

Our Ref: RL127

24 September 2025

Balonne Shire Council
193-199 Grey Street
ST GEORGE QLD 4487

Attention: Sharyn Arnold
By email: sharyn.arnold@balonne.qld.gov.au

Dear Sharyn,

Decision notice approval

(Given under section 63(2) of the *Planning Act 2016*)

The development application described below was properly made to the Balonne Shire Council on 2 September 2024.

Applicant details

| | |
|-----------------------------------|---|
| Applicant name: | Balonne Shire Council |
| Applicant contact details: | 193-199 Grey Street, St George QLD 4487 Email: sharyn.arnold@balonne.qld.gov.au |

Location details

| | |
|-----------------------------------|---|
| Street address: | 130-152 Grey Street, Carnarvon Highway and 54 Commissioners Point Road, St George |
| Real property description: | Lot 10 on RP882935, Lot 1 on SP340406, Lot 1 on RP55376 and Lots 1 & 2 on RP55377 |
| Local government area: | Balonne Shire Council |

Application details

| | |
|---|---|
| Application number: | RL127 |
| Approval sought: | Development Permit |
| Description of the development proposed: | Reconfiguring a Lot – Five (5) lots into thirteen (13) lots and dividing land into parts by agreement |
| Category of assessment: | Code Assessment |
| Planning scheme: | <i>Balonne Shire Planning Scheme 2024</i> |

Decision

I wish to advise that, on 18 September 2025, the above development application was **approved in full** subject to conditions by Council. (Refer to the conditions contained in **Attachment 1**)

Details of the approval

This application is not taken to have been approved (a deemed approval) under section 64(5) of the *Planning Act 2016*.

The following approval is given:

| | Planning Regulation 2017 reference | Development Permit | Preliminary Approval |
|---|---|-------------------------------------|-----------------------------|
| Development assessable under the planning scheme, superseded planning scheme, a temporary local planning instrument, a master plan or a preliminary approval which includes a variation approval - Reconfiguring a Lot | N/A | <input checked="" type="checkbox"/> | N/A |

Further development permits

Please be advised that the following development permits are required to be obtained before the development can be carried out:

- Development Permit – Operational Works
- Survey Plan endorsement

Referral agencies for the application

The relevant referral agencies for this application are:

| State Assessment and Referral Agency (SARA) | |
|--|--|
| Address for hand delivery: | 128 Margaret Street, Toowoomba QLD 4350 |
| Address for post: | PO Box 825, TOOWOOMBA QLD 4350 |
| Address for electronic submission: | Applications can be prepared and referred to DILGP online by using MyDAS2. MyDAS2 can be accessed at https://prod2.dev-assess.qld.gov.au/suite/ Email: ToowoombaSARA ToowoombaSARA@dsdmip.qld.gov.au |
| Reason for Referral: | As a <u>Concurrence Agency</u> for an application involving: <i>Development application for an aspect of development stated in schedule 20 that is assessable development under a local categorising instrument or section 21, if—</i> (a) <i>the development is for a purpose stated in schedule 20, column 1 for the aspect; and</i> (b) <i>the development meets or exceeds the threshold—</i> (i) <i>for development in local government area 1—stated in schedule 20, column 2 for the purpose; or</i> (ii) <i>for development in local government area 2—stated in schedule 20, column 3 for the purpose; and</i> |

| | |
|--|--|
| | <p>(c) for development in local government area 1—the development is not for an accommodation activity or an office at premises wholly or partly in the excluded area</p> <p>However, if the development is for a combination of purposes stated in the same item of schedule 20, the threshold is for the combination of purposes and not for each individual purpose.</p> <p>Schedule 10, Part 9, Division 4, Subdivision 1, Table 1, Item 1 of the <i>Planning Regulation 2017</i>.</p> |
| | <p>As a Concurrence Agency for an application involving:</p> <p>Development application for reconfiguring a lot that is assessable development under section 21, if—</p> <p>(a) all or part of the premises are within 25m of a State transport corridor; and</p> <p>(b) 1 or more of the following apply—</p> <ul style="list-style-type: none"> (i) the total number of lots is increased; (ii) the total number of lots adjacent to the State transport corridor is increased; (iii) there is a new or changed access between the premises and the State transport corridor; (iv) an easement is created adjacent to a railway as defined under the <i>Transport Infrastructure Act</i>, schedule 6; and <p>(c) the reconfiguration does not relate to government supported transport infrastructure</p> <p>Schedule 10, Part 9, Division 4, Subdivision 2, Table 1, Item 1 of the <i>Planning Regulation 2017</i></p> |
| | <p>As a Concurrence Agency for an application involving:</p> <p>Development application for reconfiguring a lot that is assessable development under section 21, if—</p> <p>(a) all or part of the premises are—</p> <ul style="list-style-type: none"> (i) adjacent to a road (the relevant road) that intersects with a State-controlled road; and (ii) within 100m of the intersection; and <p>(b) 1 or more of the following apply—</p> <ul style="list-style-type: none"> (i) the total number of lots is increased; (ii) the total number of lots adjacent to the relevant road is increased; (iii) there is a new or changed access between the premises and the relevant road; and <p>(c) the reconfiguration does not relate to government supported transport infrastructure</p> <p>Schedule 10, Part 9, Division 4, Subdivision 2, Table 3, Item 1 of the <i>Planning Regulation 2017</i></p> |

Approved plans, specifications and drawings

Copies of the following approved plans are enclosed.

| Plan/Document Number: | Plan/Document Name: | Date: |
|--------------------------|---|----------|
| 24193-A (Revision 4) | Plan showing proposed lots & road actions at St George Airport. Lot 1 RP55376, Lots 1 & 2 RP55377, Lot 10 RP882935 & Lot 1 SP340406 | 20/12/24 |
| 24193-B (Revision 3) | Plan showing proposed lots and road actions at St George Airport. Lot 1 RP55376, Lots 1 & 2 RP55377, Lot 10 RP882935 & Lot 1 SP340406 | 20/12/24 |
| 22.1003-002 (Revision 6) | St George Airport Industrial Subdivision - Road Geometry Plan | 16/10/23 |
| 22.1003-003 (Revision 6) | St George Airport Industrial Subdivision - Typical Road Sections | 16/10/23 |
| 22.1003-007 (Revision 6) | St George Airport Industrial Subdivision - Earthworks Layout Plan - Sheet 1 of 3 | 16/10/23 |

| Plan/Document Number: | Plan/Document Name: | Date: |
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| 22.1003-008 (Revision 6) | St George Airport Industrial Subdivision - Earthworks Layout Plan - Sheet 2 of 3 | 16/10/23 |
| 22.1003-009 (Revision 6) | St George Airport Industrial Subdivision - Earthworks Layout Plan - Sheet 3 of 3 | 16/10/23 |
| 22.1003-010 (Revision 6) | St George Airport Industrial Subdivision – Roadworks and Stormwater Plan - Sheet 1 of 3 | 16/10/23 |
| 22.1003-011 (Revision 6) | St George Airport Industrial Subdivision – Roadworks and Stormwater Plan - Sheet 2 of 3 | 16/10/23 |
| 22.1003-012 (Revision 6) | St George Airport Industrial Subdivision – Roadworks and Stormwater Plan - Sheet 3 of 3 | 16/10/23 |
| 22.1003-013 (Revision 6) | St George Airport Industrial Subdivision – Stormwater Catchment Plan | 16/10/23 |
| 22.1003-014 (Revision 6) | St George Airport Industrial Subdivision – Stormwater Calculation Table | 16/10/23 |
| 22.1003-015 (Revision 6) | St George Airport Industrial Subdivision – MC01 Longitudinal Section | 16/10/23 |
| 22.1003-016 (Revision 6) | St George Airport Industrial Subdivision - MC02 Longitudinal Section – Sheet 1 of 2 | 16/10/23 |
| 22.1003-017 (Revision 6) | St George Airport Industrial Subdivision - MC02 Longitudinal Section – Sheet 2 of 2 | 16/10/23 |
| 22.1003-018 (Revision 6) | St George Airport Industrial Subdivision - MC03 Longitudinal Section | 16/10/23 |
| 22.1003-019 (Revision 6) | St George Airport Industrial Subdivision - MC04 Longitudinal Section | 16/10/23 |
| 22.1003-020 (Revision 6) | St George Airport Industrial Subdivision – MC01 Cross Sections – Sheet 1 of 2 | 16/10/23 |
| 22.1003-021 (Revision 6) | St George Airport Industrial Subdivision – MC01 Cross Sections – Sheet 2 of 2 | 16/10/23 |
| 22.1003-022 (Revision 6) | St George Airport Industrial Subdivision – MC02 Cross Sections – Sheet 1 of 4 | 16/10/23 |
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| 22.1003-028 (Revision 6) | St George Airport Industrial Subdivision – Roadworks Details Plan – Sheet 1 of 3 | 16/10/23 |
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| 22.1003-030 (Revision 6) | St George Airport Industrial Subdivision – Roadworks Details Plan – Sheet 3 of 3 | 16/10/23 |
| 22.1003-034 (Revision 6) | St George Airport Industrial Subdivision – Water & Sewerage Plan – Sheet 1 of 3 | 16/10/23 |
| 22.1003-035 (Revision 6) | St George Airport Industrial Subdivision – Water & Sewerage Plan – Sheet 2 of 3 | 16/10/23 |
| 22.1003-036 (Revision 6) | St George Airport Industrial Subdivision – Water & Sewerage Plan – Sheet 3 of 3 | 16/10/23 |
| 22.1003-037 (Revision 6) | St George Airport Industrial Subdivision – Sewerage Long Sections – Sheet 1 of 2 | 16/10/23 |

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| 22.1003-038 (Revision 6) | St George Airport Industrial Subdivision – Sewerage Long Sections – Sheet 2 of 2 | 16/10/23 |
| 22.1003-039 (Revision 6) | St George Airport Industrial Subdivision – Water Connection Details | 16/10/23 |
| 22.1003-040 (Revision 6) | St George Airport Industrial Subdivision – Erosion & Sediment Control – Sheet 1 of 3 | 16/10/23 |
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Currency period for the approval (s.85 of the Planning Act)

This approval lapses if a plan for the reconfiguration that, under the Land Title Act, is required to be given to a local government for approval is not given to the local government within 4 years of the approval taking effect.

Appeal Rights

The rights of an applicant to appeal to a tribunal or the Planning and Environment Court against a decision about a development application are set out in chapter 6, part 1 of the *Planning Act 2016*. For certain applications, there may also be a right to make an application for a declaration by a tribunal (see chapter 6, part 2 of the *Planning Act 2016*).

Appeal by an applicant

An applicant for a development application may appeal to the Planning and Environment Court against the following:

- the refusal of all or part of the development application
- a provision of the development approval
- the decision to give a preliminary approval when a development permit was applied for
- a deemed refusal of the development application.

An applicant may also have a right to appeal to the Development tribunal. For more information, see schedule 1 of the *Planning Act 2016*.

The timeframes for starting an appeal in the Planning and Environment Court are set out in section 229 of the *Planning Act 2016*.

Attachment 2 is an extract from the *Planning Act 2016* detailing appeal rights.

To stay informed about any appeal proceedings which may relate to this decision visit:

<https://planning.dsdlip.qld.gov.au/planning/our-planning-system/dispute-resolution/pe-court-database>.

For further information, please contact the Council office on 07 4620 8888 or via email to council@balonne.qld.gov.au.

Yours sincerely



Kate Swepson

Consulting Town Planner

Cc: ToowoombaSARA@dsdlip.qld.gov.au

enc. Attachment 1—Assessment Manager Conditions of Approval (Balonne Shire Council)
Attachment 2 – Concurrence Agency Response
Attachment 3—Appeal Provisions
Attachment 4—Statement of Reasons
Attachment 5—Approved Plans and Specifications

ATTACHMENT 1 – ASSESSMENT MANAGER CONDITIONS OF APPROVAL (BALONNE SHIRE COUNCIL)

GENERAL ADVICE

- i. The relevant planning scheme for this development is *Balonne Shire Planning Scheme 2024*. All references to the 'Planning Scheme' and 'Planning Scheme Schedules' within these conditions refer to the above Planning Scheme.
- ii. A development permit for a Material Change of Use will be required for any activity or development on the approved lot(s) that does not comply with the accepted development criteria in the *Balonne Shire Planning Scheme 2024*.
- iii. All persons involved in the development have an obligation to take all reasonable and practical measures to prevent or minimise any biosecurity risk under the *Biosecurity Act 2014*.
- iv. This approval lapses if a plan for the reconfiguration is not given to the Council within four (4) years of the approval taking effect.
- v. The plan for the reconfiguration must be duly signed by the registered proprietor of the land and the surveyor, and submitted to Council for approval in a form acceptable to Council within the relevant period.

Unless otherwise stated all conditions shall be completed prior to the Council endorsing the relevant plan of survey.

- vi. The *Environmental Protection Act 1994* states that a person must not carry out any activity that causes, or is likely to cause, environmental harm unless the person takes all reasonable and practicable measures to prevent or minimise the harm. Environmental harm includes environmental nuisance. In this regard, persons and entities involved in the establishment of the approved development are to adhere to their 'general environmental duty' to minimise the risk of causing environmental harm to adjoining premises.
- vii. It is the responsibility of the developer to obtain all necessary permits and submit all necessary plans to the relevant authorities that are associated with the approved development, including any permits/approvals required by any State Agencies.
- viii. In completing an assessment of the proposed development, Council has relied on the information submitted in support of the development application as true and correct. Any change to the approved plans and documents may require a new or changed development approval. Council should be contacted for advice in the event of any potential change in circumstances.
- ix. All Aboriginal Cultural Heritage in Queensland is protected under the *Aboriginal Cultural Heritage Act 2003* and penalty provisions apply for any unauthorised harm. Under the legislation a person carrying out an activity must take all reasonable and practicable measures to ensure the activity does not harm Aboriginal Cultural Heritage. This applies whether or not such places are recorded in an official register and whether or not they are located in, on or under private land. The developer is responsible for implementing reasonable and practical measures to ensure Cultural Heritage Duty of Care Guidelines are met and for obtaining any clearances required from the responsible entity.

CONDITIONS

Approved development

1. The approved development is for Reconfiguring a Lot:
 - a. Subdivision – Five (5) lots into thirteen (13) lots
 - b. Dividing land into parts by agreement (Lease A)as defined in the *Planning Act 2016* and as shown on the approved plans.
2. Lease lots may be registered individually upon entering a lease for the relevant lot, subject to all applicable approval conditions being met.
3. The lease of any approved lot must reflect the extent of infrastructure and utility services (i.e. water, sewerage, electricity supply) available to the lot at the time of registering the relevant Survey Plan.
4. Complete and maintain the approved development as follows:
 - a. in accordance with development approval documents; and
 - b. strictly in accordance with those parts of the approved development that have been specified in detail by the Council unless the Council agrees in writing that those parts will be adequately complied with by amended specifications.

Compliance

5. Unless otherwise stated, all conditions must be complied with prior to the Council endorsing the relevant Survey Plan.

Approved documents

6. The approved development is to be carried out in accordance with following approved plans and documents and subject to the approval conditions. Where there is any conflict between the approval conditions and the details shown on the approved plans, the approval conditions prevail.

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7. The approved lot layout is in accordance with approved plans 24193-A (Revision 4) and 24193-B (Revision 3) as listed in the above table. Where there is any conflict between the approved lot layout and approved civil drawings, the approved lot layout prevails.

Existing buildings and structures

8. Existing buildings, structures, infrastructure and services located on the development site are not to encroach on the proposed allotment boundaries.

Development works

9. During construction of the development, the developer shall ensure that all works are carried out by appropriately qualified persons and the developer and the persons carrying out and supervising the work shall be responsible for all aspects of the works, including public and worker safety, and shall ensure adequate barricades, signage and other warning devices are always in place.
10. Any civil engineering and related work shall be designed and supervised by Registered Professional Engineers of Queensland (RPEQ) who are competent in completing the works.
11. All works on or near roadways shall be adequately signed in accordance with the "Manual for Uniform Traffic Control Devices – Part 3, Works on Roads".
12. Construction activities must be undertaken in consultation with the Airport Manager so as to ensure there is no encroachment of the Obstacle Limitation Surface or impediment to airport operations.

Services provision

13. Proposed Lots 1-11 must be connected to Council's reticulated water supply system in accordance with the approved Water & Sewerage Plan, applicable Water Services Association of Australia (WSAA) publication and relevant engineering standards, ensuring an adequate supply is available to meet the operational needs of future land uses.

