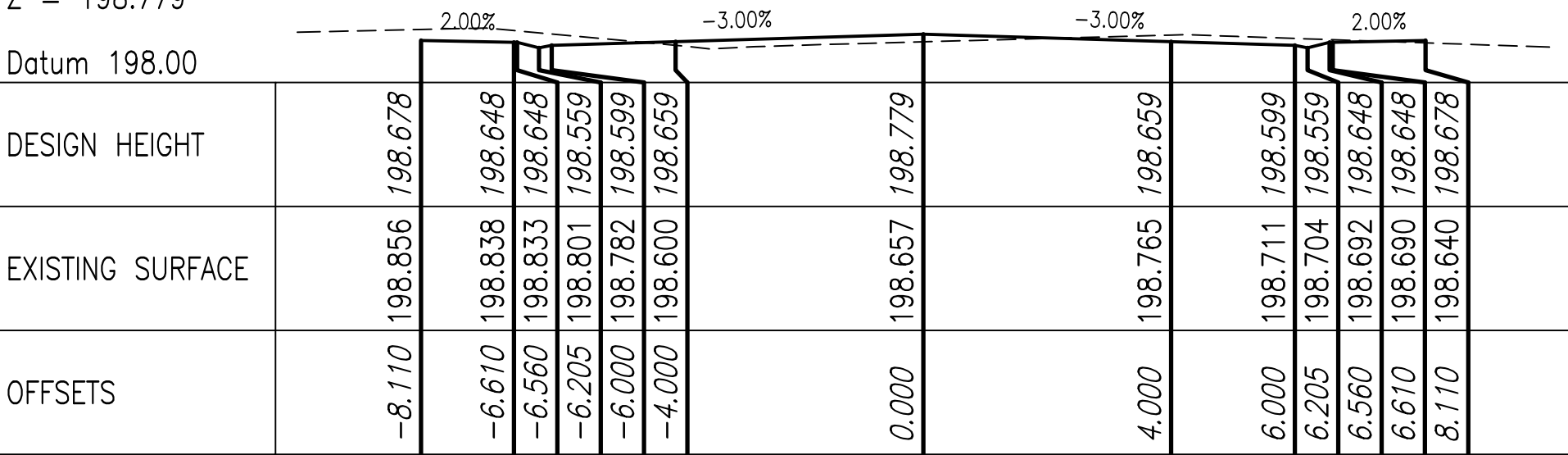
DESIGN HEIGHT	198.825	198.705	198.567
EXISTING SURFACE	198.788	198.778	198.765
OFFSETS	0.000	4.000	8.600

DESIGN HEIGHT		198.887	198.887	198.880	198.876	198.874	198.868	198.863	198.851
EXISTING SURFACE		198.887	198.884	198.880	198.876	198.874	198.868	198.863	198.851
OFFSETS		-10.929	-9.632	-8.132	-6.000	-4.000	0.000	4.000	8.600

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23	
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23	
Rev	By	Description	Approved	Date	

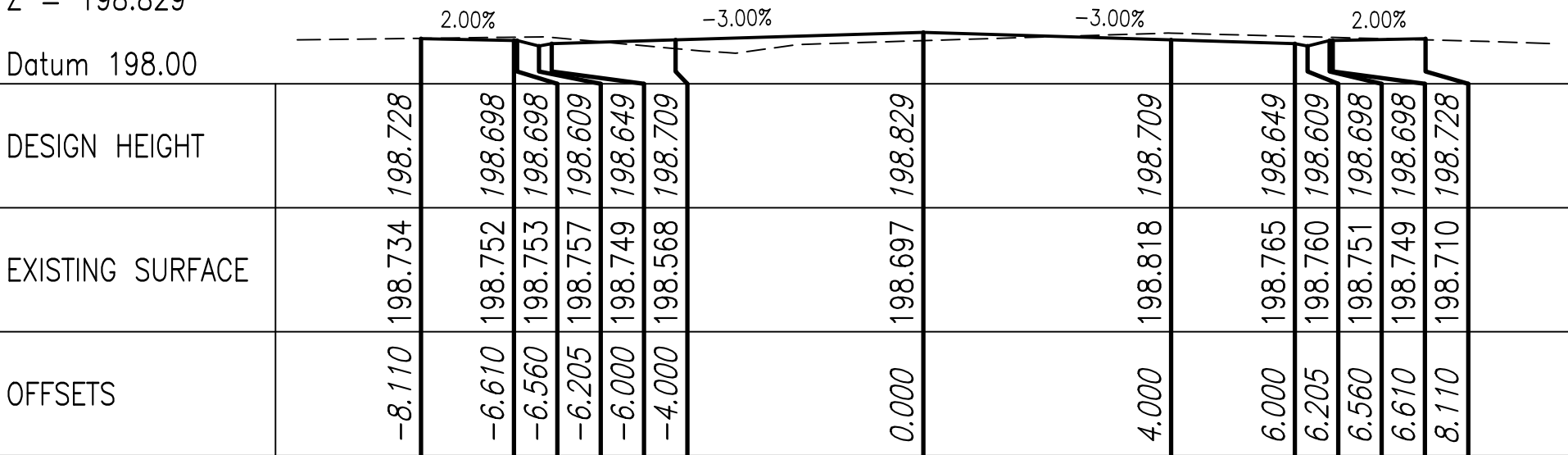
Revision. **6**

CONTROL LINE MC03
X = 656596.007
Y = 6896604.207
Z = 198.779



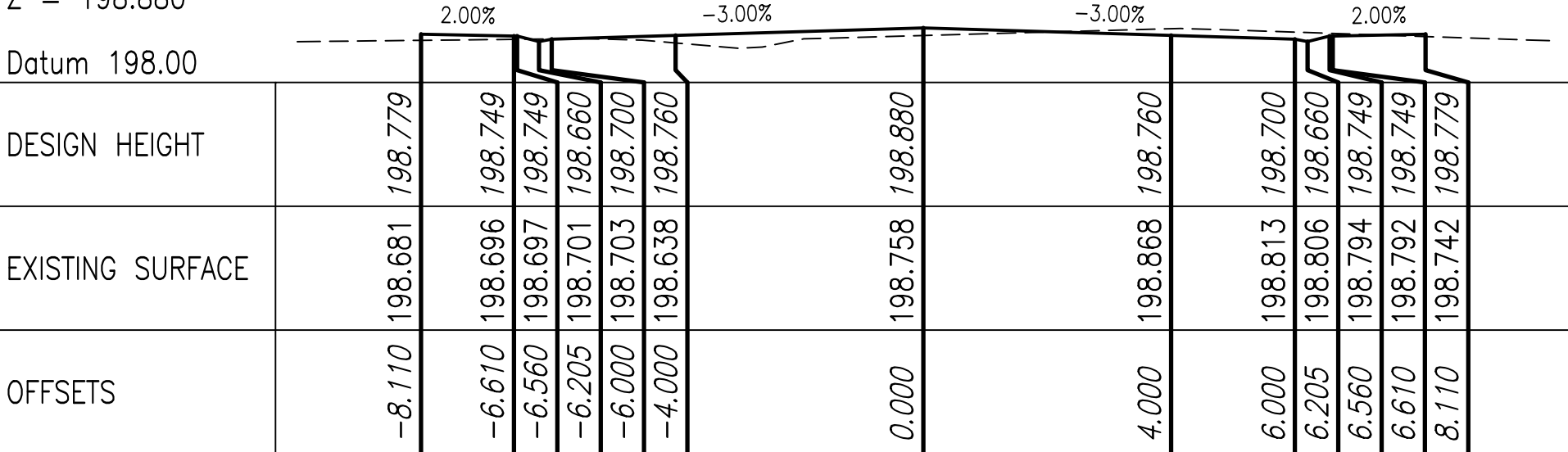
CHAINAGE 60.000

CONTROL LINE MC03
X = 656606.390
Y = 6896621.300
Z = 198.829



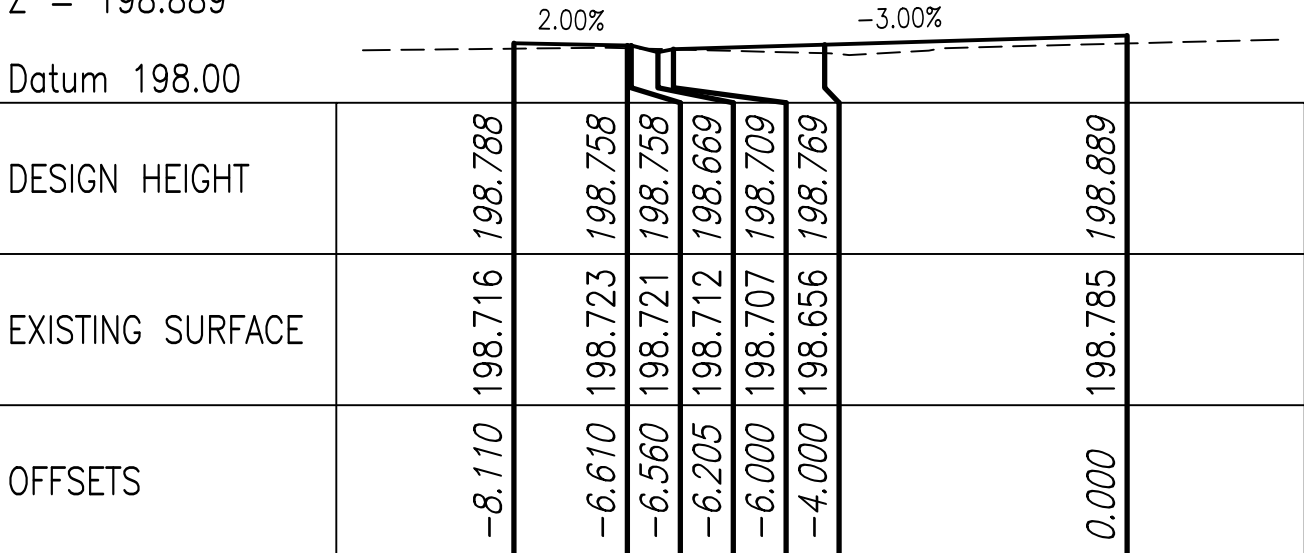
CHAINAGE 40.000

CONTROL LINE MC03
X = 656616.774
Y = 6896638.393
Z = 198.880



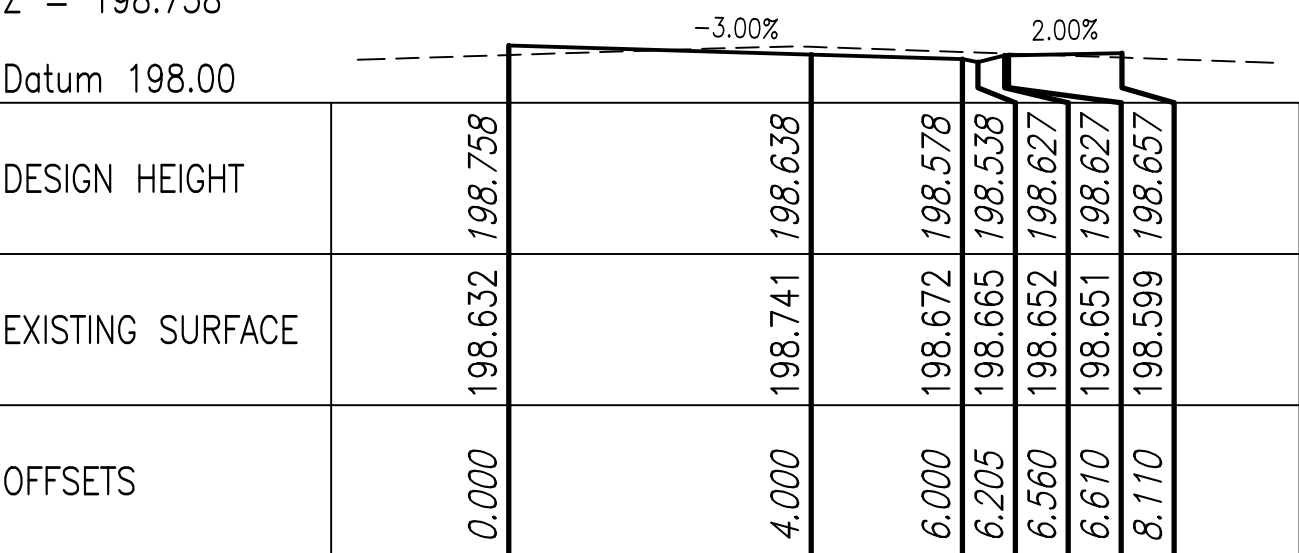
CHAINAGE 20.000

CONTROL LINE MC03
X = 656618.590
Y = 6896641.382
Z = 198.889



CHAINAGE 16.503

CONTROL LINE MC03
X = 656591.698
Y = 6896597.115
Z = 198.758



CHAINAGE 68.298

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Consultant Town Planner

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23	
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23	
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23	
1	BJS	ISSUE FOR TENDER	LOD	01/06/23	
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Datum 198.00

CHAINAGE 60.000

Datum 198.00

CHAINAGE 40.000

Datum 198.00

CHAINAGE 20.000

Datum 198.00

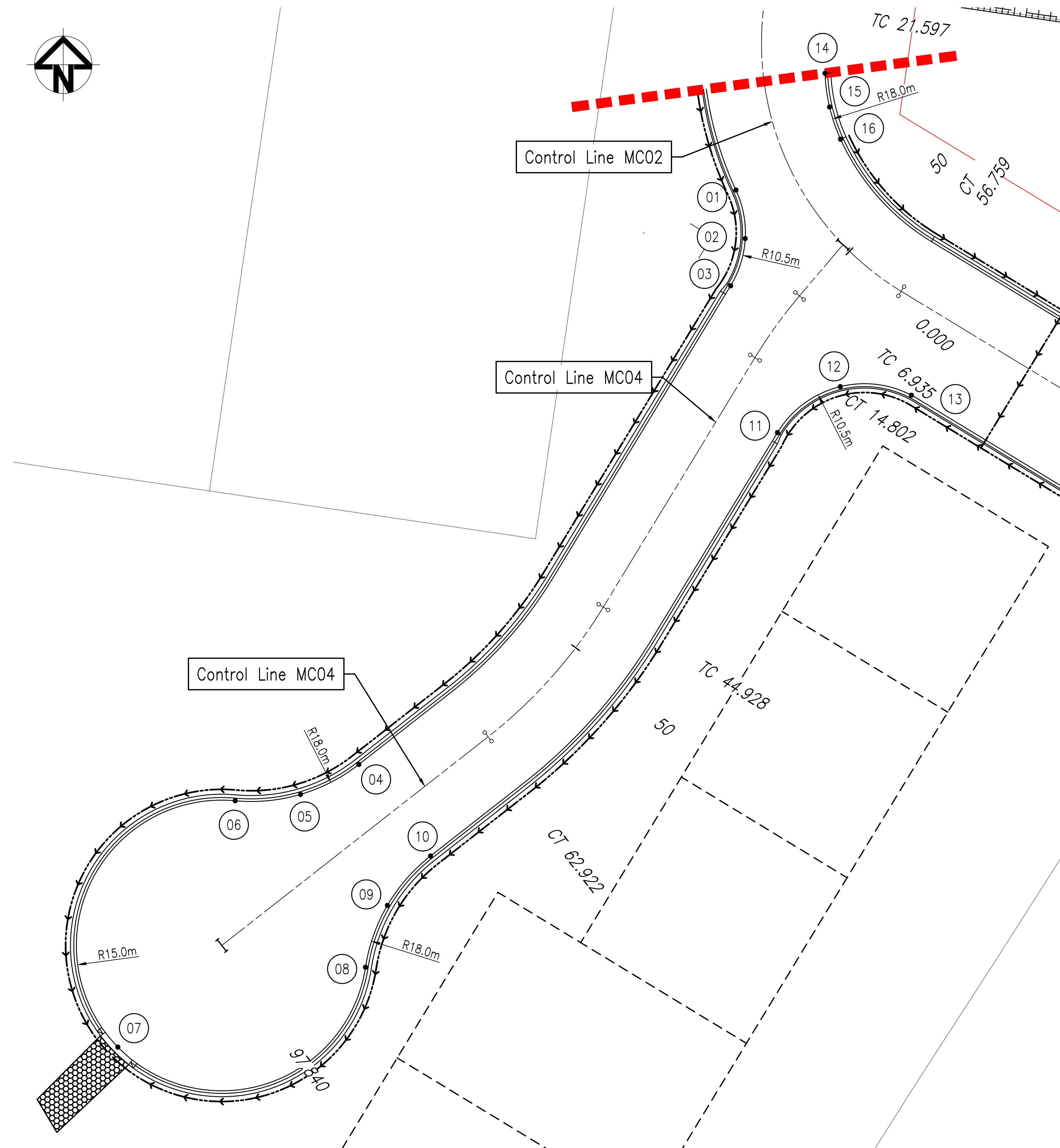
CHAINAGE 10.316

Datum 198.00

CHAINAGE 75.168

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GOONDIWINDI QLD 4390
ABN: 82 626 886 771
TEL: (07) 4671 5701

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ROADWORKS DETAILS
SCALE 1:250

ROADWORKS SETOUT DETAILS		
POINT	EASTING	NORTHING
01	656492.747	6896737.254
02	656493.710	6896732.245
03	656492.199	6896727.372
04	656453.842	6896777.963
05	656447.823	6896674.869
06	656441.087	6896674.216
07	656428.979	6896648.791
08	656454.533	6896657.033
09	656456.786	6896663.414
10	656461.237	6896668.512
11	656497.030	6896712.216
12	656503.514	6896716.963
13	656510.850	6896716.074
14	656501.938	6896749.307
15	656502.406	6896745.824
16	656503.542	6896742.499

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23			<div>Scales: </div> <div>A1 ORIGINAL SIZE</div>	Designed	BJS			ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS DETAILS PLAN SHEET 1 OF 3	Drawing No. 22.1003-028	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23				Drafter	GCC				Client BALONNE SHIRE COUNCIL	Sheet 28 of 42 Revision. 6
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23				Check	LOD					
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23				Approved						
1	BJS	ISSUE FOR TENDER	LOD	01/06/23										
Rev	By	Description	Approved	Date										

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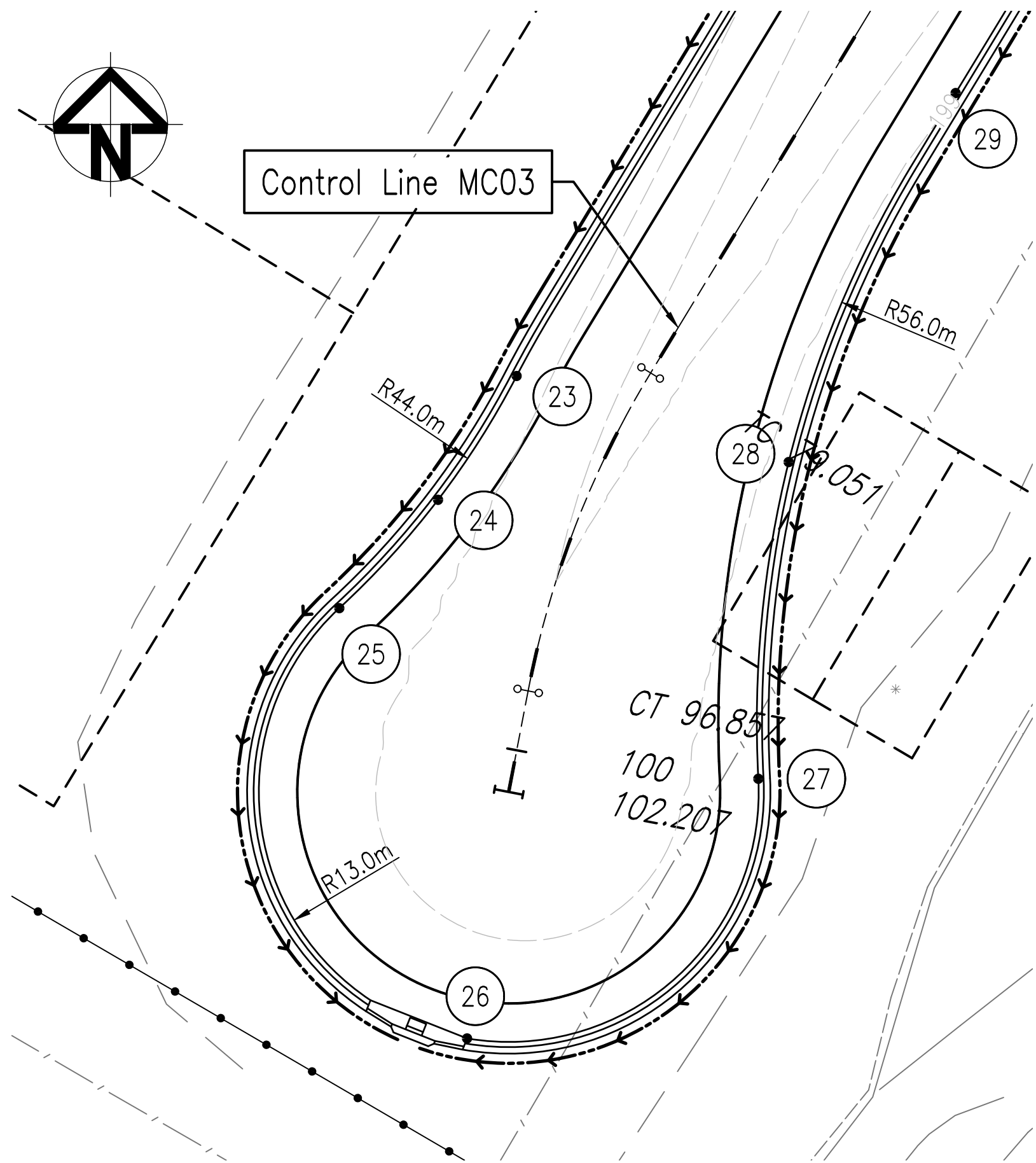
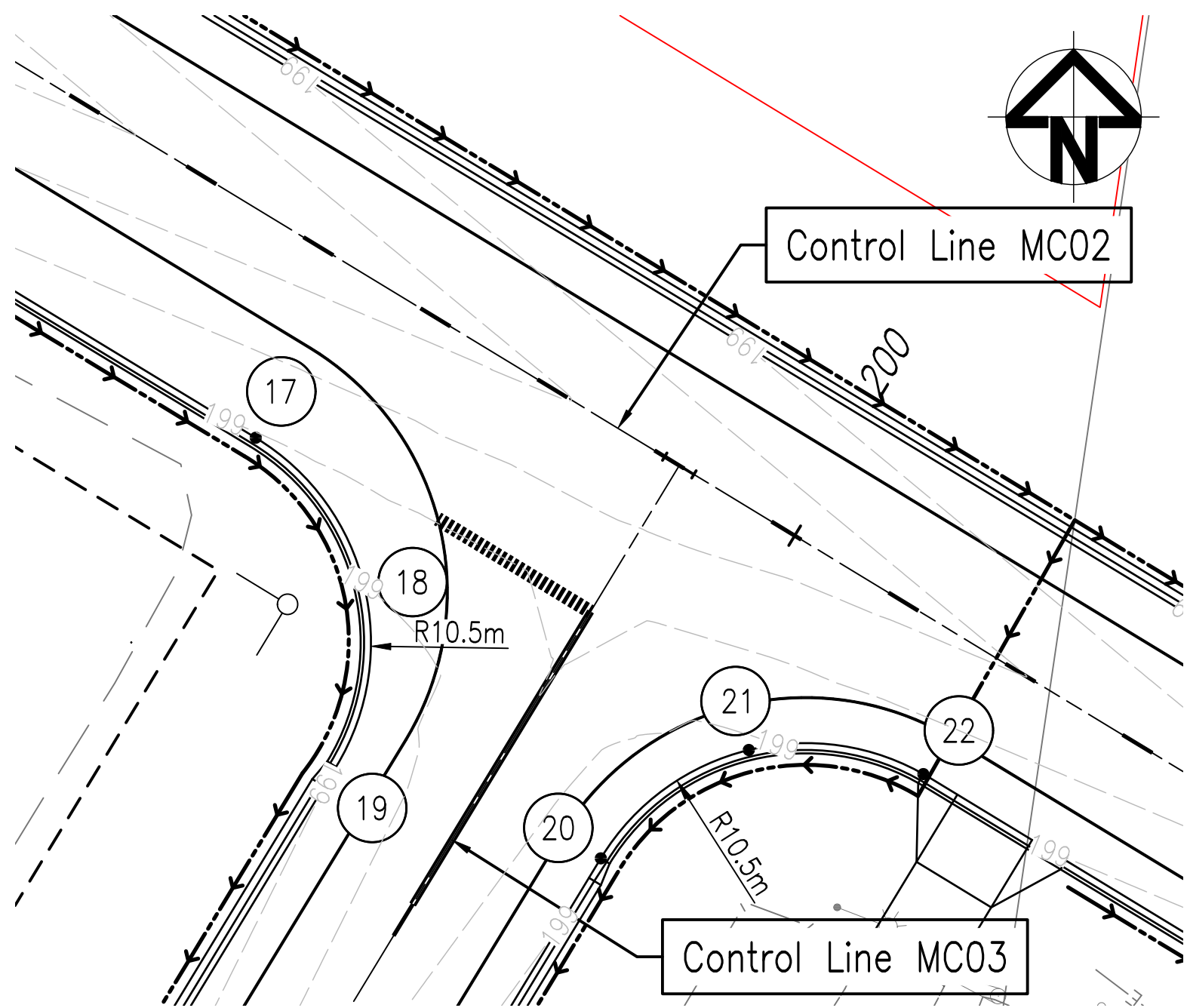
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Consultant Town Planner