14. Proposed Lots 1-11 must be connected to Council's reticulated sewerage disposal system in accordance with the approved Water & Sewerage Plan, applicable Water Services Association of Australia (WSAA) publication and relevant engineering standards, ensuring network capacity is adequate to meet the operational needs of future land uses.
15. Proposed Lots 1-11 must be connected to an electricity reticulation service in accordance with the relevant service provider's requirements and specifications along with relevant building standards, requirements and specifications (as relevant).
16. If the lots are connected to a telecommunications service, then such works shall be undertaken in accordance with the relevant service provider's requirements and specifications along with relevant building standards, requirements and specifications (as relevant).
17. All works must comply with:
 - a. the development approval conditions;
 - b. any relevant provisions in the Planning Scheme;
 - c. any relevant standards, drawings or specifications by the Institute of Public Works Engineering Australasia Queensland Division;
 - d. any relevant Australian Standard that applies to that type of work; andany alternative specifications that Council has agreed to in writing and which the developer must ensure do not conflict with any requirements imposed by any applicable laws and standards.
18. Any conflicts associated with proposed and existing services shall be forwarded by the developer to the appropriate controlling authority for approval of any proposed changes.

Stormwater and drainage

19. Stormwater is to be managed in accordance with:
 - a. Approved stormwater plans;
 - b. Queensland Urban Drainage Manual; and
 - c. Pilgrim, DH, (ed.), Australian Rainfall & Runoff – A Guide to Flood Estimation, Institution of Engineers, Australia, Barton, ACT, 1987.
20. Post-development stormwater runoff flows, the characteristics of which include volume, concentration and velocities from the development site, must not exceed pre-development stormwater runoff flows to adjoining properties. Stormwater runoff from the site must not adversely impact on flooding or drainage of properties or roads that are upstream, downstream or adjacent to the site.
21. Discharge of stormwater runoff from the development shall drain freely in all cases, and no nuisance of ponding is to be created as a result of the development.
22. There must be no increases in any silt loads or contaminates in any overland flow from the property being developed during the development process and after the development has been completed. The stormwater disposal system must be designed to include appropriate pollution control devices or methods to ensure no contamination or silting of creeks or other waterways.

Earthworks

23. All earthworks for the development shall be undertaken in accordance with the Institute of Public Works Engineering Australasia Queensland Division.
24. All earthworks, including batters, must be wholly contained within the development site and must not impact on properties, land uses or roads adjoining the site.
25. No contaminated material is to be excavated or disturbed and no contaminated material or waste materials are to be used as fill.
26. Fill material is:
 - a. contained wholly within the site;
 - b. located in a single manageable area;
 - c. located a minimum distance of 10 metres from any property boundary;
 - d. limited to the quantity necessary to establish the proposed works;
 - e. ensures no ponding develops on adjacent land at any time;
 - f. restores all surfaces exposed or damaged by the operations to their original standard immediately on conclusion of the works.

Erosion Control

27. All construction works on site are to be undertaken in accordance with:
 - a. Approved erosion and sediment control plans
 - b. Soil Erosion and Sediment Control Engineering Guidelines for Queensland Construction sites (IE Aust, Queensland Division, Brisbane – or later versions); and
 - c. The Institute of Public Works Engineering Australasia Queensland Division.
28. Erosion and sedimentation controls shall be implemented and maintained at all times during the course of the works. To the extent practicable, erosion and sediment controls are to be established prior to the commencement of works, and be monitored, maintained and adjusted as necessary to ensure ongoing effectiveness.
29. Measures shall be applied to prevent site vehicles tracking sediment and other pollutants onto adjoining roads during the course of the construction period and to prevent dust nuisance during construction. It is the developer's responsibility to ensure that all reasonable measures are taken to protect nearby properties and roads from dust pollution, erosion, siltation or sediment transport.
30. Stockpiles of material capable of being moved by the action of running water shall be stored clear of drainage paths and be prevented from entering the road and/or drainage system.
31. Should it be necessary for the road and/or drainage system to be reinstated or cleaned up due to erosion and/or sedimentation from the site, then such works shall be undertaken immediately.

Roadworks

32. Design and construct the new road, from the Grey Street/Carnarvon Highway intersections as an Industrial Road in accordance with:
 - (a) the approved Road Geometry Plan (Reference 22.1003-002 [Revision 6]) and Typical Road Sections Plan (Reference 22.1003-03 [Revision 6]);
 - (b) provision for stormwater drainage, line marking, signage, street lighting associated with the required road works and road reserve transitions between existing and proposed roads;
 - (c) the developer shall contact Council's Infrastructure Services Department to ensure the design for all civil works is approved prior to commencement of any works onsite.
33. Intersections with Grey Street/Carnarvon Highway and the new road are to be upgraded in accordance with the Department of Transport and Main Roads standards.

Access

34. Any future vehicle access to the proposed new lots is to be constructed from the edge of bitumen seal to the property boundary. Any future crossover is to be constructed to a sealed standard, generally in accordance with IPWEA Drawing – RSD-102, and must be designed to cater for the maximum vehicle size exiting the site, ensuring no damage to the roadway or kerb.

Each of the proposed new lots must ensure that any future driveways or access paths are constructed in accordance with Councils standard designs for residential driveways.
35. The landowner is responsible for the construction and maintenance of crossovers from the road carriageway to the property boundary and all internal vehicle access ways, and for obtaining any approvals that may be required and for complying with the applicable designs and standards.
36. Vehicle crossovers must be located a minimum distance of one metre from any power poles, road signage, stormwater gully pits or other Council assets, unless otherwise specified in the applicable development standards and specifications.

Protection of infrastructure

37. The developer is responsible for locating and protecting any Council and public utility services, infrastructure and assets. Any damage to existing infrastructure (road pavement, existing underground assets, etc.) attributable to the development, shall be immediately rectified in accordance with the asset owners' requirements and specifications and to the satisfaction of the asset owners' representative(s) and at no cost to Council.

Land Dedication

38. Land shown on the Approved Plan as New Road must be dedicated as Road Reserve.

Latest versions

39. Where another condition refers to a specific published standard, manual or guideline, including specifications, drawings, provisions and criteria within those documents, that condition shall be deemed as referring to the latest versions of those publications that are publicly available at the time the first operational works or compliance approval is lodged with the assessment manager or approval agency for those types of works to be performed or approved, unless a regulation or law requires otherwise.

Avoiding nuisance

40. During the establishment of the approved development, no nuisance is to be caused to adjoining properties and occupiers, including by way of smoke, dust, rubbish, contaminant, stormwater discharge or siltation at any time.
41. Dust emanating as a result of any development works onsite must be continually monitored and suppressed in order to prevent any dust impacting on airport operations, drifting onto road networks or nearby properties.
42. Parking areas for vehicles associated with the construction works must be provided within the development site.
43. The transportation of materials, equipment and machinery to and from the site during the course of the construction activities, is to be undertaken in a manner that ensures public access ways and roads are kept free of dust and spoil.
44. Construction activities must not prevent or obstruct access to adjoining properties, land uses or roads.
45. Any waste associated with the development works must be contained and disposed of from the site to prevent release to adjoining properties and roads.
46. The area and its surrounds shall be kept in an orderly fashion, free of rubbish and clear of weeds and long grasses (during construction and post construction). The premises is to be maintained in a clean and tidy condition and not to pose any health and safety risks to the community.

ATTACHMENT 2 – CONCURRENCE AGENCY RESPONSE



SARA reference: 2503-45237 SRA
 Council reference: RL127

11 August 2025

Chief Executive Officer
 Balonne Shire Council
 PO Box 201
 ST GEORGE QLD 4487
 council@balonne.qld.gov.au

Attention: Kate Swepson

Dear Kate

SARA referral agency response—130-152 Grey Street, St George

(Referral agency response given under section 56 of the *Planning Act 2016*)

The development application described below was confirmed as properly referred by the State Assessment and Referral Agency (SARA) on 1 April 2025.

Response

| | |
|-------------------|--|
| Outcome: | Referral agency response – with conditions |
| Date of response: | 11 August 2025 |
| Conditions: | The conditions in Attachment 1 must be attached to any development approval |
| Advice: | Advice to the applicant is in Attachment 2 |
| Reasons: | The reasons for the referral agency response are in Attachment 3 |

Development details

| | | |
|---------------|--------------------|---|
| Description: | Development permit | Reconfiguring a Lot – Subdivision (Five (5) lots into fourteen (14) lots and road reserve) |
| SARA role: | Referral agency | |
| SARA trigger: | | <ul style="list-style-type: none"> • Schedule 10, part 9, division 4, subdivision 1, table 1, item 1 (10.9.4.1.1.1) - Development impacting on state transport infrastructure • Schedule 10, part 9, division 4, subdivision 2, table 1, item 1 (10.9.4.2.1.1) - Reconfiguring a lot near a state transport |

corridor.

- Schedule 10, part 9, division 4, subdivision 2, table 3, item 1 (10.9.4.2.3.1) - Reconfiguring a lot near a state-controlled road intersection (Planning Regulation 2017)

SARA reference: 2503-45237 SRA

Assessment manager: Balonne Shire Council

Street address: 54 Carnarvon Highway, St George; 130-152 Grey Street, St George

Real property description: Lot 9 & 10 on RP882935 & Lot 1 on SP340406, Lots 1 & 2 on RP55377

Applicant name: Balonne Shire Council

Applicant contact details: 193-199 Grey Street
St George QLD 4487
sharyn.arnold@balonne.qld.gov.au

Human Rights Act 2019 considerations: A consideration of the 23 fundamental human rights protected under the *Human Rights Act 2019* has been undertaken as part of this decision. It has been determined that this decision does not limit human rights.

Representations

An applicant may make representations to a concurrence agency, at any time before the application is decided, about changing a matter in the referral agency response (s.30 Development Assessment Rules). Copies of the relevant provisions are in **Attachment 4**.

A copy of this response has been sent to the applicant for their information.

For further information please contact Danica Clark, Senior Planner, on 4616 7305 or via email ToowoombaSARA@dsdilgp.qld.gov.au who will be pleased to assist.

Yours sincerely



Paul Gleeson
A/Manager

cc Ms Sharyn Arnold, sharyn.arnold@balonne.qld.gov.au
enc Attachment 1 - Referral agency conditions
Attachment 2 - Advice to the applicant
Attachment 3 - Reasons for referral agency response
Attachment 4 - Representations about a referral agency response provisions

Attachment 1—Referral agency conditions

(Under section 56(1)(b)(i) of the *Planning Act 2016* the following conditions must be attached to any development approval relating to this application)

| No. | Conditions | Condition timing |
|---|--|---|
| Reconfiguring a Lot – Subdivision (Five (5) lots into fourteen (14) lots and road reserve) | | |
| Schedule 10, part 9, division 4, subdivision 1, table 1, item 1 (10.9.4.1.1.1) - Development impacting on state transport infrastructure and Schedule 10, part 9, division 4, subdivision 2, table 1, item 1 - Reconfiguring a lot near a state transport corridor & Schedule 10, part 9, division 4, subdivision 2, table 3, item 1 - Reconfiguring a lot near a state-controlled road intersection (Planning Regulation 2017)—The chief executive administering the <i>Planning Act 2016</i> nominates the Director-General of the Department of Transport and Main Roads to be the enforcement authority for the development to which this development approval relates for the administration and enforcement of any matter relating to the following conditions: | | |
| 1. | <p><u>Carnarvon Highway / Cliff Parsons Drive Intersection</u></p> <p>(a) Road works comprising CHR and AUL turn treatments are to be provided at the intersection of the Carnarvon Highway and the Cliff Parsons Drive, designed and constructed to accommodate the largest design vehicle legally permitted on the state-controlled road (i.e. Type 1 Road train).</p> <p>(b) The road works must be designed and constructed in accordance with the Department of Transport and Main Roads' Road Planning and Design Manual and any external material referenced therein.</p> | Prior to submitting the Plan of Survey to the local government for approval. |
| 2. | <p><u>Carnarvon Highway / Buchan Bypass / Proposed Airport Road Intersection</u></p> <p>(a) Road works comprising CHR and AUL turn treatments are to be provided at the four-way intersection of Carnarvon Highway, Buchan Bypass and Airport Road designed and constructed to accommodate the largest design vehicle legally permitted on the state-controlled road (i.e. Type 1 Road train).</p> <p>(b) The road works must be designed and constructed in accordance with the Department of Transport and Main Roads' Road Planning and Design Manual and any external material referenced therein.</p> | Prior to submitting the Plan of Survey to the local government for approval. |
| 3. | <p>(a) Stormwater management of the development must not cause worsening to the operating performance of the state-controlled road corridor, such that any works on the land must not:</p> <p>(i) create any new discharge points for stormwater runoff onto the state-controlled road corridor.</p> <p>(ii) concentrate or increase the velocity of flows to the state-controlled road corridor.</p> <p>(iii) interfere with and/or cause damage to the existing stormwater drainage on the state-controlled road corridor.</p> <p>(iv) surcharge any existing culvert or drain on the state-controlled</p> | <p>(a) At all times.</p> <p>(b) Prior to obtaining development approval for operational work</p> <p>(c) Within 20 business days of the completion of works.</p> |

| | | |
|--|--|--|
| | <p>road corridor.</p> <p>(v) reduce the quality of stormwater discharge onto the state-controlled road corridor.</p> <p>(vi) impede or interfere with any overland flow or hydraulic conveyance from the state-controlled road corridor.</p> <p>(vii) reduce the floodplain immunity of the state-controlled road corridor.</p> <p>(b) Submit RPEQ certification with supporting documentation to Downs.South.West.IDAS@tmr.qld.gov.au within the Department of Transport and Main Roads, confirming that the development has been designed in accordance with part (a) of this condition.</p> <p>(c) Submit RPEQ certification supporting documentation to Downs.South.West.IDAS@tmr.qld.gov.au within the Department of Transport and Main Roads, confirming that the development has been constructed in accordance with parts (a) and (b) of this condition.</p> | |
|--|--|--|

Attachment 2—Advice to the applicant

| General advice | |
|-----------------------|--|
| 1. | Terms and phrases used in this document are defined in the <i>Planning Act 2016</i> , its regulation or the State Development Assessment Provisions (SDAP) (version 3.0). If a word remains undefined it has its ordinary meaning. |
| 2. | <p>Road works approval: Under section 33 of the <i>Transport Infrastructure Act 1994</i>, written approval is required from the Department of Transport and Main Roads to carry out road works on a state-controlled road. Please contact the Department of Transport and Main Roads on 4639 0743 to make an application for road works approval.</p> <p>This approval must be obtained prior to commencing any works on the state-controlled road reserve. The approval process may require the approval of engineering designs of the proposed works, certified by a Registered Professional Engineer of Queensland (RPEQ). The road works approval process takes time – please contact Transport and Main Roads as soon as possible to ensure that gaining approval does not delay construction.</p> |

Attachment 3—Reasons for referral agency response

(Given under section 56(7) of the *Planning Act 2016*)

The reasons for the SARA's decision are:

With conditions, the development complies with State code 1 and 6. Specifically, the development:

- does not increase the likelihood or frequency of accidents, fatalities, or serious injury for users of a state-controlled road
- does not adversely impact the structural integrity or physical condition of state-controlled roads, road transport infrastructure, public passenger transport infrastructure or active transport infrastructure
- does not adversely impact the function efficiency of state-controlled roads or future state-controlled roads
- does not adversely impact the state's ability to plan, construct, maintain, upgrade or operate state-controlled roads, future state-controlled roads or road transport infrastructure
- does not significantly increase the cost to plan, construct, upgrade or maintain state-controlled roads, future state-controlled roads or road transport infrastructure.

Material used in the assessment of the application:

- the development application material and submitted plans
- *Planning Act 2016*
- Planning Regulation 2017
- the SDAP (version 3.0), as published by SARA
- the Development Assessment Rules
- SARA DA Mapping system
- section 58 of the *Human Rights Act 2019*

Attachment 4—Representations about a referral agency response provisions

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Development Assessment Rules—Representations about a referral agency response

The following provisions are those set out in sections 28 and 30 of the Development Assessment Rules¹ regarding **representations about a referral agency response**

Part 6: Changes to the application and referral agency responses

28 Concurrence agency changes its response or gives a late response

- 28.1. Despite part 2, a concurrence agency may, after its referral agency assessment period and any further period agreed ends, change its referral agency response or give a late referral agency response before the application is decided, subject to section 28.2 and 28.3.
- 28.2. A concurrence agency may change its referral agency response at any time before the application is decided if—
 - (a) the change is in response to a change which the assessment manager is satisfied is a change under section 26.1; or
 - (b) the Minister has given the concurrence agency a direction under section 99 of the Act; or
 - (c) the applicant has given written agreement to the change to the referral agency response.²
- 28.3. A concurrence agency may give a late referral agency response before the application is decided, if the applicant has given written agreement to the late referral agency response.
- 28.4. If a concurrence agency proposes to change its referral agency response under section 28.2(a), the concurrence agency must—
 - (a) give notice of its intention to change its referral agency response to the assessment manager and a copy to the applicant within 5 days of receiving notice of the change under section 25.1; and
 - (b) the concurrence agency has 10 days from the day of giving notice under paragraph (a), or a further period agreed between the applicant and the concurrence agency, to give an amended referral agency response to the assessment manager and a copy to the applicant.

¹ Pursuant to Section 68 of the *Planning Act 2016*

² In the instance an applicant has made representations to the concurrence agency under section 30, and the concurrence agency agrees to make the change included in the representations, section 28.2(c) is taken to have been satisfied.