ROADWORKS SETOUT DETAILS

POINT	EASTING	NORTHING
17	656608.594	6896656.704
18	656613.342	6896650.221
19	656611.650	6896641.506
20	656623.718	6896638.267
21	656630.202	6896643.016
22	656637.851	6896641.960
23	656579.147	6896587.766
24	656575.061	6896581.330
25	656569.926	6896575.696
26	656576.569	6896553.330
27	656591.714	6896566.828
28	656593.292	6896583.293
29	656601.974	6896602.474

ROADWORKS DETAILS
SCALE 1:250

6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23
5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23
1	BJS	ISSUE FOR TENDER	LOD	01/06/23
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Scales:

0m 2.0 4.0 6.0 8.0 10.0 1:250 AT A1

A1 ORIGINAL SIZE

Designed

BJS

Drafter

GCC

Check

LOD

Approved

REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION

**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
ROADWORKS DETAILS PLAN
SHEET 2 OF 3**

Client
BALONNE SHIRE COUNCIL

Drawing No.

22.1003-029

Sheet 29 of 42

Revision.

6

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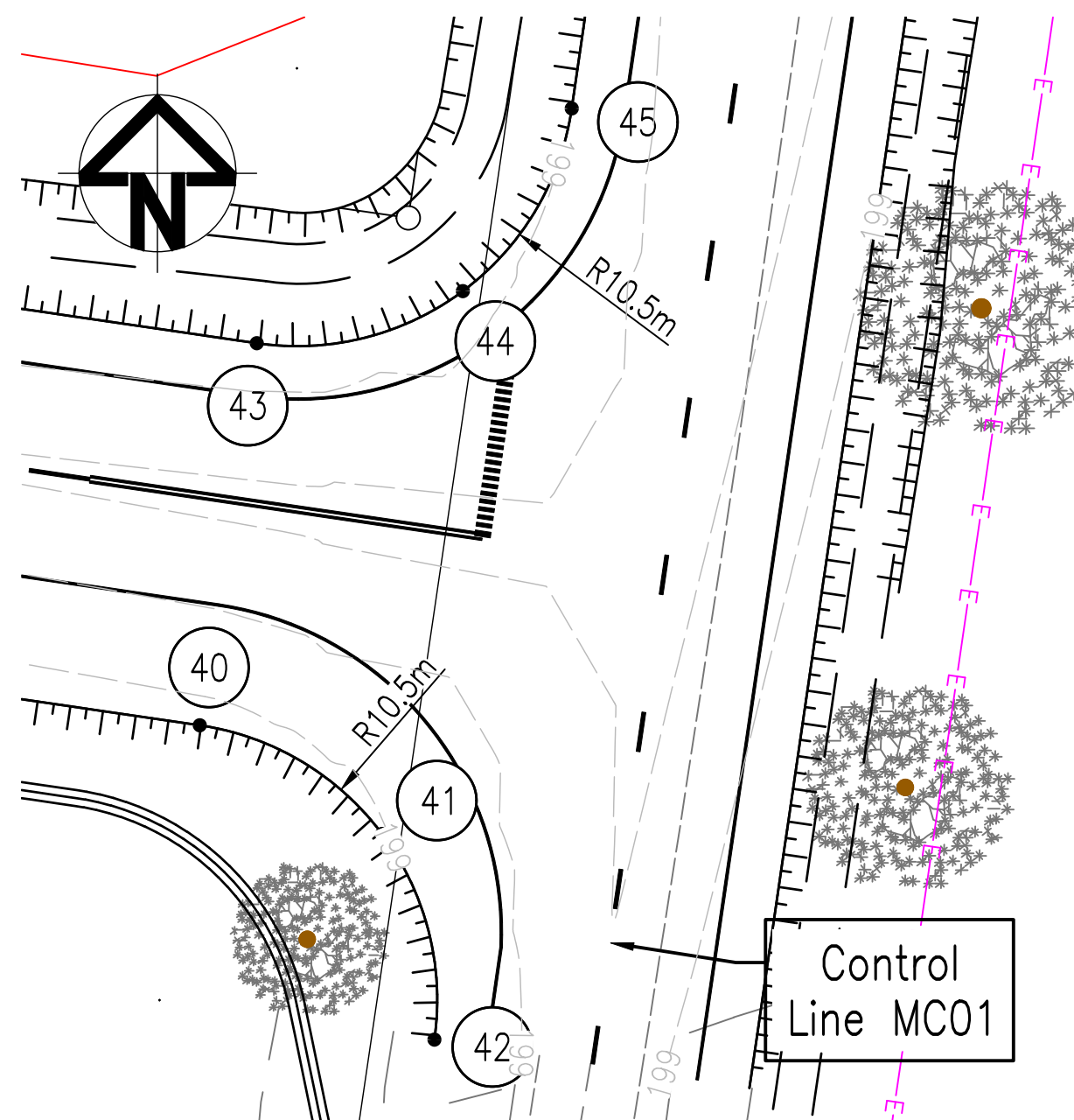
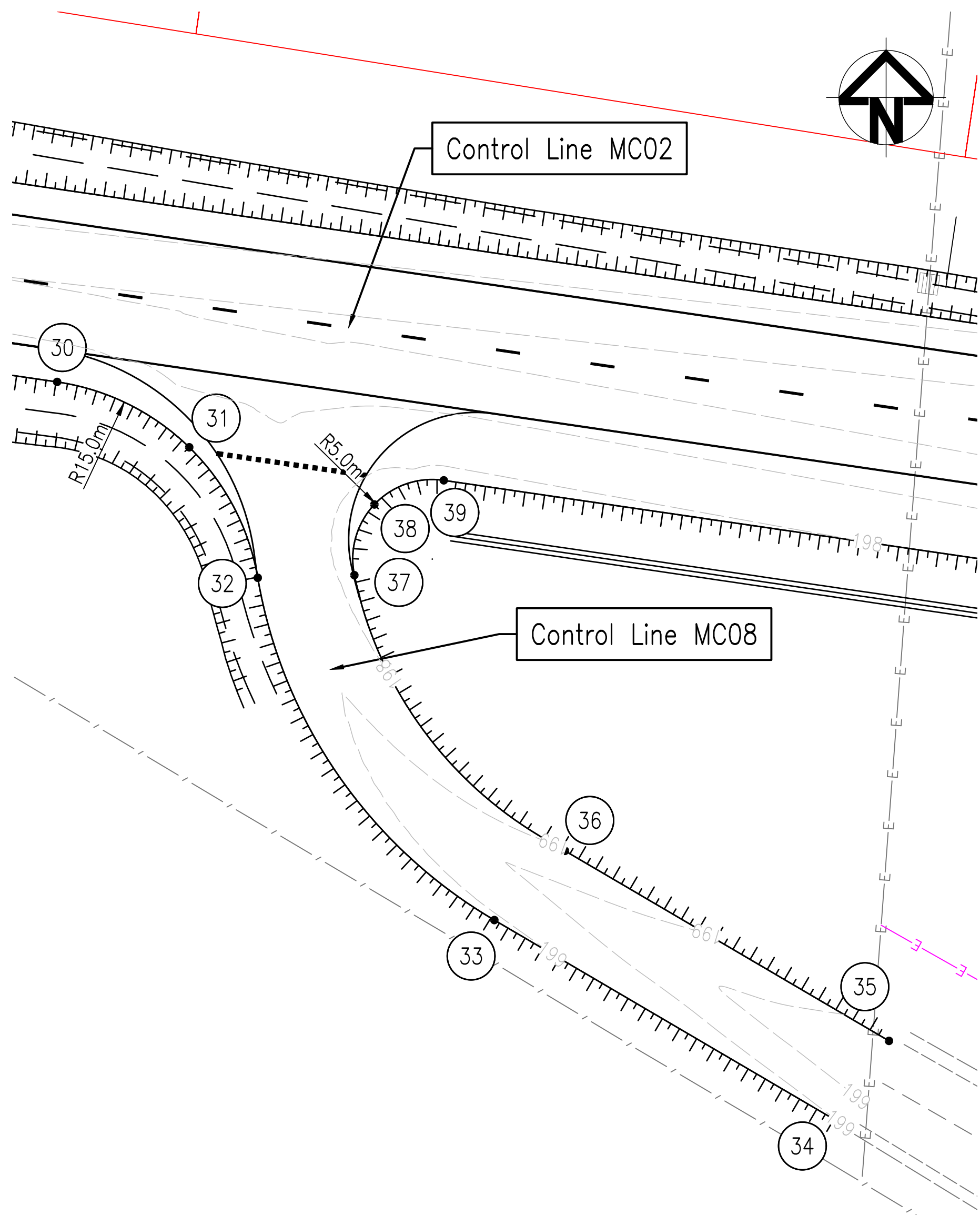
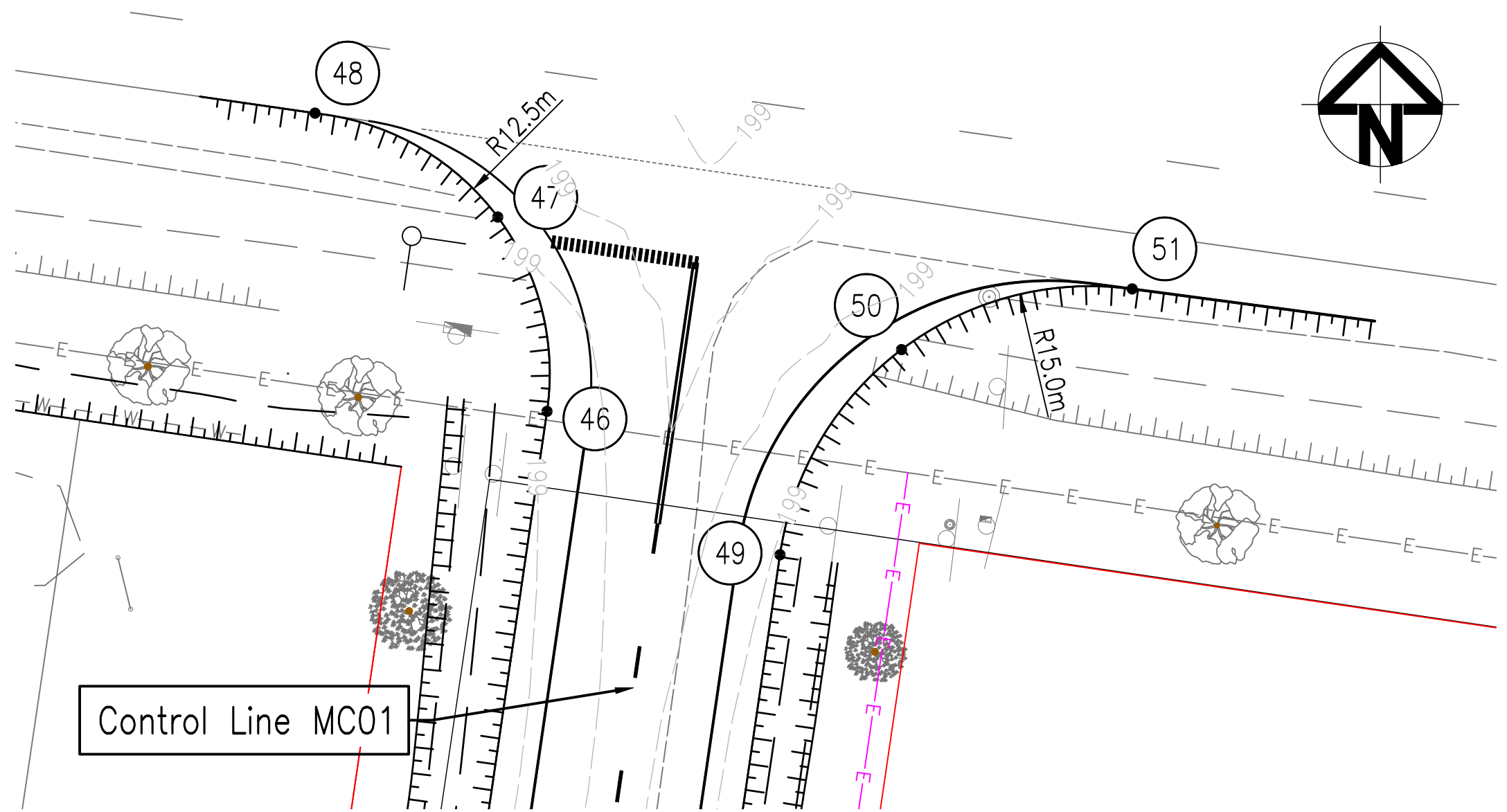
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ROADWORKS SETOUT DETAILS		
POINT	EASTING	NORTHING
30	656796.229	6896562.277
31	656804.534	6896558.169
32	656808.842	6896549.966
33	656823.715	6896528.455
34	656845.489	6896515.685
35	656848.524	6896520.861
36	656828.157	6896532.805
37	656814.916	6896550.131
38	656816.174	6896554.574
39	656820.536	6896556.091
40	656913.402	6896542.498
41	656920.294	6896538.392
42	656922.275	6896530.618
43	656915.557	6896556.938
44	656923.350	6896558.907
45	656927.467	6896565.811
46	656946.577	6896696.712
47	656944.225	6896705.995
48	656935.498	6896710.961
49	656957.704	6896689.860
50	656963.505	6896699.662
51	656974.520	6896702.563