Part 7: Miscellaneous

30 Representations about a referral agency response

30.1. An applicant may make representations to a concurrence agency at any time before the application is decided, about changing a matter in the referral agency response.³

³ An applicant may elect, under section 32, to stop the assessment manager's decision period in which to take this action. If a concurrence agency wishes to amend their response in relation to representations made under this section, they must do so in accordance with section 28.

ATTACHMENT 3 – PLANNING ACT EXTRACT APPEAL RIGHTS

Chapter 6 Dispute resolution Part 1 Appeal rights

228 Appeals to tribunal or P&E Court

(1) Schedule 1 states—

- (a) matters that may be appealed to—
 - (i) either a tribunal or the P&E Court; or
 - (ii) only a tribunal; or
 - (iii) only the P&E Court; and
- (b) the person—
 - (i) who may appeal a matter (the **appellant**); and
 - (ii) who is a respondent in an appeal of the matter; and
 - (iii) who is a co-respondent in an appeal of the matter; and
 - (iv) who may elect to be a co-respondent in an appeal of the matter.

(2) An appellant may start an appeal within the appeal period.

(3) The appeal period is—

- (a) for an appeal by a building advisory agency—10 business days after a decision notice for the decision is given to the agency; or
- (b) for an appeal against a deemed refusal—at any time after the deemed refusal happens; or
- (c) for an appeal against a decision of the Minister, under chapter 7, part 4, to register premises or to renew the registration of premises—20 business days after a notice is published under section 269(3)(a) or (4); or
- (d) for an appeal against an infrastructure charges notice—20 business days after the infrastructure charges notice is given to the person; or
- (e) for an appeal about a deemed approval of a development application for which a decision notice has not been given—30 business days after the applicant gives the deemed approval notice to the assessment manager; or
- (f) for any other appeal—20 business days after a notice of the decision for the matter, including an enforcement notice, is given to the person.

Note—

See the P&E Court Act for the court's power to extend the appeal period.

(4) Each respondent and co-respondent for an appeal may be heard in the appeal.

(5) If an appeal is only about a referral agency's response, the assessment manager may apply to the tribunal or P&E Court to withdraw from the appeal.

(6) To remove any doubt, it is declared that an appeal against an infrastructure charges notice must not be about—

- (a) the adopted charge itself; or
- (b) for a decision about an offset or refund—
 - (i) the establishment cost of trunk infrastructure identified in a LGIP; or

- (ii) the cost of infrastructure decided using the method included in the local government's charges resolution.

229 Notice of appeal

- (1) An appellant starts an appeal by lodging, with the registrar of the tribunal or P&E Court, a notice of appeal that—
 - (a) is in the approved form; and
 - (b) succinctly states the grounds of the appeal.
- (2) The notice of appeal must be accompanied by the required fee.
- (3) The appellant or, for an appeal to a tribunal, the registrar must, within the service period, give a copy of the notice of appeal to—
 - (a) the respondent for the appeal; and
 - (b) each co-respondent for the appeal; and
 - (c) for an appeal about a development application under
 - (d) schedule 1, table 1, item 1—each principal submitter for
 - (e) the development application; and
 - (f) for an appeal about a change application under
 - (g) schedule 1, table 1, item 2—each principal submitter for
 - (h) the change application; and
 - (i) each person who may elect to become a co-respondent
 - (j) for the appeal, other than an eligible submitter who is not a principal submitter in an appeal under paragraph (c) or (d); and
 - (k) for an appeal to the P&E Court—the chief executive; and
 - (l) for an appeal to a tribunal under another Act—any other person who the registrar considers appropriate.
- (4) The **service period** is—
 - (a) if a submitter or advice agency started the appeal in the P&E Court—2 business days after the appeal is started; or
 - (b) otherwise—10 business days after the appeal is started.
- (5) A notice of appeal given to a person who may elect to be a co-respondent must state the effect of subsection (6).
- (6) A person elects to be a co-respondent by filing a notice of election, in the approved form, within 10 business days after the notice of appeal is given to the person.

230 Other appeals

- (1) Subject to this chapter, schedule 1 and the P&E Court Act, unless the Supreme Court decides a decision or other matter under this Act is affected by jurisdictional error, the decision or matter is non-appealable.
- (2) The Judicial Review Act 1991, part 5 applies to the decision or matter to the extent it is affected by jurisdictional error.

(3) A person who, but for subsection (1) could have made an application under the Judicial Review Act 1991 in relation to the decision or matter, may apply under part 4 of that Act for a statement of reasons in relation to the decision or matter.

(4) In this section—

decision includes—

- (a) conduct engaged in for the purpose of making a decision; and
- (b) other conduct that relates to the making of a decision; and
- (c) the making of a decision or the failure to make a decision; and
- (d) a purported decision; and
- (e) a deemed refusal.

non-appealable, for a decision or matter, means the decision or matter—

- (a) is final and conclusive; and
- (b) may not be challenged, appealed against, reviewed, quashed, set aside or called into question in any other way under the Judicial Review Act 1991 or otherwise, whether by the Supreme Court, another court, a tribunal or another entity; and
- (c) is not subject to any declaratory, injunctive or other order of the Supreme Court, another court, a tribunal or another entity on any ground.

231 Rules of the P&E Court

(1) A person who is appealing to the P&E Court must comply with the rules of the court that apply to the appeal.

(2) However, the P&E Court may hear and decide an appeal even if the person has not complied with rules of the P&E Court.

ATTACHMENT 4 — STATEMENT OF REASONS

The following information is provided in accordance with section 63 of the *Planning Act 2016*.

| | |
|--------------------------------------|--|
| Description of development | Development Application for Reconfiguring a Lot – Five (5) lots into thirteen (13) lots and dividing land into parts by agreement |
| Assessment benchmarks | <p>The assessment manager has assessed the application against the following—</p> <ul style="list-style-type: none">• the Darling Downs Regional Plan;• the Maranoa-Balonne Regional Plan;• the State Planning Policy;• the Planning Scheme <p>Reconfiguring a lot code</p> |
| Relevant matters | N/A – there are no relevant matters for a code assessable application. |
| Matters raised in submissions | N/A – there are no submissions for a code assessable application. |
| Reasons for the decision | <ul style="list-style-type: none">• The proposed reconfiguration facilitates the creation of a range of lot sizes to support a range of industrial land uses or aviation related businesses.• The proposed lot layout removes conflicts with boundaries over existing airport infrastructure and constructed roads.• All lots will be serviced by reticulated infrastructure services.• The proposal will not adversely impact mapped areas of regulated vegetation on the site.• The proposal will not impact the viability of the stock route network <p>On balance, the proposal presents no significant inconsistencies with assessment benchmarks. Development conditions have been imposed to ensure compliance to the greatest extent possible.</p> |

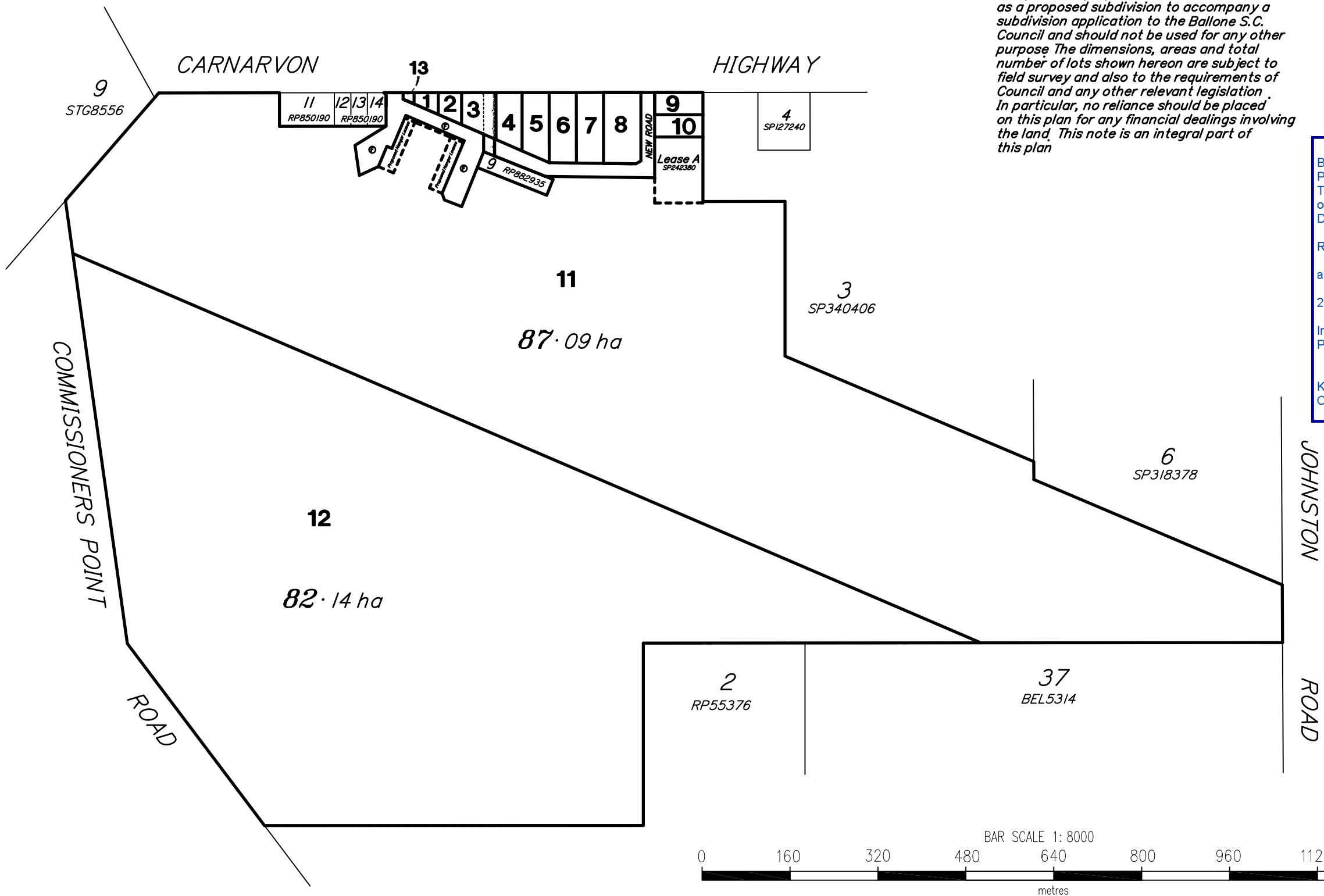
ATTACHMENT 5 — APPROVED PLANS AND SPECIFICATIONS



Note:

This plan was prepared for BALLONE S.C. as a proposed subdivision to accompany a subdivision application to the Ballone S.C. Council and should not be used for any other purpose. The dimensions, areas and total number of lots shown hereon are subject to field survey and also to the requirements of Council and any other relevant legislation. In particular, no reliance should be placed on this plan for any financial dealings involving the land. This note is an integral part of this plan.

BALONNE SHIRE COUNCIL
Planning Act 2016
This document comprises part of
Development Permit No.
RL127
and was issued on
24 September 2025
In accordance with the :-
Planning Act 2016
KATE SWEPSON
Consultant Town Planner



BALONNE SHIRE COUNCIL

SMK QLD PTY. LTD.

Goondiwindi | Brisbane | Gold Coast | Toowoomba | Gatton
Ph (07)4671 2445 Email admin@smkqld.com.au

PLAN SHOWING PROPOSED LOTS & ROAD ACTIONS
AT ST GEORGE AIRPORT. LOT 1 RP55376,
LOTS 1 & 2 RP55377, LOT 10 RP882935
& LOT 1 SP340406.

24193 REV 4

SCALE: 1:8000

Drawn RPK 20/12/24 Checked JFJ 20/12/24

24193-A A3

BALONNE SHIRE COUNCIL
Planning Act 2016
This document comprises part
of
Development Permit No.

RL127

and was issued on

24 September 2025

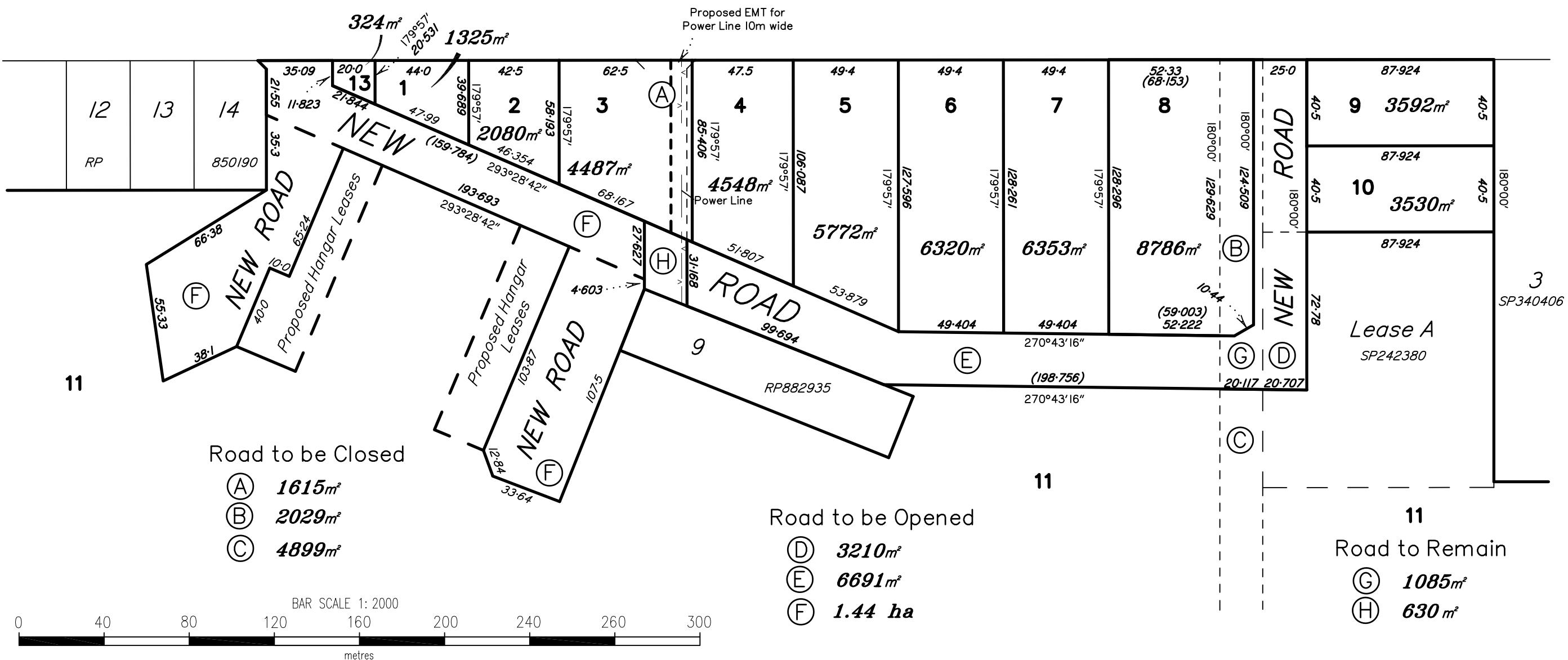
In accordance with the :-
Planning Act 2016

KATE SWEPSON
Consultant Town Planner



CARNARVON

HIGHWAY



BALONNE SHIRE COUNCIL

SMK QLD PTY. LTD

Goondiwindi | Brisbane | Gold Coast | Toowoomba | Gatton

*PLAN SHOWING PROPOSED LOTS AND ROAD
ACTIONS AT ST GEORGE AIRPORT
LOT 1 RP55377, LOT 10 RP882935 & LOT 1 SP340406
CARNARVON HWY. ST GEORGE*

24193 Rev 3

3/24 Checked 1F 1 20/12/23

24193-B



STN 9007

ROAD MC02 GEOMETRY

| PT | CHAINAGE | EASTING | NORTHING | HEIGHT | BEARING | DEP.SEG | DEP.RAD | DEP.LEN |
|----|----------|------------|-------------|---------|---------------|---------|----------|---------|
| S | 0.000 | 656498.884 | 6896778.206 | 199.690 | 188°26'33.79" | LINE | | 21.597 |
| TC | 21.597 | 656495.713 | 6896756.843 | 199.271 | 188°26'33.79" | ARC | -30.000 | 35.162 |
| CT | 56.759 | 656509.808 | 6896726.801 | 199.240 | 121°17'14.97" | LINE | | 180.896 |
| TC | 237.655 | 656664.396 | 6896632.856 | 198.890 | 121°17'14.97" | ARC | 500.000 | 30.406 |
| CT | 268.061 | 656689.884 | 6896616.285 | 198.789 | 124°46'18.26" | LINE | | 48.445 |
| TC | 316.506 | 656729.678 | 6896588.657 | 198.676 | 124°46'18.26" | ARC | -100.000 | 46.154 |
| CT | 362.659 | 656772.226 | 6896571.854 | 198.681 | 98°19'38.96" | LINE | | 160.485 |
| E | 523.144 | 656931.018 | 6896548.611 | 198.841 | 98°19'38.96" | | | |