ROADWORKS DETAILS
SCALE 1:250

6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23
5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23
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A1 ORIGINAL SIZE	

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Designed	BJS		
Drafter	GCC		
Check	LOD		
Approved			
ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS DETAILS PLAN SHEET 3 OF 3		Drawing No. 22.1003-030	
Client BALONNE SHIRE COUNCIL		Sheet 30 of 42	
		Revision. 6	

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LOT 10
RP882935

LOT 13
RP850190

LOT 14
RP850190

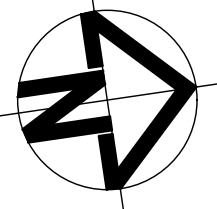
LOT 3
SP132508

LOT 10
RP213570

LOT 9
RP213570

LOT 3
SP133324

LOT 2
SP133324



WATER LEGEND

- W100 DN100 uPVC PN16 POTABLE WATER MAIN
W150 DN150 uPVC PN16 RIVER WATER (NON POTABLE)
FH SV W FIRE HYDRANT, VALVE AND REDUCER
W-W EXISTING WATER MAIN, VALVE AND HYDRANT

SEWER LEGEND

- D-D STORMWATER DRAINAGE LINE
S-S SEWER MAIN AND MANHOLE (MH)
SRM SEWER RISING MAIN
S-O-S EXISTING SEWER MAIN AND MANHOLE

MH.6.57
1
MLO4f
TYPE & INDICATIVE TOP OF MANHOLE
LEVEL MANHOLE NUMBER
LINE NUMBER

WATER NOTES

- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CURRENT WATER SERVICES ASSOCIATION OF AUSTRALIA STANDARDS.
- UNLESS SPECIFIED OTHERWISE ALL MATERIALS AND WORK SHALL COMPLY WITH THE RELEVANT AUSTRALIAN STANDARDS.
- FOR CONSTRUCTION OF PVC WATER RETICULATION SYSTEMS, REFER TO WSA 03-2011-3.1 "PART 2: CONSTRUCTION"
- THE CONSTRUCTION OF WATER SUPPLY RETICULATION WORK SHOWN ON THIS DRAWING MUST BE SUPERVISED BY AN ENGINEER WHO HAS RPEQ REGISTRATION. THE CONTRACTOR SHALL ENSURE THAT ALL RELEVANT SET-OUT PEGS ARE PLACED BY THE SURVEYOR BEFORE COMMENCING CONSTRUCTION.
- ADOPT THE EXISTING ROAD AC SURFACE AS THE PERMANENT LEVEL TO ENSURE MINIMUM COVER.
- COVER ON MAINS FROM PERMANENT LEVEL TO BE AS FOLLOWS:
7.1. DN100 PVC MAIN - MINIMUM 600mm TO FUTURE SURFACE, WHERE POSSIBLE
- PVC PIPES SHALL BE SERIES 2, PN16 TO AS/NZS 1477
- FITTINGS SHALL BE ELASTOMERIC SEAL JOINT TO AS/NZS 1477. U.N.O
- PE AND PVC PIPE DEFLECTIONS AND CURVING TO BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND WSA03-2011.3.1, SECTION 15.2
- SCOURS SHALL DISCHARGE AS DETAILED, FITTING AND PLACEMENT OF CONCRETE TO SCOUR BENDS SHALL BE IN ACCORDANCE WITH WATER SERVICES ASSOCIATION OF AUSTRALIA (WSA 03-2011-3.1)
- VALVES AND HYDRANTS SHALL BE PLACED OPPOSITE REAL PROPERTY PEGS UNLESS NOTED OTHERWISE (TOLERANCE $\pm 0.2m$)
- WATER QUALITY TESTING IS TO BE PERFORMED IN ACCORDANCE WITH WATER SERVICES ASSOCIATION OF AUSTRALIA (WSA 03-2011-3.1)
- ALL PIPEWORK SHALL BE APPROVED BY THE SUPERINTENDENTS REPRESENTATIVE BEFORE BACKFILLING.
- MAINS SHALL NOT BE ACCEPTED 'ON MAINTENANCE' UNTIL THE 'AS CONSTRUCTED' DRAWINGS ARE APPROVED, THE MAINS ARE PASSED BY BALONNE SHIRE COUNCIL.

SEWER NOTES

- ALL WORK TO BE IN ACCORDANCE WITH THE WSAA STANDARD DRAWINGS WHERE APPLICABLE AND THE REQUIREMENTS OF THE BALONNE SHIRE COUNCIL.
- WHERE APPROPRIATE, EXISTING MANHOLE LID LEVELS TO BE RAISED OR LOWERED TO ALIGN WITH ADJACENT FINISHED SURFACE LEVELS AS REQUIRED.
- ALL SEWER PIPES TO BE 150 DIA. (225 DIA. WHERE INDICATED) DWV uPVC S18 RUBBER RING JOINTED TO AS/NZS 1260.
- ALL PROPERTY CONNECTIONS TO BE TERMINATED 500mm ABOVE THE FINISHED SURFACE LEVEL, CLEARLY MARKED AS SEWER AND SEALED WITH A GLUED CAP.
- ALL MANHOLE DROP TYPES TO BE IN ACCORDANCE WITH WSAA STANDARD DRAWINGS.
- ALL WORK ASSOCIATED WITH CONNECTIONS TO LIVE SEWERS WILL BE CARRIED OUT BY BALONNE SHIRE COUNCIL AT THE CONTRACTORS EXPENSE.
- TRENCING FOR SEWERS TO BE CARRIED OUT IN ACCORDANCE WITH BALONNE SHIRE COUNCIL REQUIREMENTS.
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- THE CONTRACTOR MUST MAINTAIN ACCURATE RECORDS OF LEVELS AND LOCATION OF ALL SERVICES TO FULLY COMPLY WITH BALONNE SHIRE COUNCIL REQUIREMENTS FOR AS-CONSTRUCTED INFORMATION AND MUST SUPPLY AS-CONSTRUCTED PLANS TO COUNCIL AT THE COMPLETION OF THE PROJECT.
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- THE CONTRACTOR MUST CHECK ALL INFORMATION AND DIMENSIONS SHOWN ON THESE DRAWINGS, ON SITE, PRIOR TO COMMENCEMENT OF CONSTRUCTION.

JOINS SHEET 2
WATER AND SEWERAGE LAYOUT
SCALE 1:500

6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23
5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23
1	BJS	ISSUE FOR TENDER	LOD	01/06/23
Rev	By	Description	Approved	Date

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Scales:

0m 5.0 10 15 20 1:500 AT A1

A1 ORIGINAL SIZE

Designed	BJS		
Drafter	GCC		
Check	LOD		
Approved			

REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION

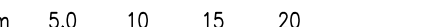
**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
WATER & SEWERAGE PLAN
SHEET 1 OF 3**

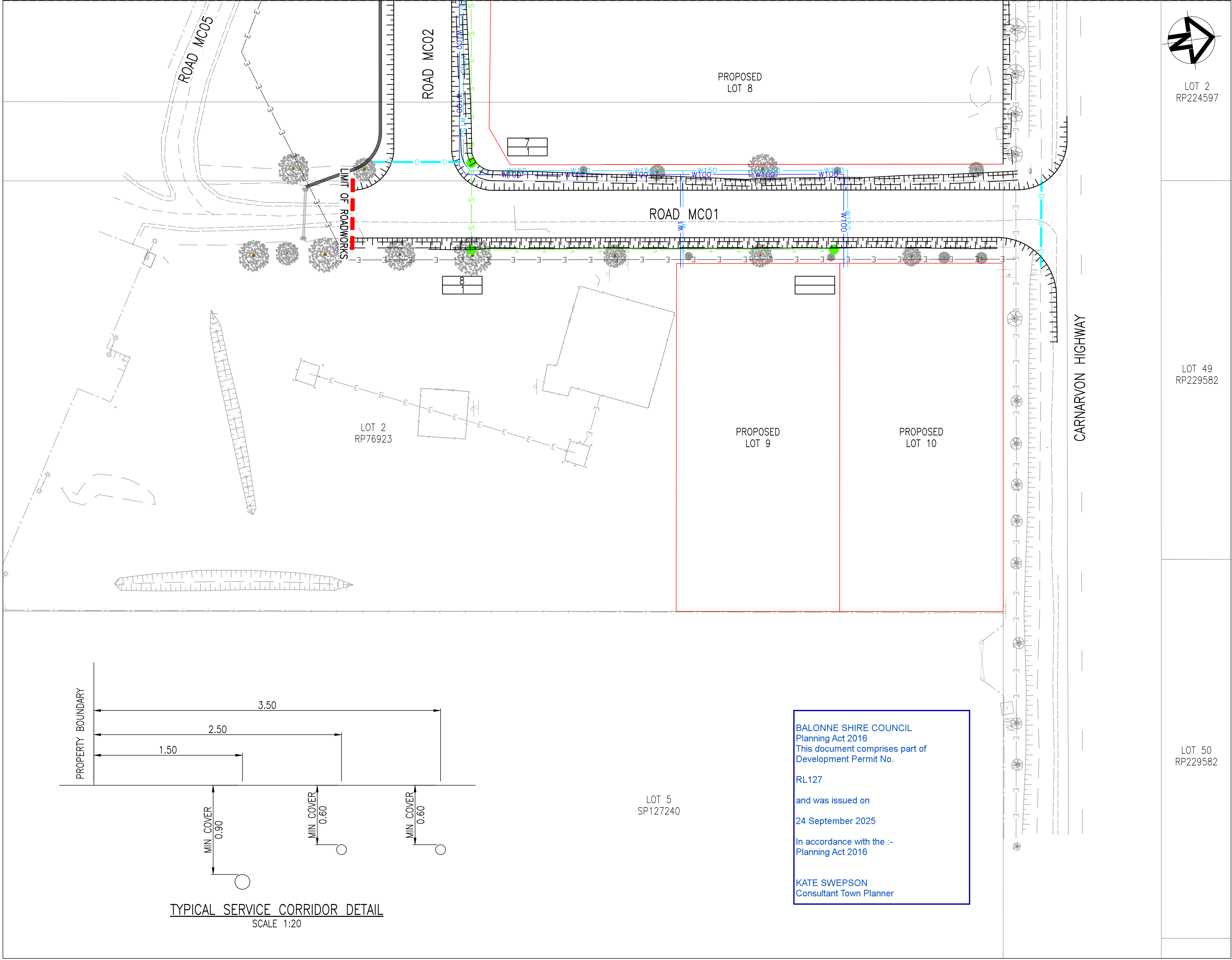
Client
BALONNE SHIRE COUNCIL

Drawing No.
22.1003-034

Sheet 34 of 42

Revision.
6

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<div>Scales:</div> <div></div>		Designed	BJS		<div>ST GEORGE AIRPORT</div> <div>INDUSTRIAL SUBDIVISION</div> <div>WATER & SEWERAGE PLAN</div> <div>SHEET 2 OF 3</div>	Drawing No.	22.1003-035
		Drafter	GCC				
		Check	LOD				Sheet 35 of 42
		Approved					Client
A1 ORIGINAL SIZE							



LOT 2
RP224597

LOT 49
RP229582

LOT 50
RP229582

WATER LEGEND

- W100 DN100 uPVC PN16 POTABLE WATER MAIN
- W150 DN150 uPVC PN16 RIVER WATER (NON POTABLE)
- FH SV W Fire Hydrant, Valve and Reducer
- W Existing Water Main, Valve and Hydrant

SEWER LEGEND

- D Stormwater Drainage Line
- S Sewer Main and Manhole (MH)
- SRM Sewer Rising Main
- S Existing Sewer Main and Manhole

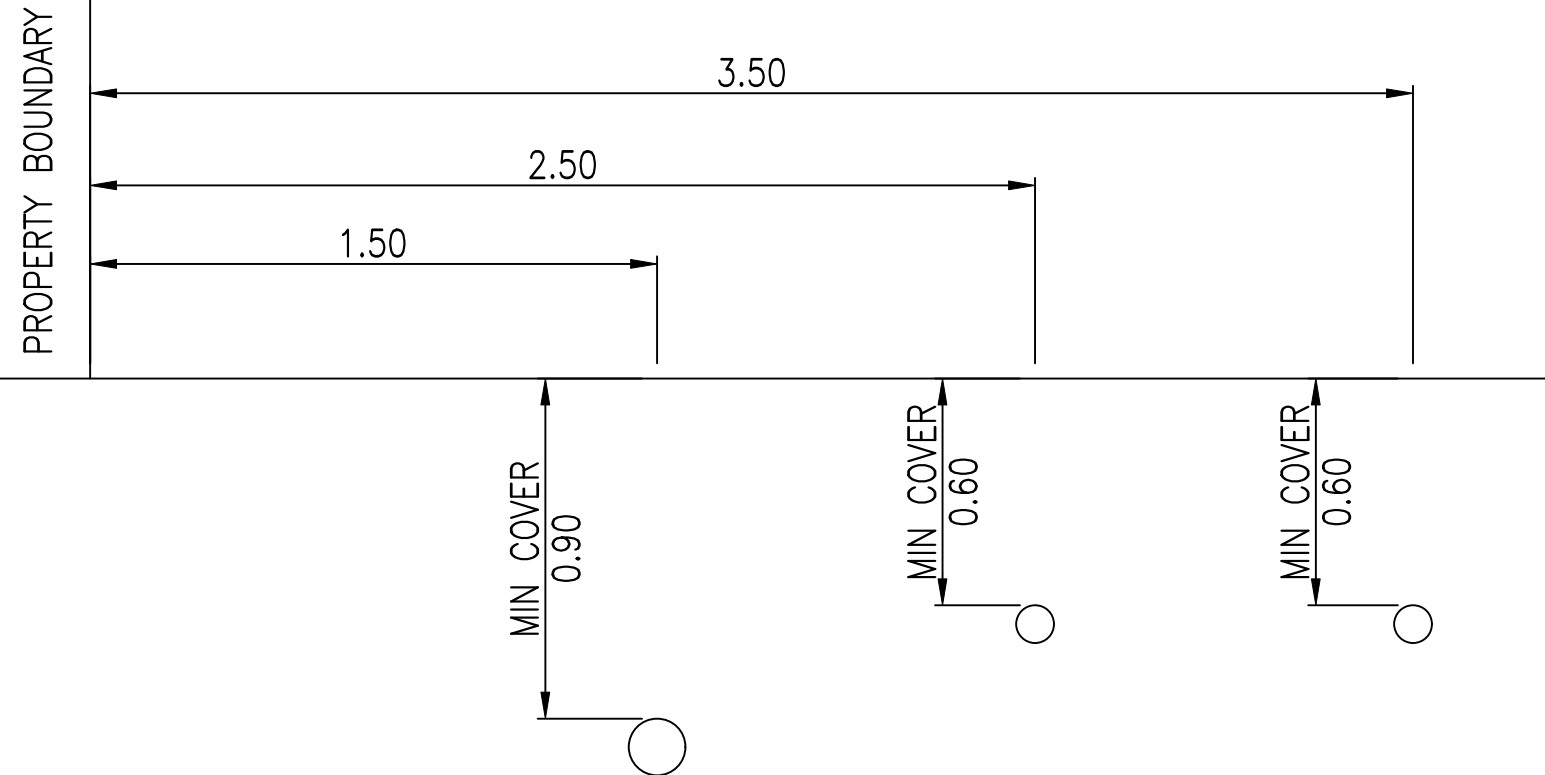


WATER NOTES

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TYPICAL SERVICE CORRIDOR DETAIL
SCALE 1:20

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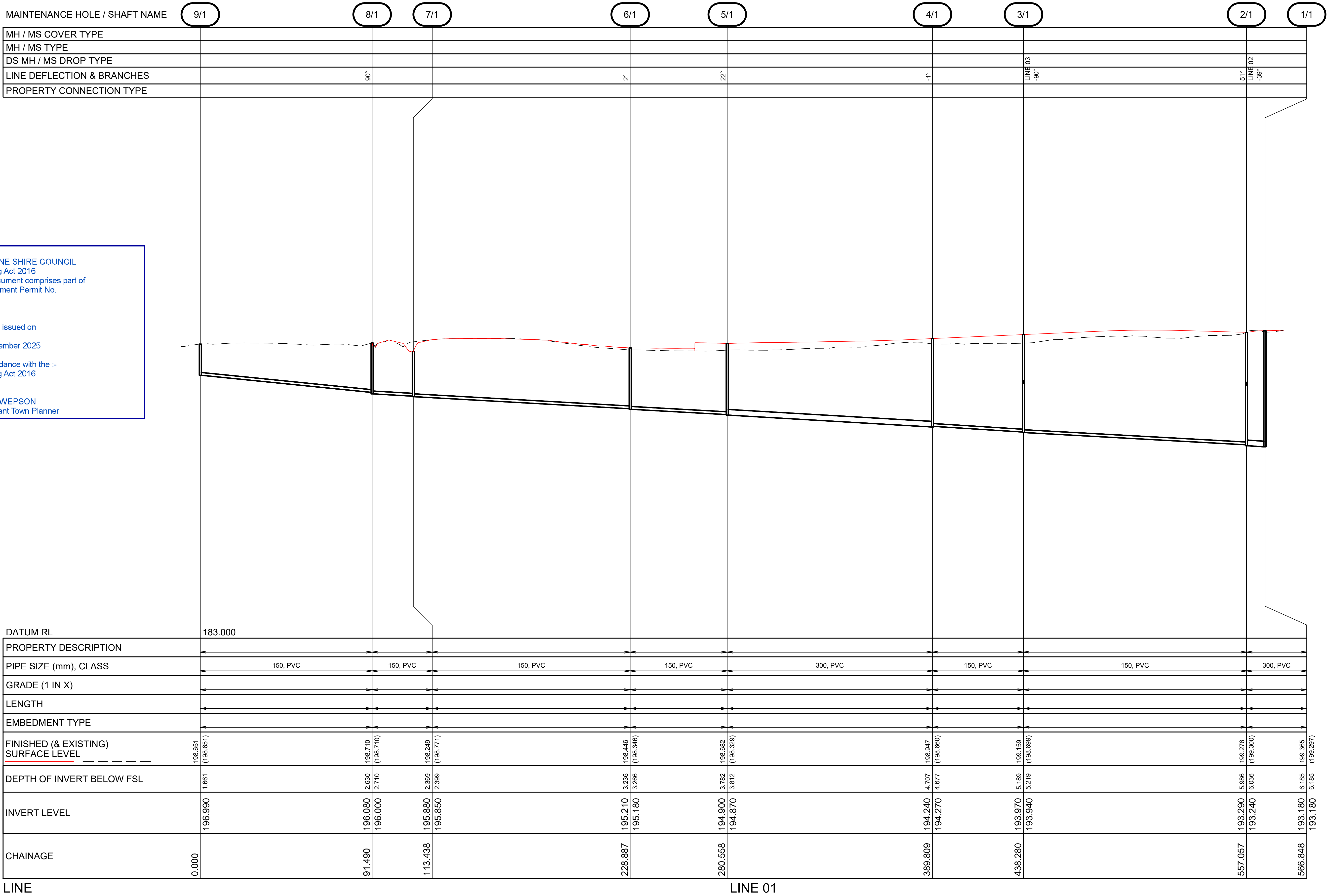
24 September 2025