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PROTERRA GROUP
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GOONDIWINDI QLD 4390
ABN: 82 626 886 771
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DO NOT SCALE FROM THIS DRAWING. USE DIMENSIONS WHERE MARKED

Scales:
 1:1000 AT A1

Designed: BJS
Drafter: GCC
Check: LOD
Approved:

ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
ROAD GEOMETRY PLAN

Client: BALONNE SHIRE COUNCIL
Revision: 6

A1 ORIGINAL SIZE

| | | | | |
|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |
| Rev | By | Description | Approved | Date |

ROAD MC03 GEOMETRY

| PT | CHAINAGE | EASTING | NORTHING | HEIGHT | BEARING | DEP.SEG | DEP.RAD | DEP.LEN |
|----|----------|------------|-------------|---------|---------------|---------|---------|---------|
| S | 0.000 | 656627.158 | 6896655.486 | 199.079 | 211°16'40.30" | LINE | | 79.051 |
| TC | 79.051 | 656586.116 | 6896587.924 | 198.730 | 211°16'40.30" | ARC | -50.000 | 17.805 |
| CT | 96.857 | 656579.746 | 6896571.398 | 198.685 | 190°52'27.48" | LINE | | 5.350 |
| E | 102.207 | 656578.737 | 6896566.143 | 198.672 | 190°52'27.48" | | | |

ROAD MC04 GEOMETRY

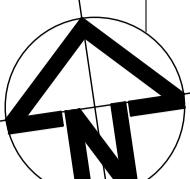
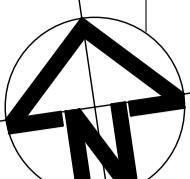
| PT | CHAINAGE | EASTING | NORTHING | HEIGHT | BEARING | DEP.SEG | DEP.RAD | DEP.LEN |
|----|----------|------------|-------------|---------|---------------|---------|---------|---------|
| S | 0.000 | 656503.792 | 6896731.614 | 199.218 | 220°18'08.70" | LINE | | 6.935 |
| TC | 6.935 | 656499.307 | 6896726.325 | 199.149 | 220°18'08.70" | ARC | -50.000 | 7.867 |
| CT | 14.802 | 656494.710 | 6896719.951 | 199.082 | 211°17'14.97" | LINE | | 30.126 |
| TC | 44.928 | 656479.065 | 6896694.206 | 198.972 | 211°17'14.97" | ARC | 50.000 | 17.994 |
| CT | 62.922 | 656467.183 | 6896680.822 | 198.981 | 231°54'26.14" | LINE | | 34.918 |
| E | 97.840 | 656439.702 | 6896659.280 | 198.888 | 231°54'26.14" | | | |

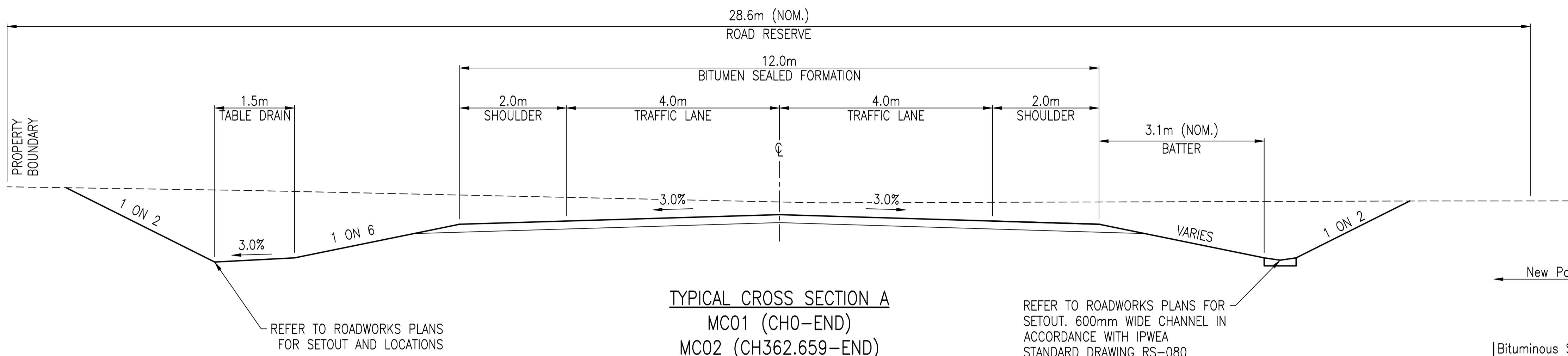
NOTES

1. ALL RL LEVELS SHOWN IN TABLES ARE F.S.L (FINISHED SURFACE LEVEL)

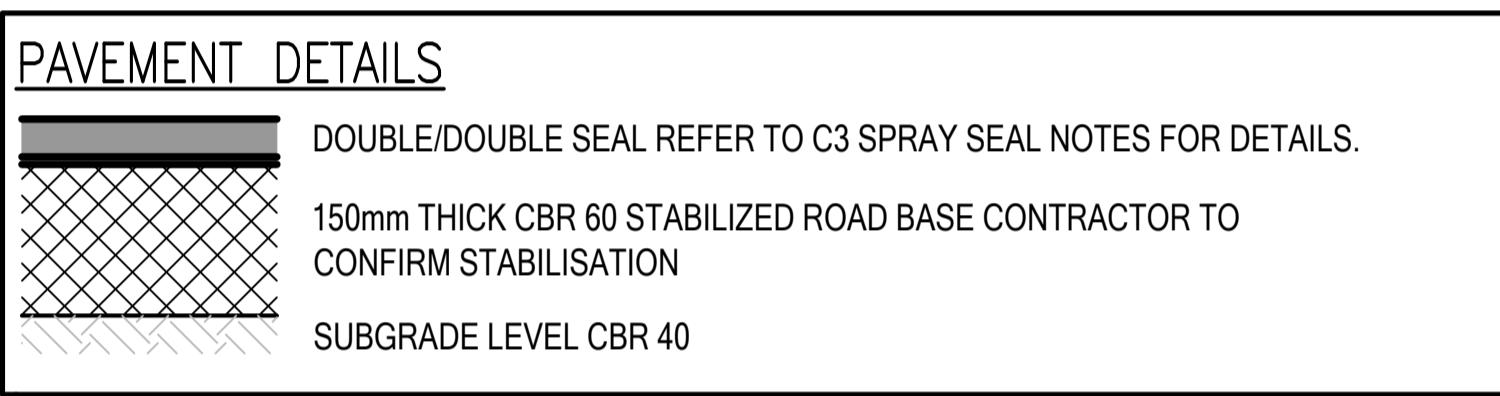
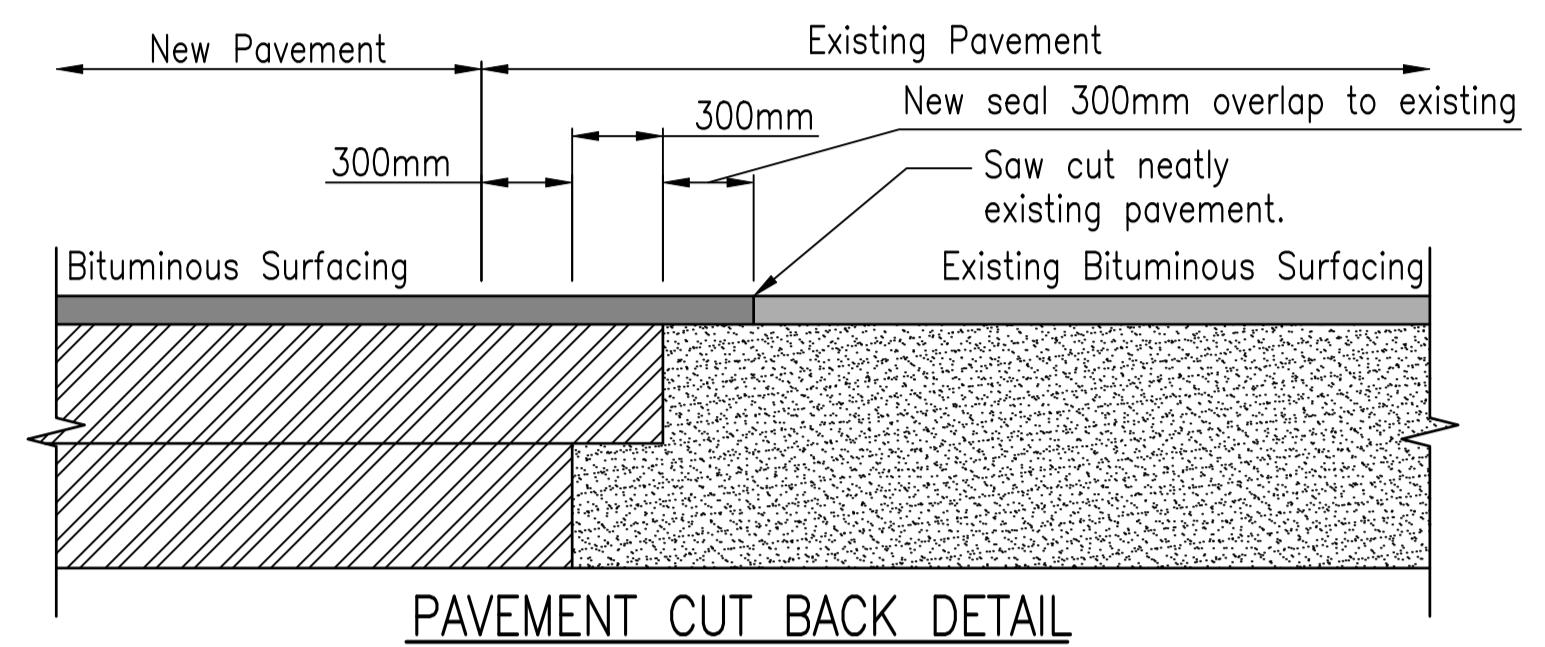
SURVEY CONTROL POINTS

| STN NUMBER | EASTING | NORTHING | RL (MARK) | MARK TYPE |
|------------|------------|-------------|-----------|---------------|
| 9000 | 656726.560 | 6896629.769 | 198.380 | DPY PEG |
| 9001 | 656705.310 | 6896606.271 | 198.527 | SCREW IN BIT |
| 9002 | 656495.403 | 6896748.930 | 199.532 | DPY PEG |
| 9003 | 656625.333 | 6896712.589 | 198.946 | DPY PEG |
| 9004 | 656610.545 | 6896578.529 | 198.713 | SCREW IN BIT |
| 9005 | 656648.673 | 6896591.408 | 198.723 | SCREW IN CONC |
| 9007 | 656388.360 | 6896505.314 | 199.414 | DPY PEG |
| 9020 | 656443.898 | 6896801.888 | 199.330 | DPY PEG |
| 9023 | 656666.632 | 6896569.270 | 198.630 | SCREW IN BIT |





THE CONTRACTOR IS TO NOTIFY THE SUPERVISING ENGINEER OF ANY DISCREPANCIES BETWEEN THE DESIGN PLANS AND THE CONDITIONS ON SITE PRIOR TO COMMENCEMENT OF ANY WORK.

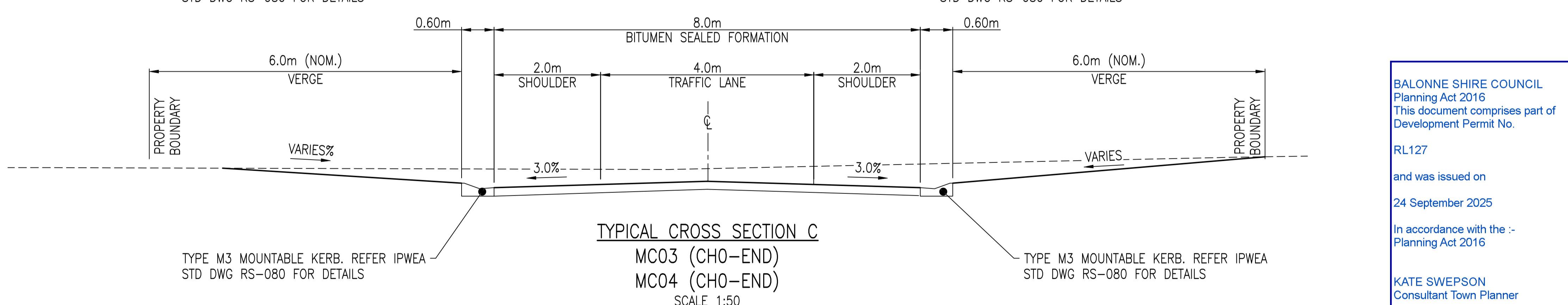
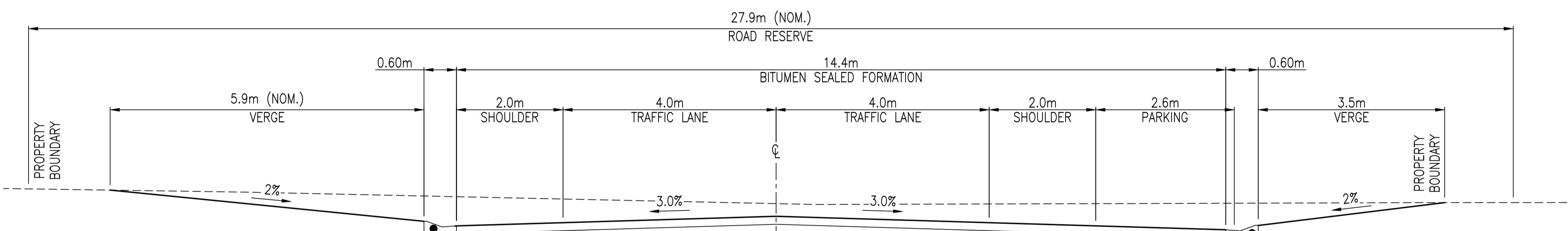


SPRAY SEAL SURFACING NOTES

1. Spray seal surfacing of the road pavement is to be carried out in accordance with DTMR specification MRTS11.
2. Inspection and Testing of Spray Seal Surfacing is to be carried out in accordance with DTMR requirements.
3. Double/double seal design to be determined by contractor and to be provided to supervising engineer prior to construction for Approval.

SEAL DESIGN

2nd Coat seal – Seal S10E/S35E @ 1.2 L/m² & 10mm Precoated Aggregate @ 140m²/m³
1st Coat Seal – Primersal AMC7 @ 1.6L/m² & 14mm Precoated Aggregate @ 100m²/m³

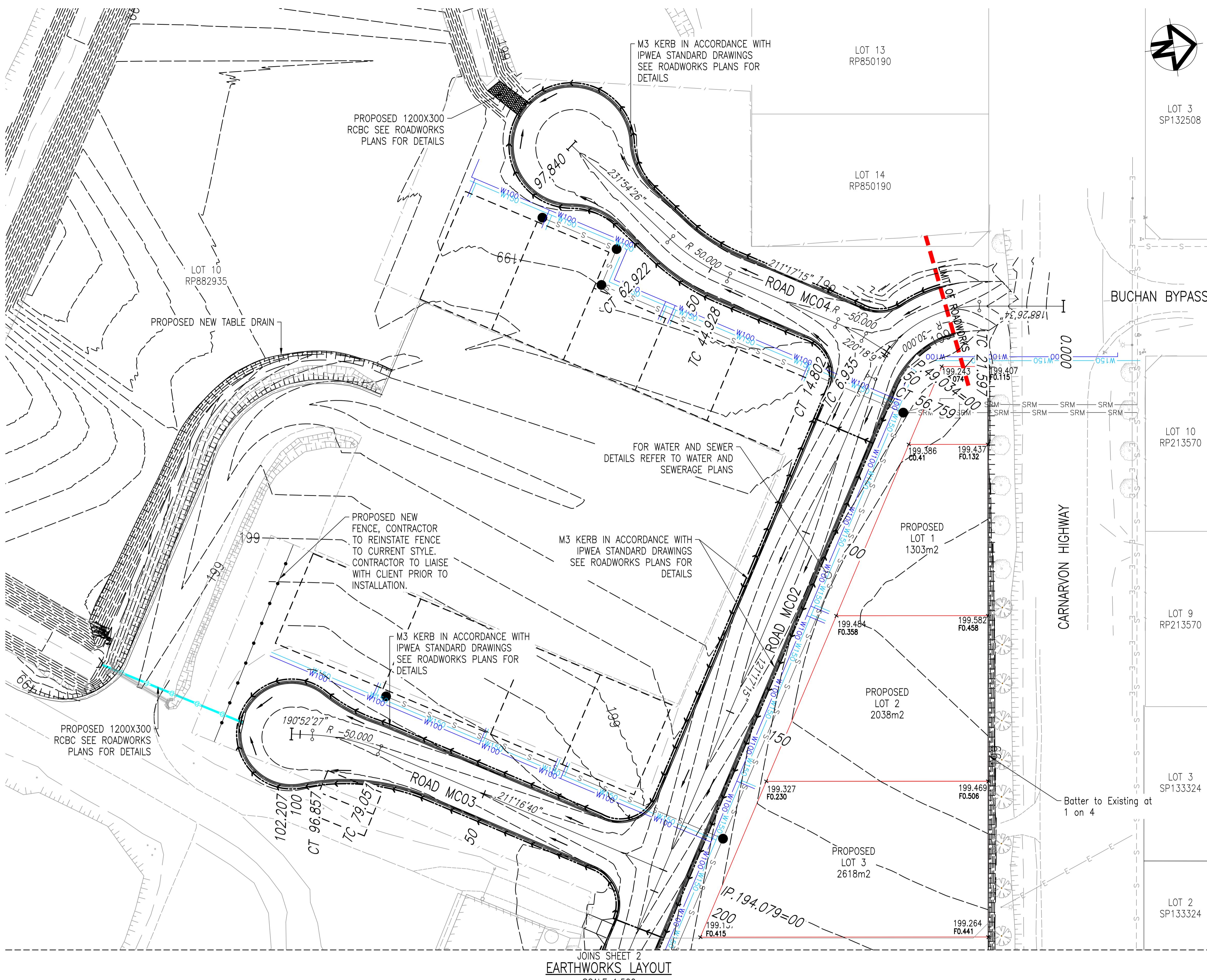


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Consultant Town Planner

ROAD SUBGRADE AND GRAVEL PAVEMENT NOTES

1. Subgrade is to be trimmed to an even surface, free from loose material, organic matter and graded to be free-draining
2. Compact subgrade to 97% characteristic Standard Density Ratio as defined by Queensland main roads test method Q020, based on test methods AS 1289.5.1.1, AS 1289.5.7.1, and AS 1289.5.4.1
3. Proof roll subgrade to detect any soft spots, soft spots shall be removed and backfilled with approved sub-base material.
4. subgrade affected by rainfall after final trimming is not to be accepted until appropriate drying out treatment has been affected.
5. Each pavement course is not to be commenced until the previous course has been inspected and approved by the supervising engineer.
6. Select Fill to be minimum CBR15 material, placed in maximum 150mm layers and placed at optimum moisture content and compacted to 97% characteristic standard density Ratio as defined by Queensland Main Roads Test method Q020, based on test methods AS 1289.5.1.1, AS 1289.5.7.1, and AS 1289.5.4.1.
7. Pavement courses to be placed as optimum moisture content in 100mm (minimum) – 150mm (maximum) thick layers and compacted to 100% Characteristic standard Dry density Ratio as defined by Queensland Main Roads Test Method Q020, based on test methods AS 1289.5.1.1, and AS1289.5.4.1.
8. Compaction and preparation is to be carried out in accordance with Balonne Shire Council Planning Scheme and the contractor shall ensure construction results are provided in accordance with this document.
9. Inspections and Testing of compacted material is to be carried out in accordance with Balonne Shire Council Planning Scheme and the contractor shall ensure results are provided in accordance with this document.
10. Pavement depths are to be verified by the provision of "as Constructed" Survey information. The contractor shall ensure survey information of each pavement course is collected and submitted in accordance with Balonne Shire Council requirements.

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| 5 | CDH | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | PROTERRA GROUP 31 GLASSER STREET GOONDIWINDI QLD 4390 ABN: 82 626 886 771 TEL: (07) 4671 5701 WWW.PROTERRAGROUP.COM.AU | | | | Designed BJS Drafter GCC Check LOD Approved | | | | Client BALONNE SHIRE COUNCIL | | | | Sheet 3 of 42 |
| 4 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | A1 ORIGINAL SIZE | | | | Revision. 6 | | | | | | | | |
| 3 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | | | | | | | | | | | | |
| 2 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | | | | | | | | | | | | |
| 1 | Rev | By | Description | Approved | Date | | | | | | | | | | | | | | |



EARTHWORKS LEGEND

| | |
|-------------------------|--|
| — | DESIGN SURFACE CONTOURS |
| 6.32 x _{CD} | FINISHED ALLOTMENT LEVEL CUT/FILL VALUE |
| 6.41 x _{Ex} | DENOTES EXISTING ALLOTMENT LEVEL |
| — | TOP OF BATTER |
| — | BOTTOM OF BATTER |
| — | INVERT OF DRAIN |
| — | DIRECTION OF KERB & CHANNEL FLOW |

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Consultant Town Planner

EXISTING SERVICES

ALL EXISTING SERVICES ARE TO BE
LOCATED BY THE CONTRACTOR
THROUGH CONTACTING THE RELEVANT
SERVICE AUTHORITY PRIOR TO THE
COMMENCEMENT OF ANY WORK

EXISTING LEVELS

THE CONTRACTOR IS TO VERIFY ALL
LEVELS PRIOR TO CONSTRUCTION.
ANY DISCREPANCIES ARE TO BE
REPORTED TO THE SUPERINTENDENT
FOR DIRECTION.

NOTES

1. Strip all topsoil, vegetation and deleterious material from earthworks area.
2. Proof roll subgrade to detect soft spots. soft spots shall be removed and backfilled with approved fill material.
3. All earthworks filling to be in accordance with AS 3798.
4. All fill material within the road corridor must be a minimum of CBR15 and fill under proposed structures are to be a minimum of CBR30.
5. Filling and testing of proposed dilling shall be carried out in accordance with AS 3798 and Balonne Shire council requirements.
6. Test results are to be made available to the superintendent upon request.
7. all imported fill material to be minimum CBR15 material, placed in maximum 150mm layers and placed at optimum moisture content and compacted moisture content and compacted in accordance with AS3798 table 5.1
8. All fill material won from site must be a minimum of CBR15 and placed maximum 150mm layers and placed at optimum moisture content and compacted in accordance with AS3798 table 5.1
9. all fill material must be free of rubbish and organic material.
10. Materials unsuitable for use as fill include organic soils, silts, contaminated soils and materials prone to dissolving (discursive soils)
11. A topsoil depth of 75mm has been used to determine the earthwork quantities. The contractor is to satisfy himself of the accuracy of these quantities and make any necessary allowance if he disagrees with them.

| | | | | |
|-----|-----|----------------------------|----------|----------|
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| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |
| Rev | By | Description | Approved | Date |

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0m 5.0 10 15 20 1:500 AT A1

Designed
BJS
Drafter
GCC
Check
LOD
Approved

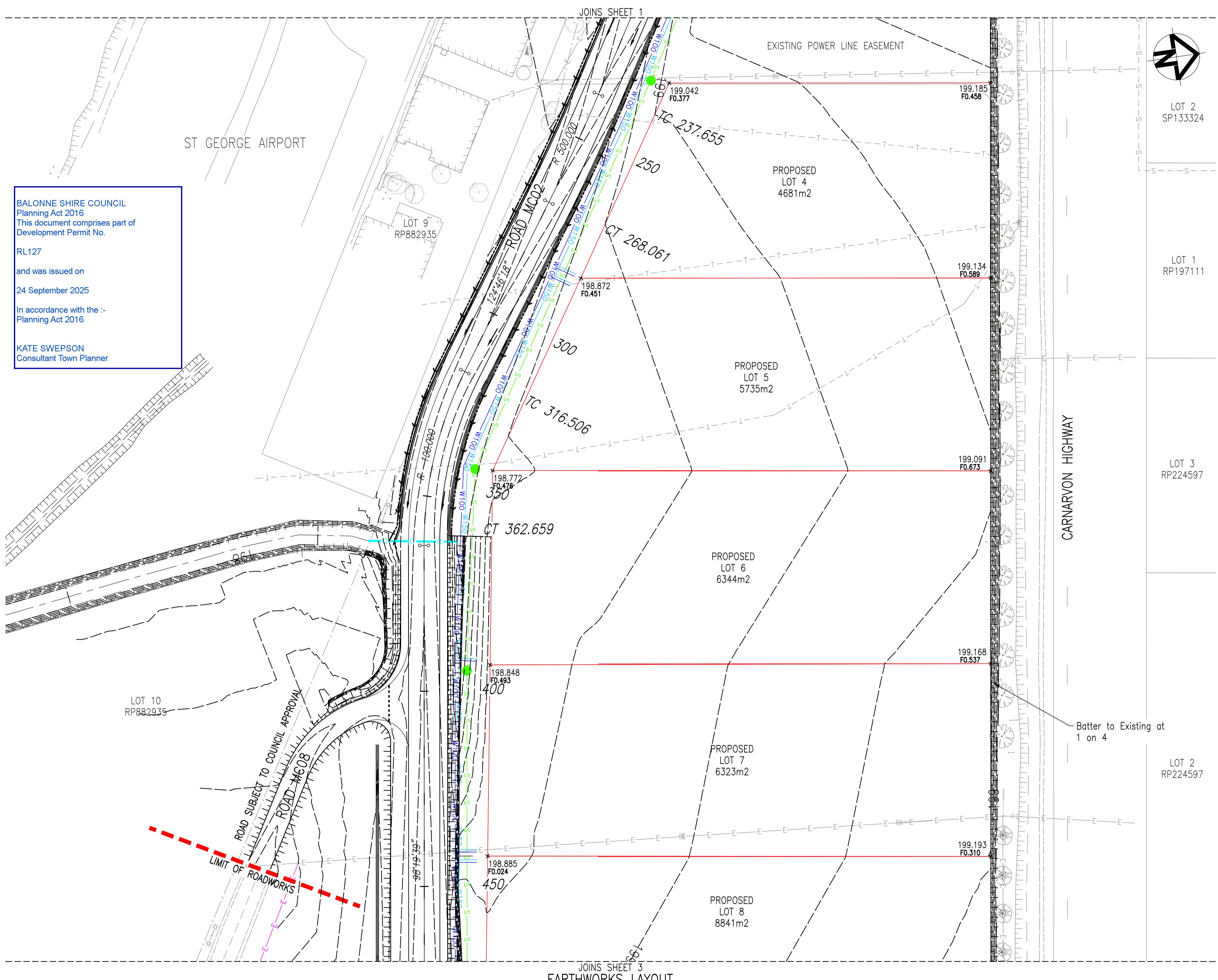
**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
EARTHWORKS LAYOUT PLAN
SHEET 1 OF 3**

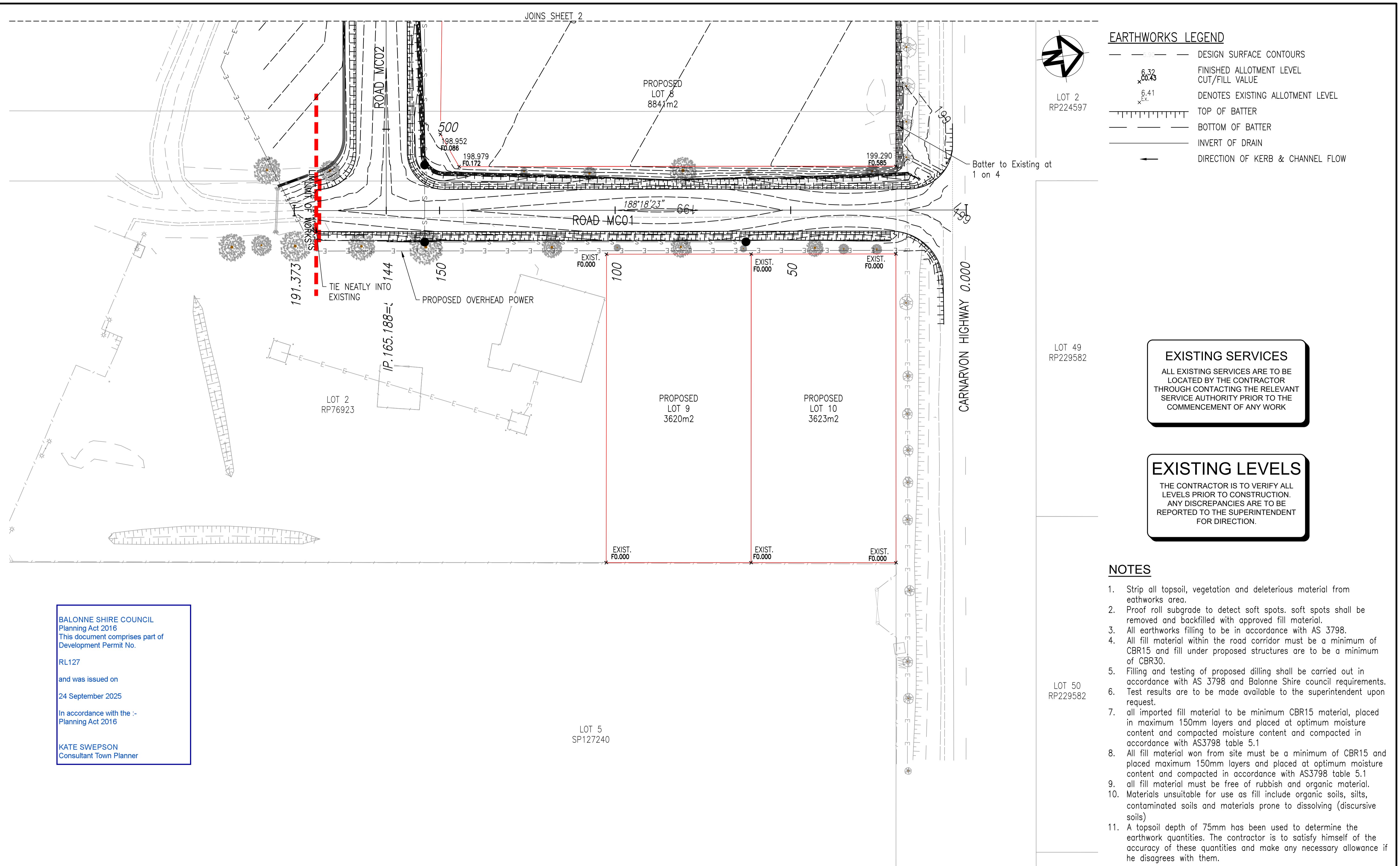
Drawing No.
22.1003-007

Sheet 7 of 42

Client
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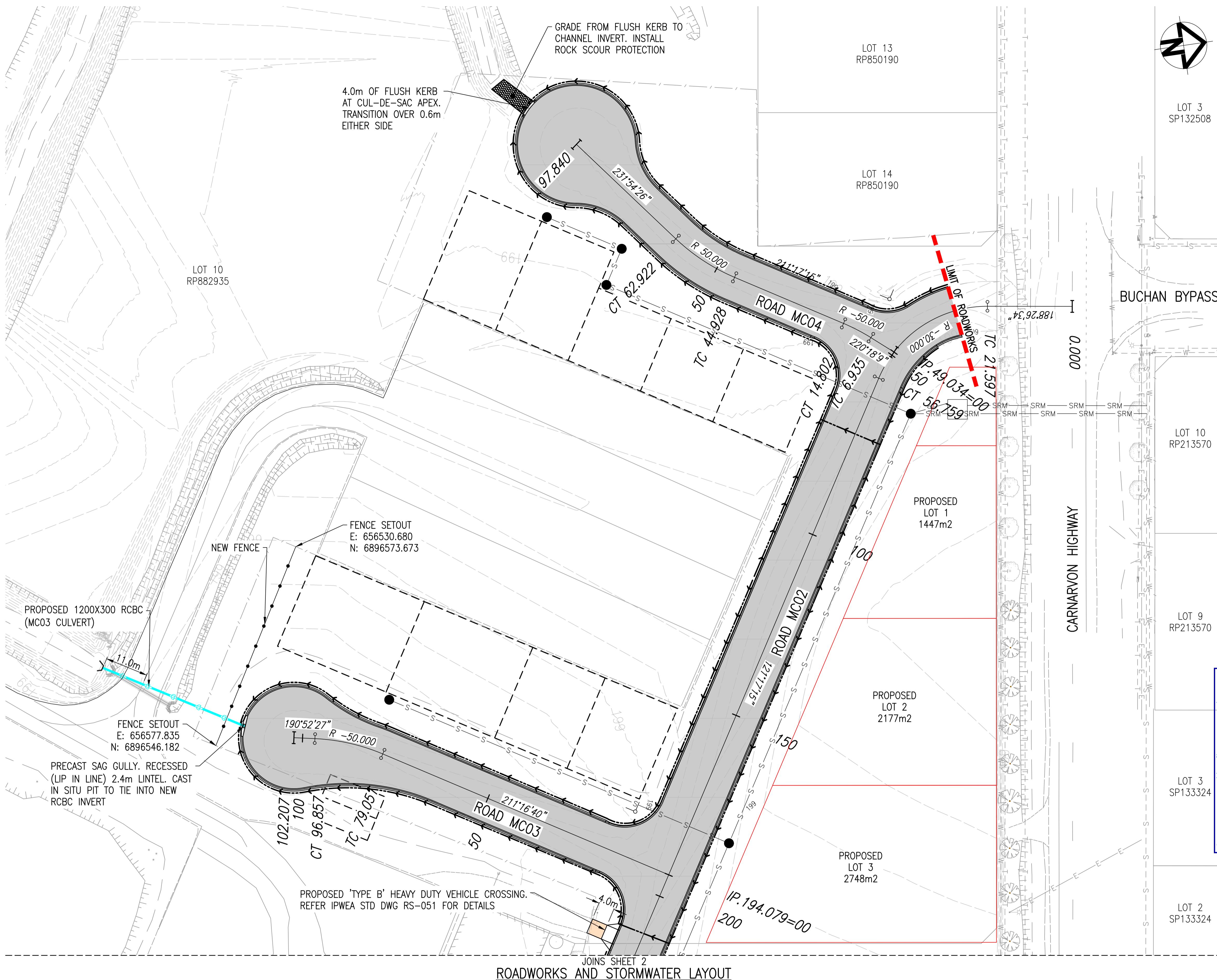
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EARTHWORKS LAYOUT
SCALE 1:500