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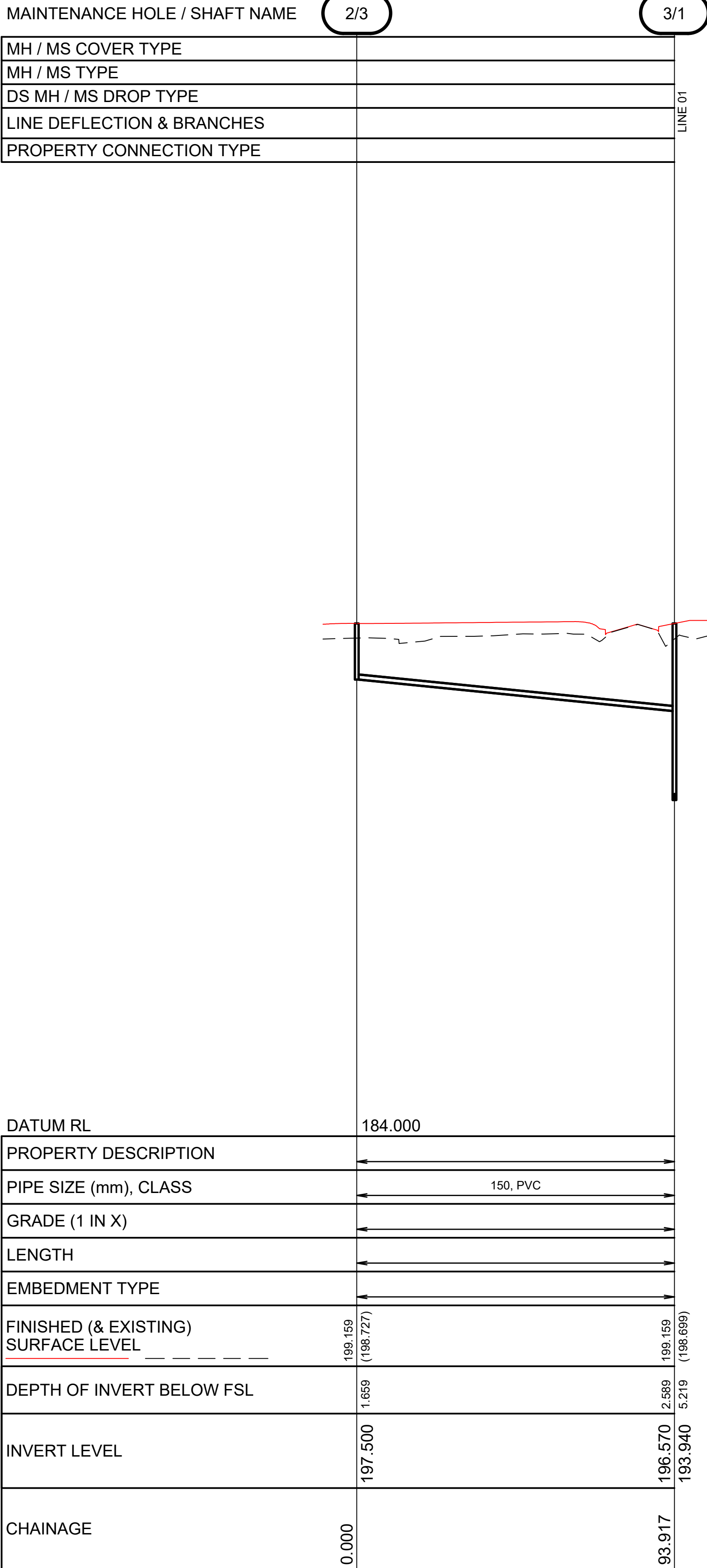
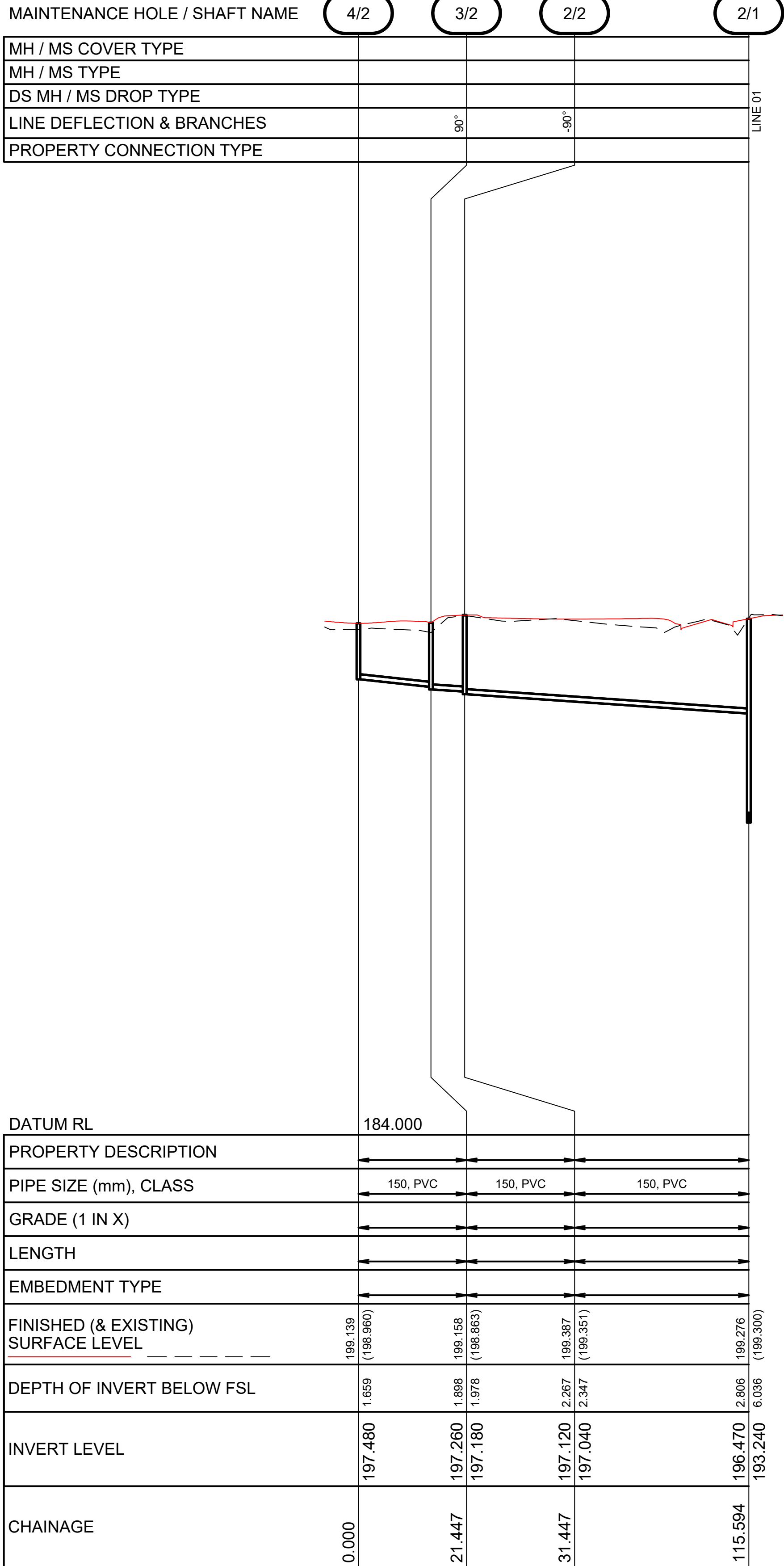
KATE SWEPSON
Consultant Town Planner

WATER AND SEWERAGE LAYOUT
SCALE 1:500

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5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23	<div><div>PROTERRA GROUP 31 GLASSER STREET GOONDIWINDI QLD 4390 ABN: 82 626 886 771 TEL: (07) 4671 5701 WWW.PROTERRAGROUP.COM.AU</div></div>	<div>0m5.01015201:500 AT A1</div> <div>A1 ORIGINAL SIZE</div>	Scales:		Designed	BJS			<div>ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION WATER & SEWERAGE PLAN SHEET 3 OF 3</div> <div>Client BALONNE SHIRE COUNCIL</div>	Drawing No. 22.1003-036	
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23			Drafter	GCC			Sheet 36 of 42				
3	BJS	AMENDMENT 2 TENDER SET	LOD	29/07/23			Check	LOD							
2	BJS	AMENDMENT 1 TENDER SET	LOD	01/07/23			Approved				Revision. 6				
1	BJS	ISSUE FOR TENDER	LOD	01/06/23											
Rev	By	Description	Approved	Date											



SEWERAGE LONGITUDINAL SECTION
SCALE 1:1000 HORIZONTAL, SCALE 1:100 VERTICAL



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SEWERAGE LONGITUDINAL SECTION
SCALE 1:1000 HORIZONTAL, SCALE 1:100 VERTICAL

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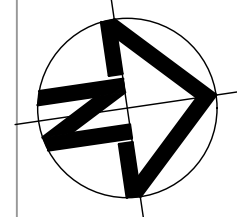
RL127

and was issued on

24 September 2025

In accordance with the :-
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Consultant Town Planner



LOT 3
SP132508

SESC LEGEND

- 6.75 DESIGN SURFACE CONTOURS
- (SF) SEDIMENT FENCE (SF)
- GEO-LOG
- 1.0m WIDE TURF STRIP BEHIND KERB & CHANNEL. PROVIDE RETURNS AT 30.0m CENTRES
- DIRECTION OF OVERLAND FLOW PATH
- CHECK DAM
- HYDROMULCH
- SAND BAG
- BATTER MARK

SESC NOTES

1. AVOID STRIPPING & EXCAVATION OF ALL AREAS UNTIL NECESSARY
2. MINIMIZE STRIPPING & EXCAVATION WHERE POSSIBLE TO LEAVE THE LEAST AMOUNT OF EXPOSED/DISTURBED SOIL AT ANY ONE TIME.
3. PROTECT ALL STORMWATER PITS & INLETS FROM SEDIMENT BY ERECTING SEDIMENT BARRIERS OR SAND BAGS AS REQUIRED.
4. THE CONTRACTOR SHALL MONITOR THE INSTALLED EROSION & SEDIMENT CONTROL SYSTEM DURING CONSTRUCTION & REPAIR/MODIFY IT AS REQUIRED TO MAINTAIN IT IN GOOD WORKING ORDER DURING ALL STAGES OF CONSTRUCTION UNTIL THE SITE HAS BEEN STABILISED/REVEGETATED.
5. ALL SERVICE TRENCHES TO BE BACKFILLED & COMPACTED AS REQUIRED & LEFT RAISED 100MM ABOVE THE SURROUNDING SURFACE LEVEL TO PREVENT DEPRESSION FORMING.
6. STOCKPILED MATERIALS SHALL BE KEPT WITHIN THE STOCKPILE AREA ABOVE THE STRAW BALES/SILT FENCE AS SHOWN ON THE PLANS.
7. ALL HARD WASTE STORED ON SITE SHALL BE CONFINED WITHIN A 3-SIDED SILT FENCE BARRIER CONSTRUCTED TO PREVENT MATERIAL FROM BEING REMOVED FROM SITE BY WIND OF FLOWING WATER.
8. GRASSED FILTER STRIPS & BIODEGRADABLE EROSION CONTROL MATS SHOULD BE USED DURING CONSTRUCTION TO STABILISE HIGH RISE AREAS.
9. CUTTING OF SILT PRODUCING MATERIALS (BRICK, TILES ETC.) & WASHING OF TOOLS & PAINTING EQUIPMENT SHALL BE CONFINED TO THE AREA SHOWING ON THE EROSION & SEDIMENT CONTROL LAYOUT.
10. PREPARE AREAS OF DISPERSIBLE SOIL BY COVERING WITH MINIMUM 100mm LAYER OF NON-DISPERSIBLE SOIL & TURF / MULCH AS REQUIRED TO PREVENT EROSION.
11. REVEGETATE DISTURBED AREAS AS SOON AS POSSIBLE AFTER EXCAVATION.
12. ALL EROSION & SEDIMENT CONTROL MEASURES TO BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH LOCAL COUNCIL'S EROSION AND SEDIMENT CONTROL REQUIREMENTS AND THE IECA STANDARDS.
13. A VEHICLE WASH-DOWN PAD SHALL BE PROVIDED AT THE SITE EXIT IN ACCORDANCE WITH LOCAL COUNCIL REQUIREMENTS.
14. ALL VEHICLES MUST BE CLEANED DOWN BEFORE LEAVING THE SITE. DURING CONSTRUCTION, ALL MATERIAL TRACKED ONTO THE ROAD SURFACE BY VEHICLES LEAVING THE SITE, SHALL BE BROOMED UP AND REMOVED, OR RELOCATED ON TO THE SITE. - NOT MATERIAL SHALL BE WASHED INTO THE STORMWATER SYSTEM.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF SILT MANAGEMENT FACILITIES FROM THE TIME OF COMMENCEMENT OF CONSTRUCTION UNTIL THE SUBDIVISION HAS BEEN RELEASED 'OFF MAINTENANCE' BY COUNCIL.

SITWORKS NOTES

1. ANY FILL REQUIRED TO UNDERSIDE OF SLAB TO BE CBR 15, COMPACTED IN 150MM LAYERS TO 98% STANDARD DENSITY AS DEFINED BY AS 1289.5.1.1.
2. STRIP BACK TOP SOIL AND ORGANIC MATTER BEFORE PLACING FILL, PROOF ROLL FOR SOFT SPOTS AND FILL SOFT SPOTS AS ABOVE.
3. FOOTINGS TO BEAR ON STABLE SOIL WITH MINIMUM BEARING CAPACITY = 100kPA

SOIL EROSION AND SEDIMENT CONTROL LAYOUT

SCALE 1:500

6	GCC	PUMP STATION DETAILS ADDED	LOD	16/10/23
5	CDH	ISSUE FOR 100% REVIEW	LOD	26/09/23
4	BJS	AMENDMENT 3 TENDER SET	LOD	24/08/23
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Rev	By	Description	Approved	Date

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Scales:

0m 5.0 10 15 20 1:500 AT A1

A1 ORIGINAL SIZE

REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION

Designed	BJS		
Drafter	GCC		
Check	LOD		
Approved			

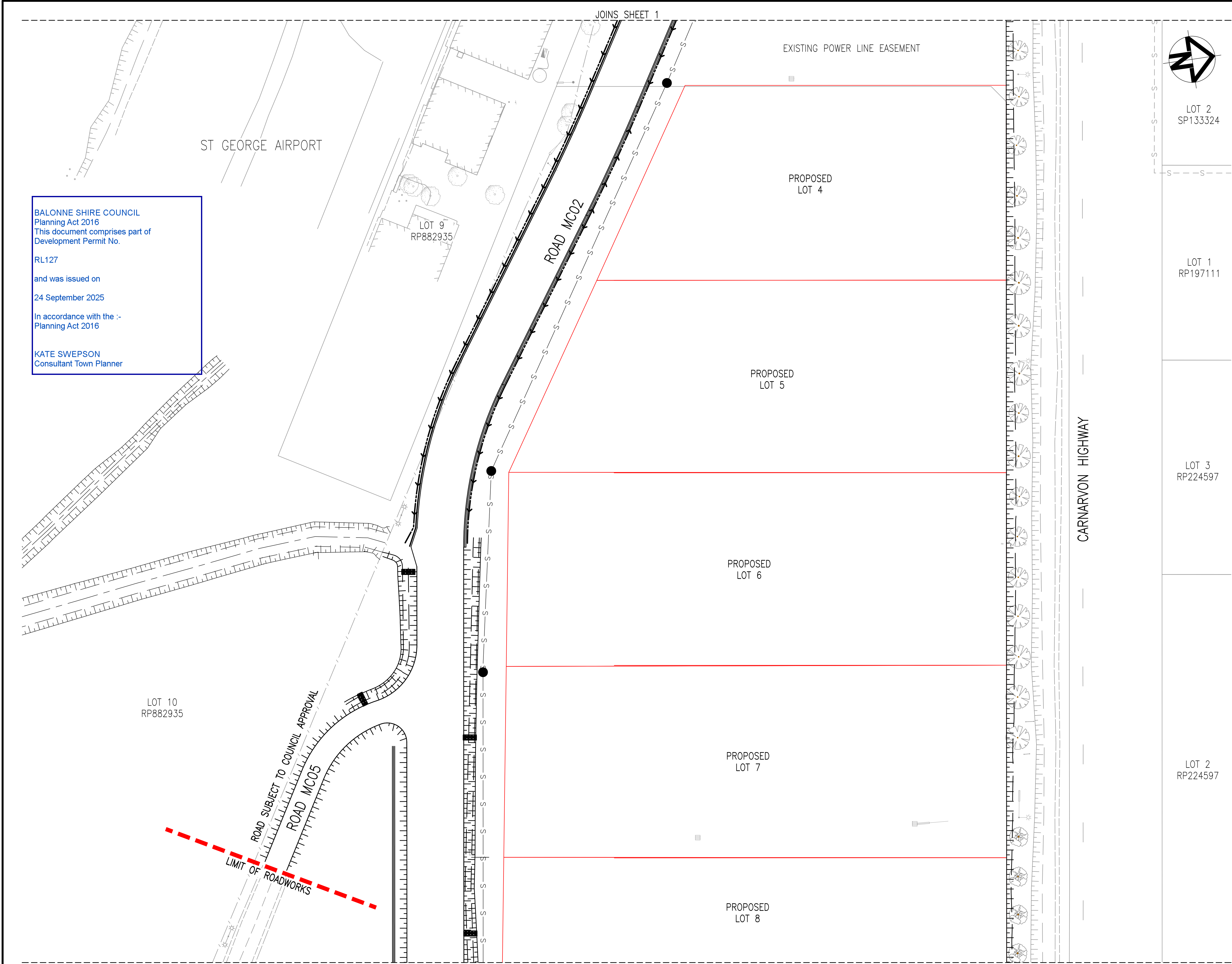
**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
EROSION & SEDIMENT CONTROL
SHEET 1 OF 3**

Client
BALONNE SHIRE COUNCIL

Drawing No.
22.1003-040

Sheet 40 of 42

Revision.
6



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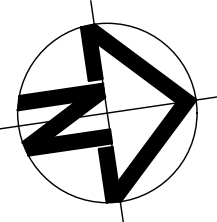
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KATE SWEPSON
Consultant Town Planner



LOT 2
SP133324

LOT 1
RP197111

LOT 3
RP224597

LOT 2
RP224597

SESC LEGEND

- 6.75 DESIGN SURFACE CONTOURS
- (SF) SEDIMENT FENCE (SF)
- GEO-LOG
- 1.0m WIDE TURF STRIP BEHIND KERB & CHANNEL. PROVIDE RETURNS AT 30.0m CENTRES
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- CHECK DAM
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- SAND BAG
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SESC NOTES

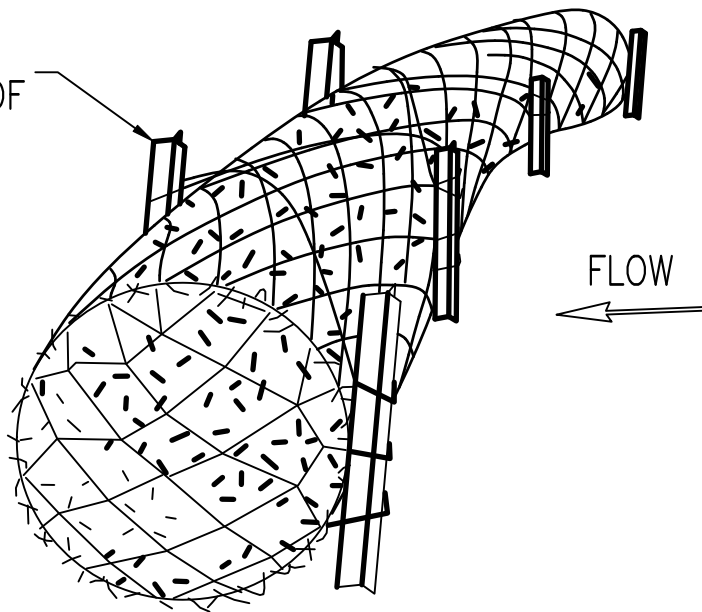
- FOR SESC GENERAL NOTES, REFER TO DRG. NO 22.1003-032.

NOTE
ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURE MAY BE
REQUIRED DURING THE COURSE OF CONSTRUCTION. THE CONTRACTOR
SHALL IDENTIFY POTENTIAL PROBLEM AREAS AND TREAT THEM IN
ACCORDANCE WITH THE IECA (AUST.) AND BSC REQUIREMENTS.

SANDBAGS ARE TO BE INSTALLED ACROSS THE
EXISTING ROAD PRIOR TO RAIN EVENT AND/OR
AT THE END OF EACH WORKING DAY

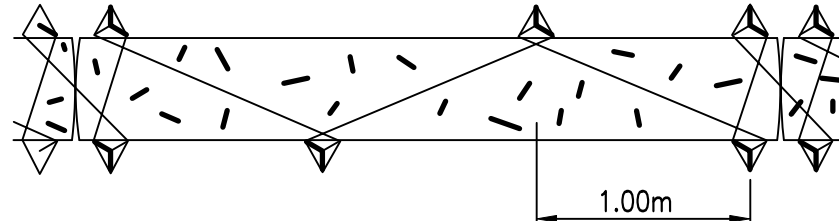
ALL DISTURBED AREAS OF SITE
INCLUDING ALLOTMENT FILL AREAS TO
BE TOPSOILED AND GRASS SEED

PROVIDE A MINIMUM OF
FOUR STAKES ATTACHED
TO DOWNSTREAM SIDE OF
GEO-LOG



TYPICAL ANCHORAGE OF GEO-LOG

Geo Log to be installed in accordance with
IECA Sediment and Erosion Control Design Fact
Sheets - Instream Practices



STAKING METHOD

GEO-LOG DETAIL
Not To Scale

SOIL EROSION AND SEDIMENT CONTROL LAYOUT
SCALE 1:500

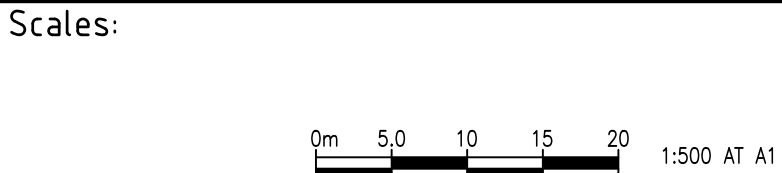
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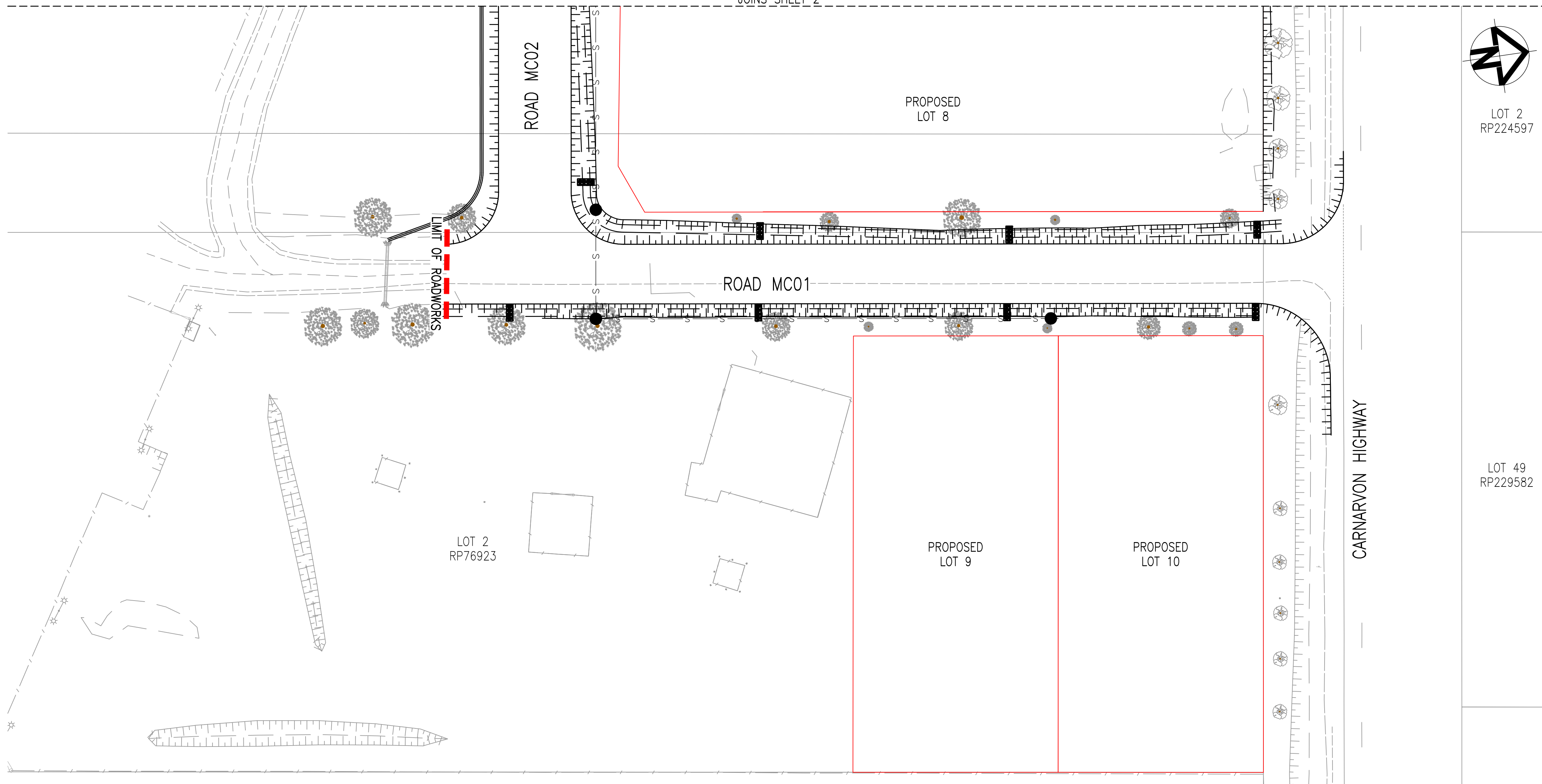
Designed
Drafter
Check
Approved

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**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
EROSION & SEDIMENT CONTROL
SHEET 2 OF 3**

Client
BALONNE SHIRE COUNCIL

Drawing No.
22.1003-041
Sheet 41 of 42
Revision.