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| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | Proterra Group 31 GLASSER STREET GOONDIWINDI QLD 4390 ABN: 82 626 886 771 TEL: (07) 4671 5701 WWW.PROTERRAGROUP.COM.AU | Designed | BJS | | | | | | | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | Drafter | GCC | | | | | | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | Proterra Group 31 GLASSER STREET GOONDIWINDI QLD 4390 ABN: 82 626 886 771 TEL: (07) 4671 5701 WWW.PROTERRAGROUP.COM.AU | Check | LOD | | | | | | | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | Approved | | | | | | | | | | |
| Rev | By | Description | Approved | Date | A1 ORIGINAL SIZE | | | | Client | BALONNE SHIRE COUNCIL | | | | Revision. | 6 | |



ROADWORKS LEGEND

- TYPE M3 MOUNTABLE KERB IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-080
- DOUBLE STREET NAME PLATE
- DIRECTION OF KERB & CHANNEL FLOW
- PROPOSED PAVEMENT REFER PAVEMENT DESIGN FOR DETAILS
- PROPOSED CONCRETE DRIVEWAY REFER PAVEMENT DESIGN FOR DETAILS
- SUBSOIL DRAIN IN ACCORDANCE WITH RS-140 (INDICATIVE LOCATION ONLY)

ROADWORKS NOTES

- ALL WORKS TO BE CONSTRUCTED IN ACCORDANCE WITH RELEVANT LATEST ISSUE COUNCIL STANDARD DRAWINGS AND SPECIFICATIONS, UNLESS NOTED OTHERWISE.
- FOR ROAD GRADING, LEVELS AND TYPICAL SECTIONS, REFER ROAD LONGITUDINAL/CROSS SECTION AND ROADWORKS DETAILS DRAWINGS.
- ALL SUBSOIL DRAINS TO BE GRADED AT MINIMUM 0.5% FROM HIGH POINT TO OUTLET, UNLESS NOTED OTHERWISE.
- SUBSOIL DRAIN TO BE REVERSE GRADED AT MINIMUM 0.5% TO ENSURE POSITIVE GRADE.
- SUBSOIL IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-140.

STORMWATER LEGEND:

- S — S — S — SEWER MAIN
- D — D — D — STORMWATER DRAINAGE LINE
- D — D — 3/1 — STORMWATER MANHOLE (MANHOLE NO./LINE NO.)
- D — D — — — 1/12 — KERB INLET PIT (PIT NO./LINE NO.)
- D — D — — — EXISTING STORMWATER DRAINAGE LINE
- D — D — — — EXISTING STORMWATER MANHOLE (MANHOLE NO./LINE NO.)
- D — D — — — EXISTING KERB INLET PIT (PIT NO./LINE NO.)

STORMWATER NOTES

- ALL WORKS TO BE CONSTRUCTED IN ACCORDANCE WITH RELEVANT LATEST ISSUE COUNCIL STANDARD DRAWINGS AND SPECIFICATIONS, UNLESS NOTED OTHERWISE.
- FOR STORMWATER SETOUT, LEVELS, PIPE LENGTHS, PIPE CLASS AND STRUCTURE TYPES, REFER STORMWATER LONGITUDINAL DRAWINGS.
- ALL PIPE AND BOX CULVERT CUTS TO BE MADE USING A MASONRY SAW.
- ALL TRENCHES UNDER ROAD PAVEMENT (INCLUDING FUTURE) TO BE BACKFILLED WITH CRUSHER DUST TO SUBGRADE LEVEL. REFER PROJECT SPECIFICATION FOR COMPACTION AND COMPLIANCE TESTING REQUIREMENTS.

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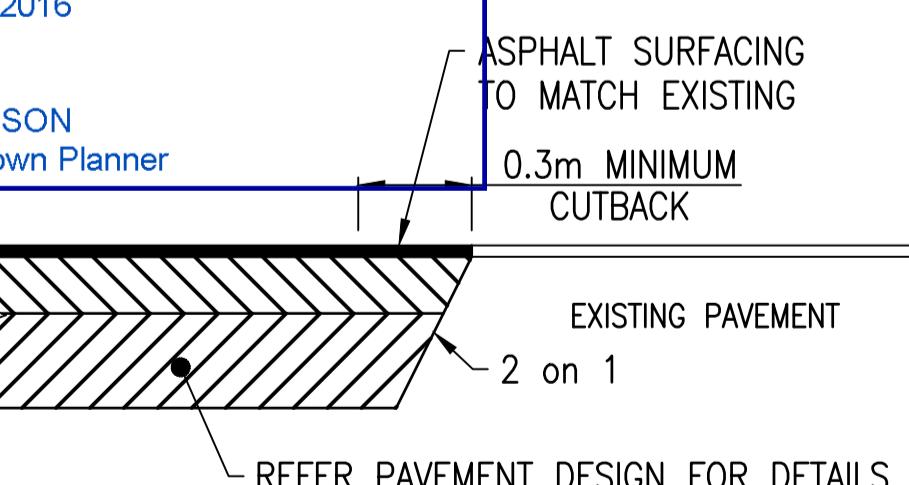
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Consultant Town Planner



TYPICAL PAVEMENT CUTBACK DETAIL
SCALE 1:20

JOINS SHEET 2

ROADWORKS AND STORMWATER LAYOUT

SCALE 1:500

| | | | | |
|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |
| Rev | By | Description | Approved | Date |

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A1 ORIGINAL SIZE

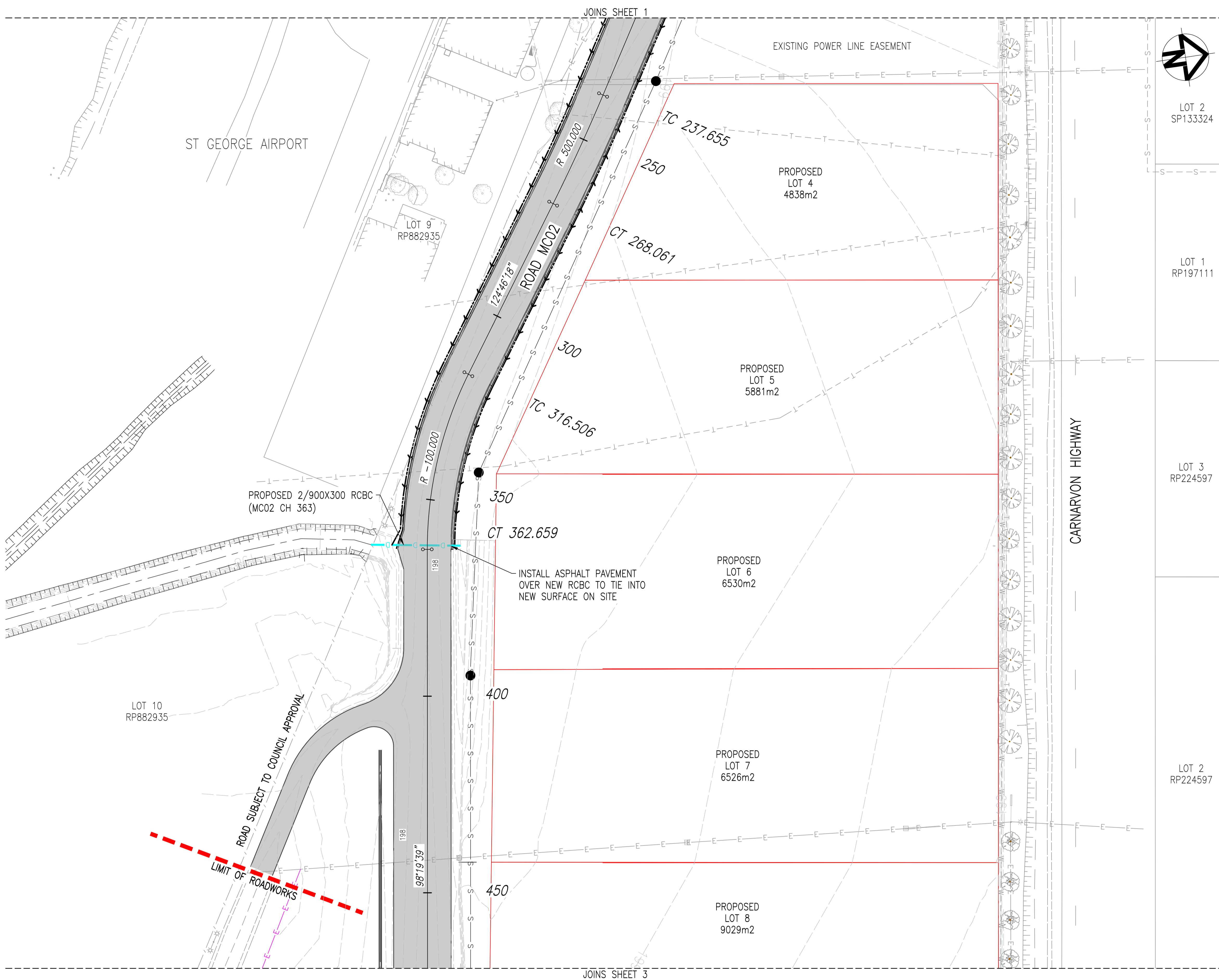
ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
ROADWORKS AND STORMWATER PLAN
SHEET 1 OF 3

Client
BALONNE SHIRE COUNCIL

Revision.
6

Drawing No.
22.1003-010

Sheet 10 of 42



ROADWORKS LEGEND

PE M3 MOUNTABLE KERB IN ACCORDANCE
WITH IPWEA STANDARD DRAWING RS-080

DOUBLE STREET NAME PLATE

DIRECTION OF KERB & CHANNEL FLOW

EXPOSED PAVEMENT
REFER PAVEMENT DESIGN FOR DETAILS

EXPOSED CONCRETE DRIVEWAY
REFER PAVEMENT DESIGN FOR DETAILS

BSOIL DRAIN IN ACCORDANCE WITH RS-140
INDICATIVE LOCATION ONLY)

ROADWORKS NOTES

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4. SUBSOIL DRAIN TO BE REVERSE GRADED AT MINIMUM 0.5% TO ENSURE POSITIVE GRADE.
5. SUBSOIL IN ACCORDANCE WITH IPWEA STANDARD DRAWING RS-140.

STORMWATER LEGEND:

| | |
|---|---|
| — S — S — S — | SEWER MAIN |
| — D — D — D — | STORMWATER DRAINAGE LINE |
| — D — D — 3/1 — D — | STORMWATER MANHOLE (MANHOLE NO./LINE NO.) |
|  | KERB INLET PIT (PIT NO./LINE NO.) |
| — — D — — D — | EXISTING STORMWATER DRAINAGE LINE |
| — — D — — Ex.6/1 — | EXISTING STORMWATER MANHOLE (MANHOLE NO./LINE NO.) |
|  | EXISTING KERB INLET PIT (PIT NO./LINE NO.) |

STORMWATER NOTES

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2. FOR STORMWATER SETOUT, LEVELS, PIPE LENGTHS, PIPE CLASS AND STRUCTURE TYPES, REFER STORMWATER LONGITUDINAL DRAWINGS.
3. ALL PIPE AND BOX CULVERT CUTS TO BE MADE USING A MASONRY SAW.
4. ALL TRENCHES UNDER ROAD PAVEMENT (INCLUDING FUTURE) TO BE BACKFILLED WITH CRUSHER DUST TO SUBGRADE LEVEL. REFER PROJECT SPECIFICATION FOR COMPACTION AND COMPLIANCE TESTING REQUIREMENTS.

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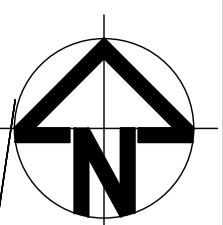
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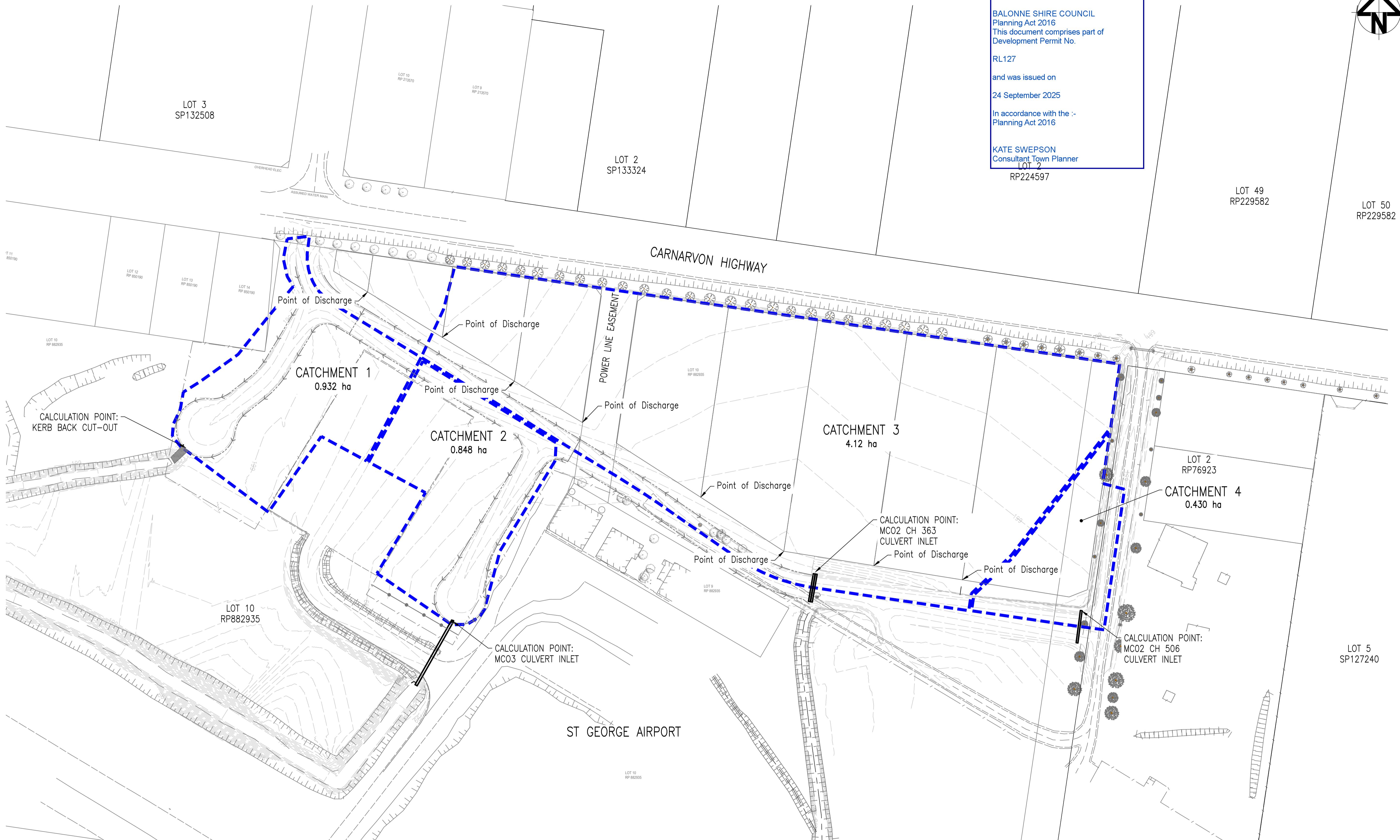
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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | | | | | | | | | | | | | |
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| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | | | | | | | | | | | | | |
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| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | | | | | | | | | | | | | |
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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | Scales: | 0m | 10 | 20 | 30 | 40 | 1:1000 AT A1 | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION STORMWATER CATCHMENT PLAN | | Drawing No. 22.1003-013 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | Drafter | WF | | | | | | | | Sheet 13 of 42 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | Check | LOD | | | | | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | Approved | | | | | | | | | |
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| Rev | By | Description | Approved | Date | | | | | A1 ORIGINAL RP55377E | | | | | | | | |

| LOCATION | | SUB-CATCHMENT RUNOFF | | | | | INLET DESIGN | | | | | | | DRAIN DESIGN | | | | | | | HEAD LOSSES | | | PART FULL | | DESIGN LEVELS | | | | | | | | | | | | | | | | | | | |
|------------------|---------------|---|----------------------------|--------------------------------|--------------------------------|-----------------------------|---|---|------------------------|-------------------|-------------------|---------------------------------|--------------------------|--------------------------|------------|-------------|------------------------------|------------------------|----------------------------------|-------------------------------------|-----------------------------|--------------------------------|-----------------------------|-----------------------------|-------------------------------------|------------------------------|----------------------|------------------------------|------------------------------|-----------------------------------|----------------|-----------------|-----------------------------------|--------------------------------|------------------------------|----------------------------|------------------------------|----------------------|----------------------|----------------------|----------------------|--------------------------|---------------------------|------------------|----------------|
| STRUCTURE No. | DRAIN SECTION | SUB- CATCHMENT TIME OF CONC. min | RAINFALL mm/h | I SUB- CATCHMENT AREA | A SUB- CATCHMENT AREA | CA EQUIVALENT ha AREA | Qc SUB- CATCHMENT DISCHARGE L/s | Qa FLOW IN K&C (INC. BYPASS) L/s | Qa HALF ROAD L/s | m FLOW WIDTH m | m FLOW DEPTH m | m FLOW DxV m ² /s | ROAD GRADE % AT INLET | ROAD XFALL % AT INLET | INLET TYPE | INLET CURVE | Qg FLOW INTO L/s INLET | Qg PEAK FLOW L/s | Qb BYPASS STRUCTURE No. | Tc CRITICAL TIME min OF CONC. | I TOTAL (C x A) mm/hr | CA ha | Qrat Qf=Q/A | Q PIPE FLOW L/s | Vf=Q/A FULL PIPE m/s VELOCITY | Qcap PIPE CAPACITY L/s | Vcap m/s VELOCITY | Vt m/s TRAVEL VELOCITY | h U/S HEAD LOSS | K U/S HEAD LOSS COEFFICIENT | h W LOSS | W/S E LOSS | W/S E PIPE FRICTION SLOPE % | Sf PIPE FRICTION SLOPE % | hf HEAD LOSS | d NORMAL m DEPTH | v NORMAL m DEPTH VEI. | z PIPE U/S H.G.L. | z PIPE D/S H.G.L. | z PIPE U/S H.G.L. | z PIPE D/S H.G.L. | z W/S E PIPE LEVEL | z W/S E GRADE LEVEL | STRUCTURE No. | |
| 1-1 | 1-1 to 1-2 | 9 | 103 (minor) 249 (major) | 0.932 | 0.546 (minor) 0.76 (major) | 156 (minor) 527 (major) | 156 (minor) 527 (major) | | | | | | 1 | 3 | NODE | | 156 (minor) 527 (major) | 0 | | 9 | 103 (minor) 249 (major) | 0.546 (minor) 0.76 (major) | 156 (minor) 527 (major) | 156 (minor) 527 (major) | 9.527 | | | | | | | | | 19.870 (minor) | 1-1 | | | | | | | | | | |
| 1-2 | | | | | | | | | | | | | 1 | 3 | NODE | | | | | | | | | | | | | | | | | 19.84 (minor) | 1-2 | | | | | | | | | | | | |
| 2-1 | 2-1 to 2-2 | 8 | 108 (minor) 262 (major) | 0.848 | 0.548 (minor) 0.763 (major) | 165 (minor) 555 (major) | 165 (minor) 555 (major) | | | | | | 0.03 | 3 | SAL3D | | 165 (minor) 555 (major) | 0 | | 8 | 108 (minor) 262 (major) | 0.548 (minor) 0.763 (major) | 165 (minor) 555 (major) | 165 (minor) 555 (major) | 40.191 | 0.1 | 1200x300 | RCBC | 0.51 (minor) 1.71 (major) | 257 | 0.79 | 2 | 0.013 (minor) 0.012 (major) | 0.026 (minor) 0.025 (major) | 0.2 (minor) 0.25 (major) | 0.5 (minor) 0.4 (major) | 0.6 (minor) 0.71 (major) | 19.816 | 19.836 (minor) | 19.822 (minor) | 19.844 (minor) | 19.831 (major) | 19.843 (major) | 19.843 (minor) | 19.843 (major) |
| 2-2 | | | | | | | | | | | | | 1 | 3 | HW OUT | | | | | | | | | | | | | | | | | 19.824 (minor) | 2-2 | | | | | | | | | | | | |
| 3-1 | 3-1 to 3-2 | 23 | 64 (minor) 157 (major) | 4.12 | 2.129 (minor) 2.965 (major) | 381 (minor) 1297 (major) | 381 (minor) 1297 (major) | | | | | | 1 | 3 | HW IN | | 381 (minor) 1297 (major) | 0 | | 23 | 64 (minor) 157 (major) | 2.129 (minor) 2.965 (major) | 381 (minor) 1297 (major) | 381 (minor) 1297 (major) | 15.404 | 0.1 | (2x)900x300 | RCBC | 0.71 (minor) 2.4 (major) | 413 | 0.77 | 2 | 0.025 (minor) 0.016 (major) | 0.013 (minor) 0.008 (major) | 0.13 (minor) 0.08 (major) | 0.4 (minor) 0.5 (major) | 0.37 (minor) 0.08 (major) | 19.845 | 19.827 (minor) | 19.828 (minor) | 19.816 (minor) | 19.824 (minor) | 19.845 (major) | 19.845 (minor) | 19.845 (major) |
| 3-2 | | | | | | | | | | | | | 1 | 3 | HW OUT | | | | | | | | | | | | | | | | | 19.8196 (minor) | 3-2 | | | | | | | | | | | | |
| 4-1 | 4-1 to 4-2 | 25 | 61 (minor) 149 (major) | 0.43 | 0.222 (minor) 0.309 (major) | 38 (minor) 128 (major) | 38 (minor) 128 (major) | | | | | | 1 | 3 | HW IN | | 38 (minor) 128 (major) | 0 | | 25 | 61 (minor) 149 (major) | 0.222 (minor) 0.309 (major) | 38 (minor) 128 (major) | 38 (minor) 128 (major) | 17.823 | 0.08 | 900x300 | RCBC | 0.14 (minor) 0.47 (major) | 192 | 0.71 | 2 | 0.01 (minor) 0.011 (major) | 0.01 (minor) 0.006 (major) | 0.01 (minor) 0.06 (major) | 0.2 (minor) 0.4 (major) | 0.31 (minor) 0.38 (major) | 19.822 | 19.839 (minor) | 19.821 (minor) | 19.831 (minor) | 19.840 (major) | 19.840 (major) | 19.832 (minor) | 19.832 (major) |
| 4-2 | | | | | | | | | | | | | 1 | 3 | HW OUT | | | | | | | | | | | | | | | | | 19.826 (minor) | 4-2 | | | | | | | | | | | | |

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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | |
| Rev By Description Approved Date | | | | | Scales: | | | |
| | | | | | Client BAONNE SHIRE COUNCIL | | | Revision 6 |
| | | | | | A1 ORIGINAL SIZE | | | |

PAVEMENT DESIGN

ROAD MC01

**DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40**

BALONNE SHIRE COUNCIL
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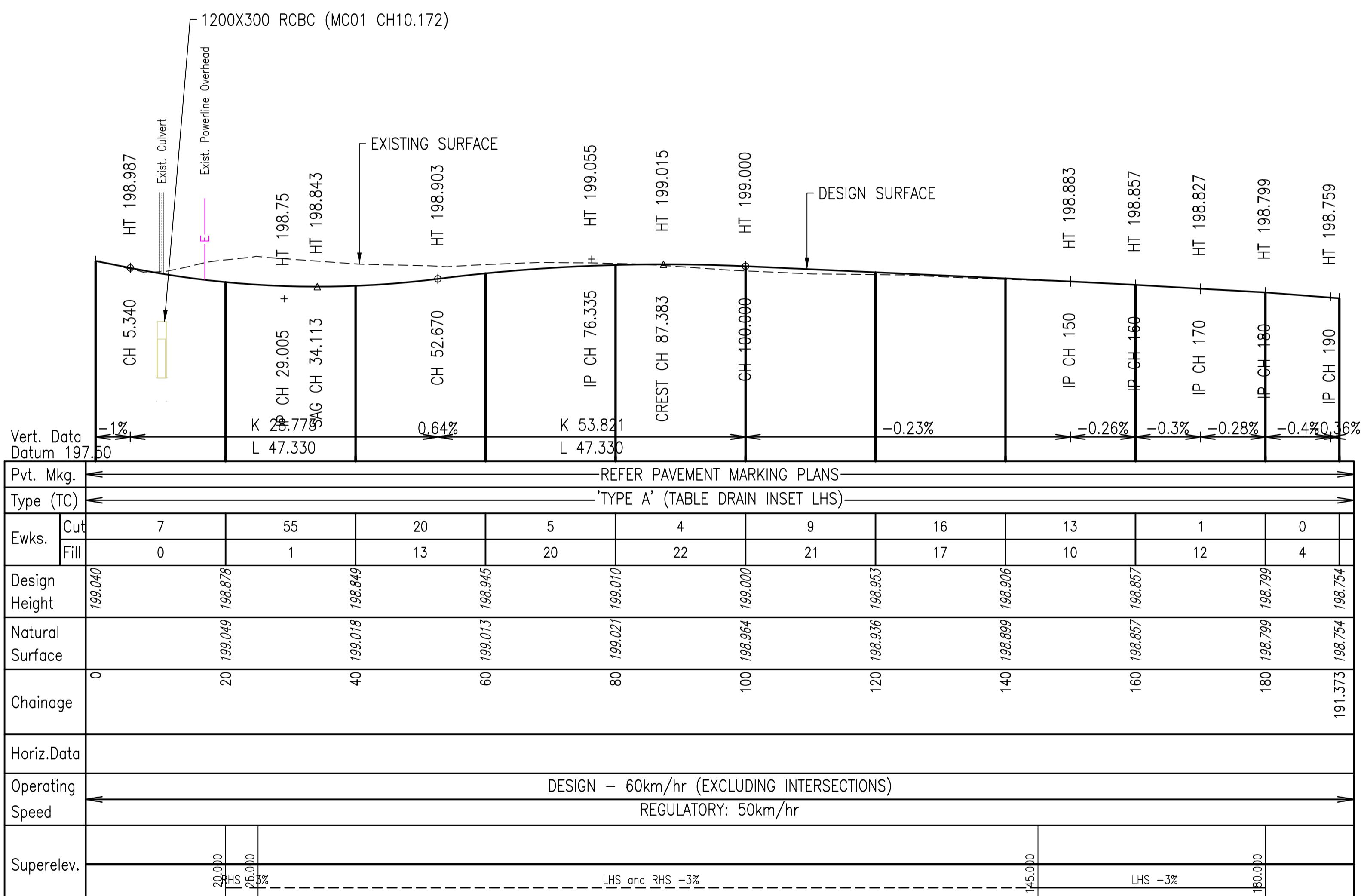
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24 September 2025

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KATE SWEPS
Consultant Town Planner



MC01 – LONGITUDINAL SECTION

SCALE HORZ. 1:500 VERT. 1:25

| | | | | | | | | | | | | | | | | | | | | | | | |
|-------------|---------|----------------------------|----------|----------|---|---|---|----------------------------------|--|--|-------------|---------|-----|--|--|------------|-------|-----|--|--|---|---|----------------------|
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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | <p>Scales:</p> <table border="1" data-bbox="4355 3988 4660 4042"><tr><td>1:500 AT A</td><td>Design</td><td>BJS</td><td></td><td></td></tr><tr><td>1:1000 AT A</td><td>Drafter</td><td>GCC</td><td></td><td></td></tr><tr><td>1:250 AT A</td><td>Check</td><td>LOD</td><td></td><td></td></tr></table> <p>Approved</p> | 1:500 AT A | Design | BJS | | | 1:1000 AT A | Drafter | GCC | | | 1:250 AT A | Check | LOD | | |  | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC01 LONGITUDINAL SECTION | Sheet 15 of 2 |
| 1:500 AT A | Design | BJS | | | | | | | | | | | | | | | | | | | | | |
| 1:1000 AT A | Drafter | GCC | | | | | | | | | | | | | | | | | | | | | |
| 1:250 AT A | Check | LOD | | | | | | | | | | | | | | | | | | | | | |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | | | | | | | | | | | | | | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | | | | | | | | | | | | | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | | | | | | | | | | | | | | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | | | | | | | | | | | | | | | | |
| Rev | By | Description | Approved | Date | | | | | | | | | | | | | | | | | | | |

PAVEMENT DESIGN

ROAD MC02

DOUBLE/DDOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

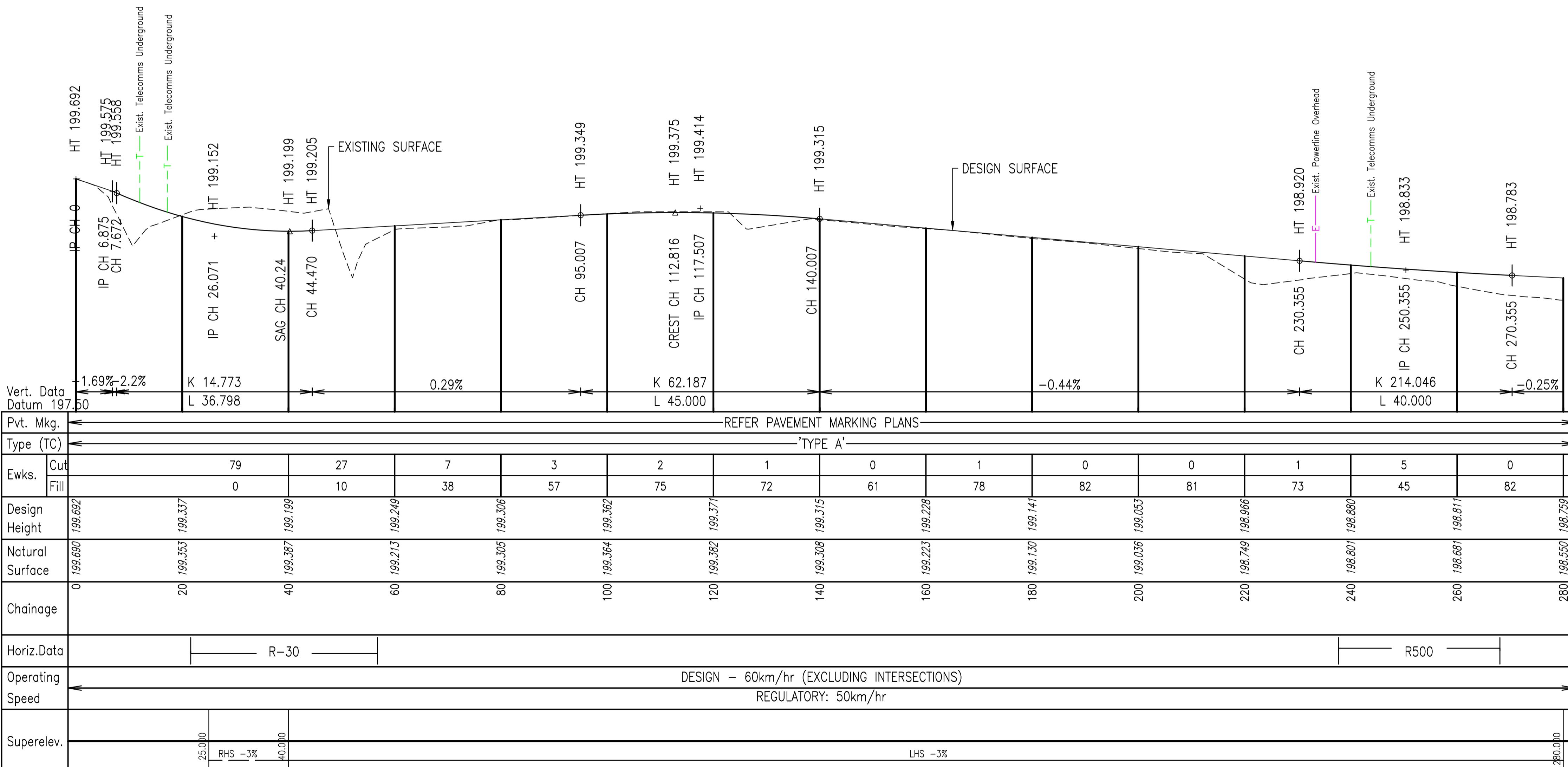
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MC02 – LONGITUDINAL SECTION

SCALE HORZ. 1:500 VERT. 1:25

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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | | | Scales: | Designed | BJS | | | | | |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | | | Drafter | GCC | | | | | <p>ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC02 LONGITUDINAL SECTION SHEET 1 OF 2</p> <p>Client: BALONNE SHIRE COUNCIL</p> | Drawing No. 22.1003-016 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | | | Check | LOD | | | | | | Sheet 16 of 42 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | | | Approved | | | | | | Revision. 6 | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | | | A1 ORIGINAL SIZE | | | | | | | |
| Rev | By | Description | Approved | Date | | | | | | | | | | | | | |