GENERAL LAYOUT
SCALE 1:1000

SESC LEGEND

- 6.75 DESIGN SURFACE CONTOURS
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- SAND BAG
- BATTER MARK

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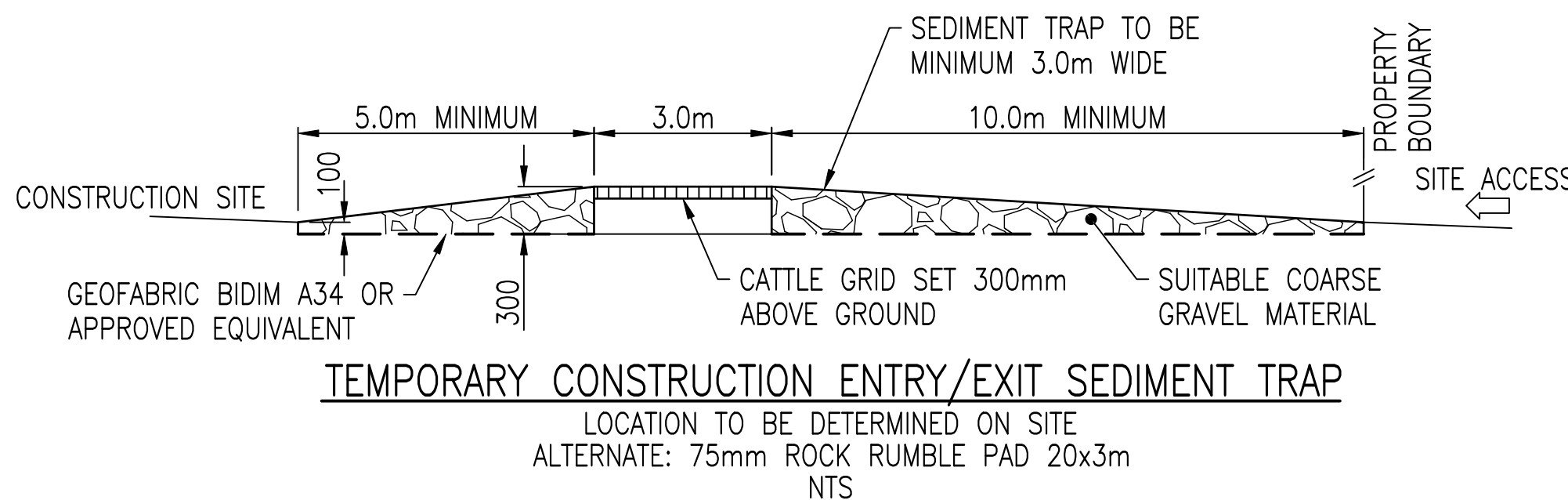
RL127

and was issued on

24 September 2025

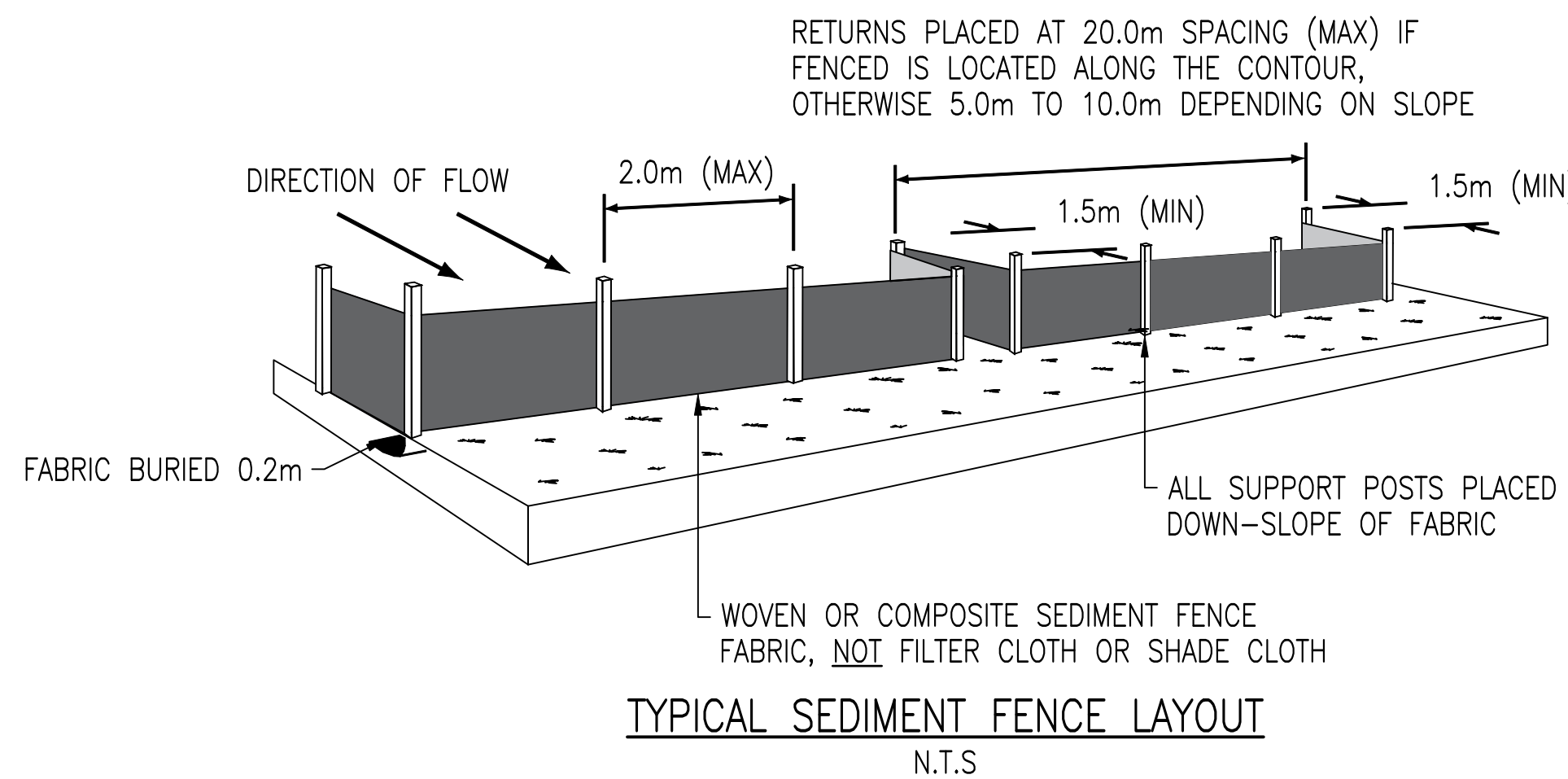
In accordance with the :-
Planning Act 2016

KATE SWEPSON
Consultant Town Planner



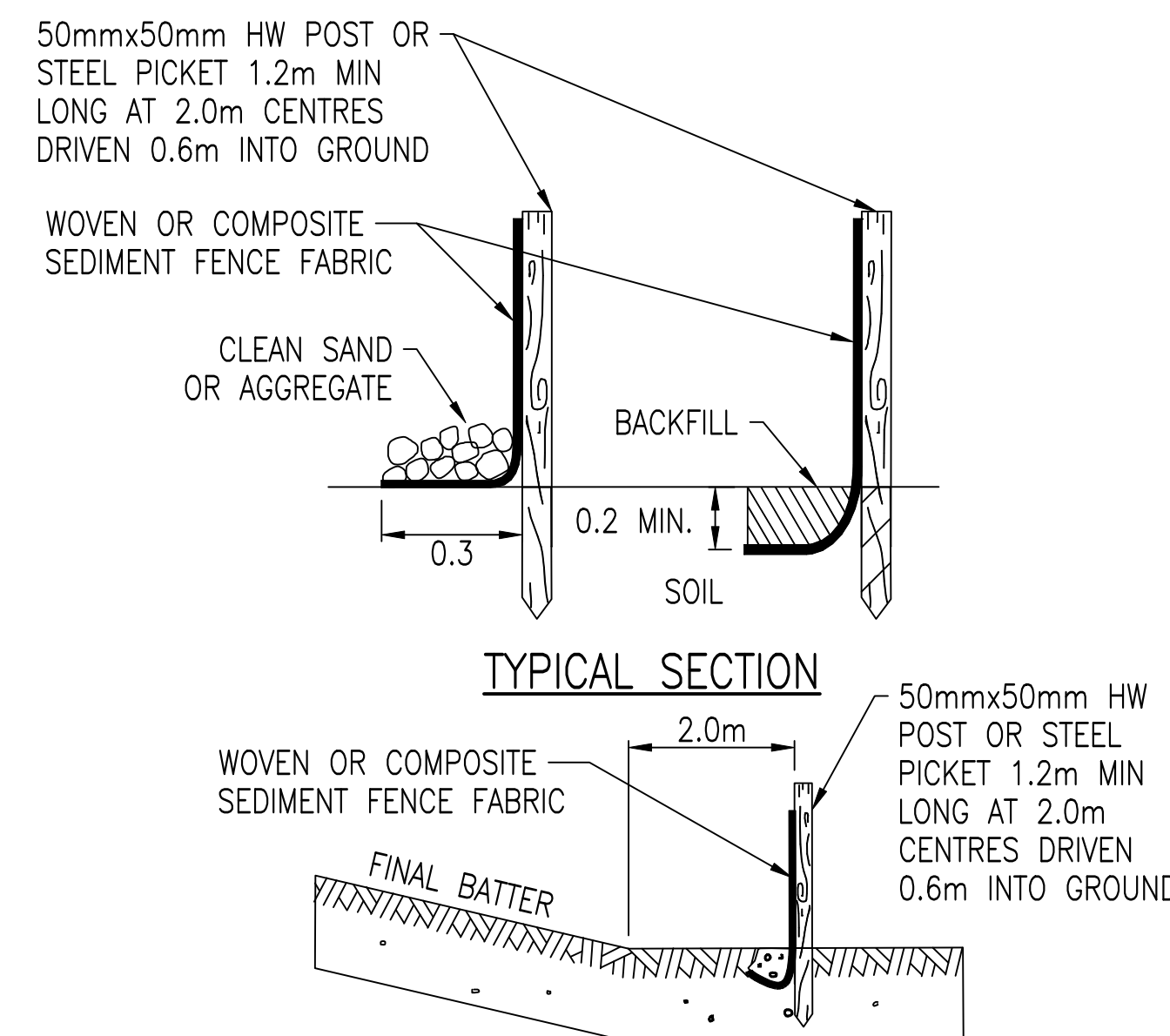
TEMPORARY CONSTRUCTION ENTRY/EXIT SEDIMENT TRAP

LOCATION TO BE DETERMINED ON SITE
ALTERNATE: 75mm ROCK RUMBLE PAD 20x3m
NTS



TYPICAL SEDIMENT FENCE LAYOUT

N.T.S



TYPICAL SECTION

LOCATION OF FENCE RELATIVE
TO BASE OF SLOPE

Rev	By	Description	Approved	Date
0	0		reg	
1	0	PRELIMINARY ISSUE	0	0

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DO NOT SCALE FROM THIS DRAWING. USE DIMENSIONS WHERE MARKED

Scales:

0m 5.0 10 15 20 1:500 AT A1

A1 ORIGINAL SIZE

Designed

GCC

Drafter

GCC

Check

GCC

Approved

REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION

ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
EROSION & SEDIMENT CONTROL
SHEET 3 OF 3

Client
BALONNE SHIRE COUNCIL

Drawing No.

22.1003-042

Sheet 42 of 42

Revision.

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