PAVEMENT DESIGN

ROAD MC02

**DOUBLE/DOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40**

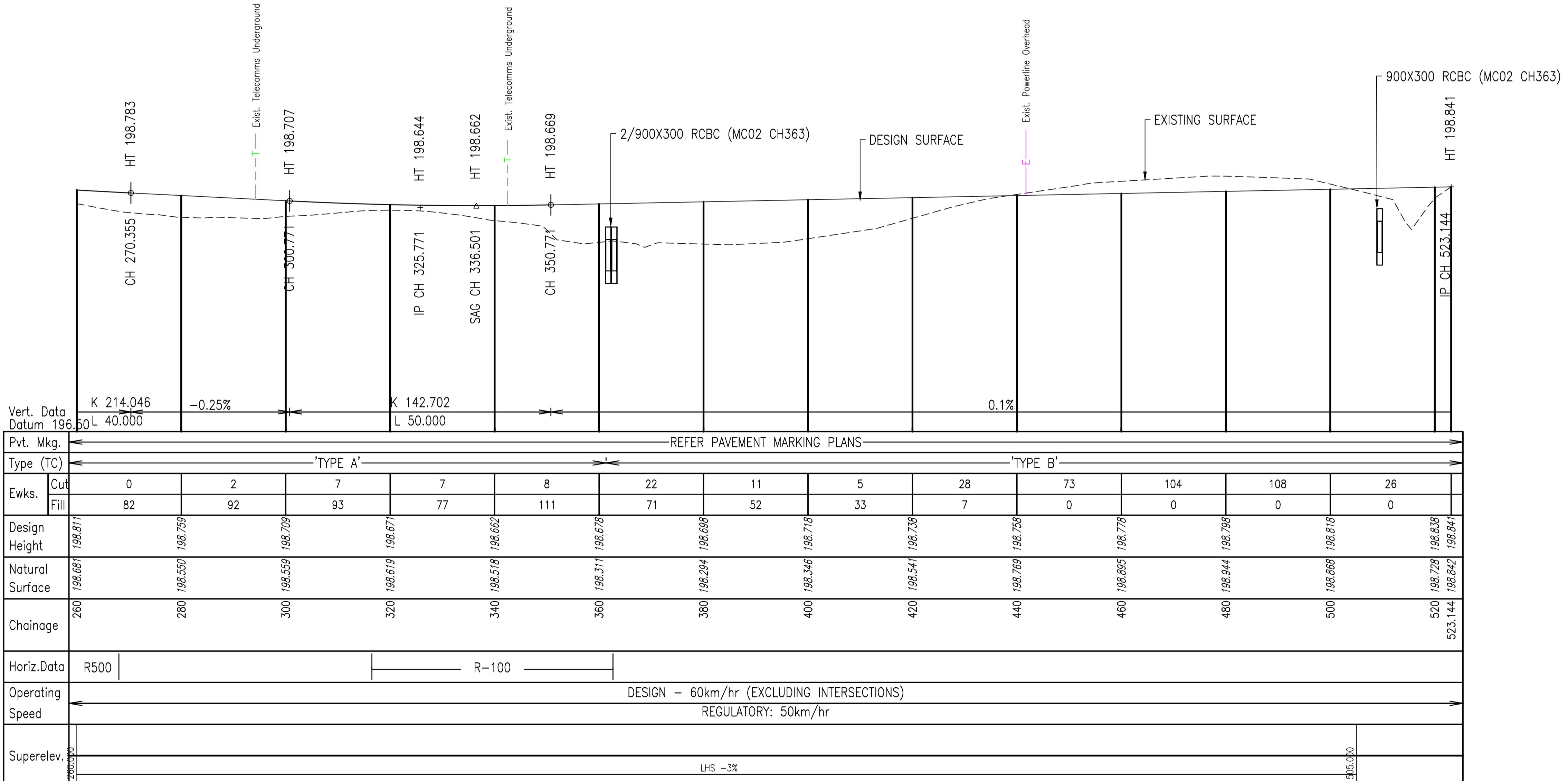
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MC02 – LONGITUDINAL SECTION

SCALE HORZ. 1:500 VERT. 1:25

| | | | | | | | |
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| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | Designed BJS | 22.1003-017 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | Drafter GOC | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | Check LOD | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | Sheet 1 of 42 |
| Rev | By | Description | Approved | Date | | | |

PAVEMENT DESIGN

ROAD MC03

DOUBLE/DIDOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

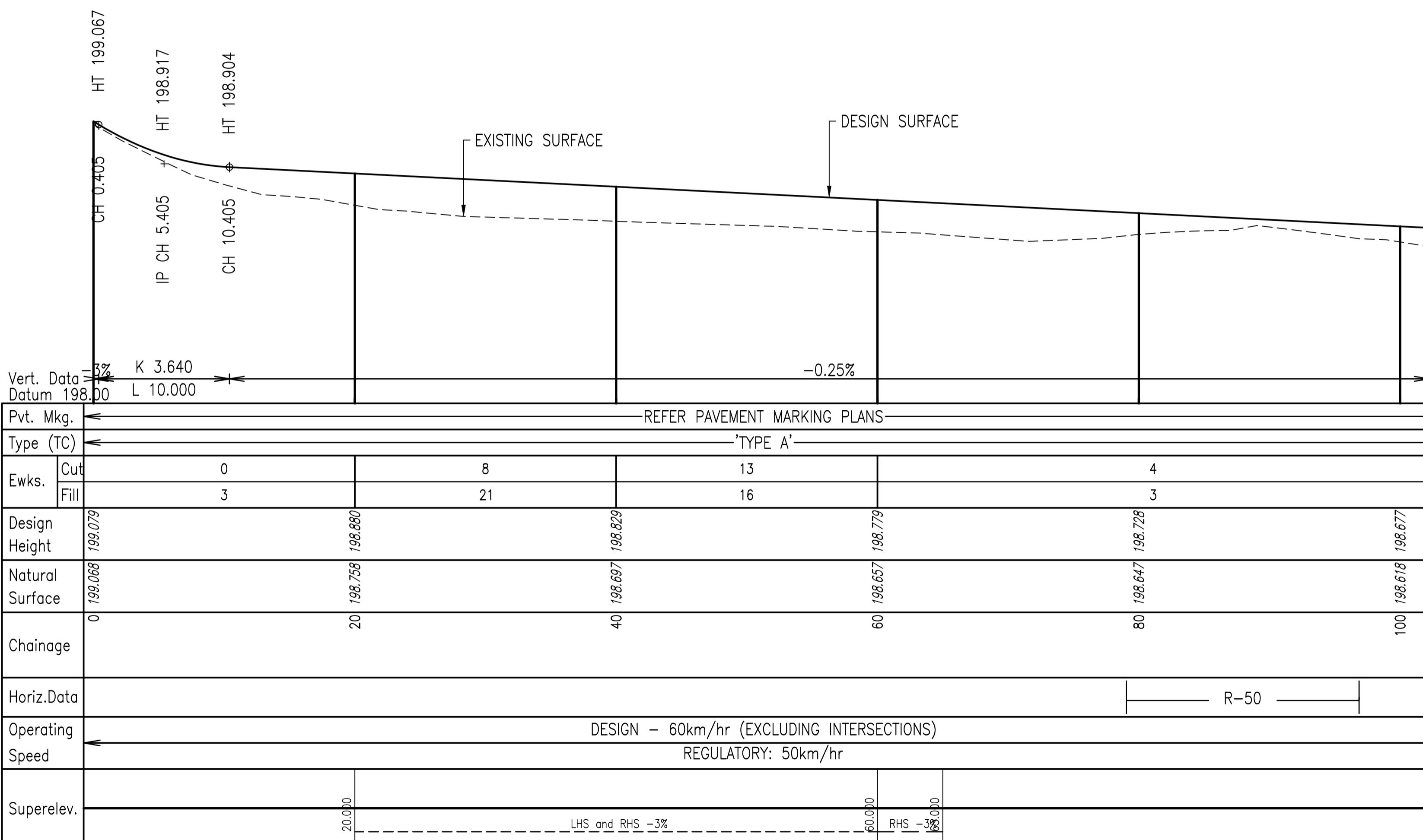
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MC03 – LONGITUDINAL SECTION

SCALE HORZ. 1:500 VERT. 1:25

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|-----|-----|----------------------------|----------|----------|----------------------------|--|--|---|-----|------------|------------|---|--|-----------------------------------|--|
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | Scales: | | Designed | BJS | | <p>ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC03 LONGITUDINAL SECTION</p> | Drawing No. 22.1003-018 | |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | 0m | 5.0 | 10 | 15 | 20 | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | 0m | 0.2 | 0.4 | 0.6 | 0.8 | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | 0m | 1.0 | 1.25 AT A1 | 1.50 AT A1 | 1.75 AT A1 | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | Approved | | | | | | Sheet 18 of 42 | |
| Rev | By | Description | Approved | Date | | | | Client | | | | | | Revision. 6 | |

PAVEMENT DESIGN

ROAD MC04

DOUBLE/DIDOUBLE SEAL
150mm THICK CBR 60 STABILISED ROAD BASE
(CONTRACTOR TO CONFIRM STABILISATION)
SUBGRADE LEVEL CBR 40

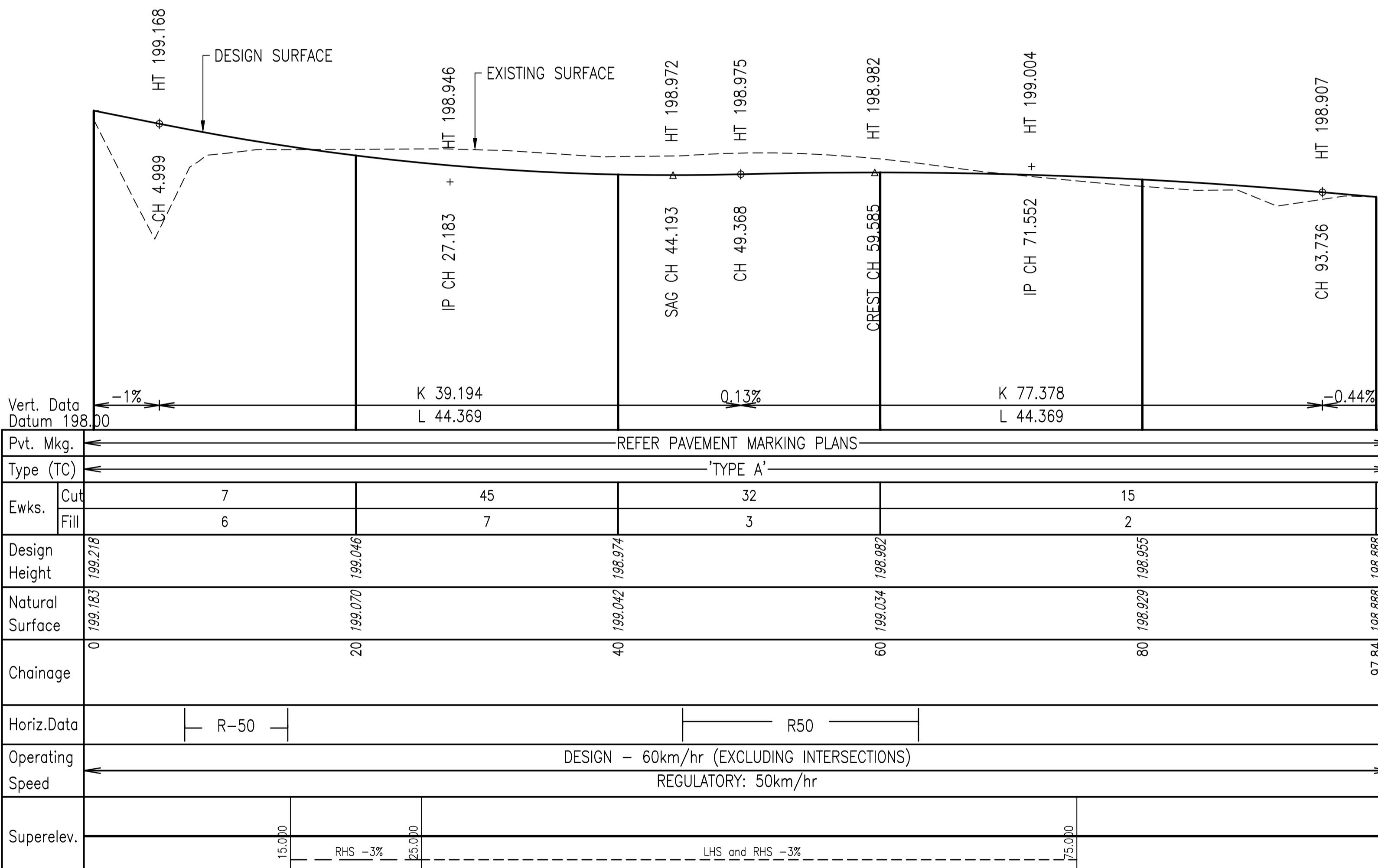
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MC04 – LONGITUDINAL SECTION

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|-----|-----|----------------------------|----------|----------|--|----------|---|---|--------|-----------------------|----|---|----|--------------------|--|
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | Scales: | 0m | 5.0 | 10 | 15 | 20 | 1:500 AT A1 | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC04 LONGITUDINAL SECTION |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | The information and data recorded herein and it's presented format ("This Drawing"), is Copyright and the property of Proterra Group. This Drawing may not be used, copied or reproduced in whole or part for any purpose other than the consent by which it is supplied by Proterra Group. Proterra Group is not responsible for any document or part of this Drawing unless it is identical to the document provided herewith. The terms printed on this Drawing are part of the data and documentation and must be attached or affixed to any reproduction of the data or accompanying documents. | Designed | BJS | | | | | | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | Drafter | GCC | | | | | | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | Check | LOD | | | | | | | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | Approved | | | | | | | | | Sheet 19 of 42 | |
| Rev | By | Description | Approved | Date | A1 ORIGINAL SIZE | | | | Client | BALONNE SHIRE COUNCIL | | | | Revision. 6 | |

CONTROL LINE MC01
X = 656946.214
Y = 6896652.696
Z = 198.945

| Datum 198.00 | | -3.00% 1 in -6 | | -3.00% | | -3.00% | | 1 in -6 -3.00% | |
|------------------|--|----------------|---------|---------|--|--------|---------|----------------|--|
| DESIGN HEIGHT | | -8.619 | 198.613 | 198.613 | | -8.500 | 198.611 | 198.554 | |
| EXISTING SURFACE | | -7.000 | 198.564 | 198.599 | | -6.000 | 198.538 | 198.765 | |
| OFFSETS | | -4.000 | 198.715 | 198.825 | | 0.000 | 199.013 | 198.945 | |

CHAINAGE 60.000

CONTROL LINE MC01
X = 656949.104
Y = 6896672.486
Z = 198.849

| Datum 198.00 | | -3.00% 1 in -6 | | -3.00% | | -3.00% | | 1 in -6 -3.00% | |
|------------------|--|----------------|---------|---------|--|--------|---------|----------------|--|
| DESIGN HEIGHT | | -8.645 | 198.530 | 198.530 | | -8.500 | 198.530 | 198.457 | |
| EXISTING SURFACE | | -7.000 | 198.527 | 198.502 | | -6.000 | 198.548 | 198.669 | |
| OFFSETS | | -4.000 | 198.740 | 198.729 | | 0.000 | 199.018 | 198.849 | |

CHAINAGE 40.000

CONTROL LINE MC01
X = 656951.993
Y = 6896692.276
Z = 198.878

| Datum 198.00 | | -3.00% | | 1 in -6 | | -3.00% | | |
|------------------|--|--------|---------|---------|--|--------|---------|---------|
| DESIGN HEIGHT | | 0.000 | 199.049 | 198.878 | | 4.000 | 198.767 | 198.758 |
| EXISTING SURFACE | | 6.000 | 198.773 | 198.698 | | 8.270 | 198.772 | 198.319 |
| OFFSETS | | 9.770 | 198.695 | 198.274 | | 10.564 | 198.671 | 198.671 |

CHAINAGE 20.000

CONTROL LINE MC01
X = 656952.514
Y = 6896695.845
Z = 198.898

| Datum 198.00 | | -3.00% | | 1 in -6 | | -3.00% | | |
|------------------|--|--------|---------|---------|--|--------|---------|---------|
| DESIGN HEIGHT | | 0.000 | 199.022 | 198.898 | | 4.000 | 198.834 | 198.778 |
| EXISTING SURFACE | | 6.000 | 198.801 | 198.718 | | 8.506 | 198.672 | 198.300 |
| OFFSETS | | 10.006 | 198.630 | 198.255 | | 10.784 | 198.644 | 198.644 |

CHAINAGE 16.393

CONTROL LINE MC01
X = 656934.657
Y = 6896573.535
Z = 198.906

| Datum 198.00 | | -3.00% 1 in -6 | | -3.00% | | -3.00% | | 1 in -6 -3.00% | |
|------------------|--|----------------|---------|---------|--|--------|---------|----------------|--|
| DESIGN HEIGHT | | -8.619 | 198.613 | 198.554 | | -7.000 | 198.538 | 198.765 | |
| EXISTING SURFACE | | -7.000 | 198.553 | 198.538 | | 6.000 | 198.576 | 198.765 | |
| OFFSETS | | 4.000 | 198.749 | 198.825 | | 7.384 | 198.488 | 198.535 | |

CHAINAGE 140.000

CONTROL LINE MC01
X = 656937.546
Y = 6896593.325
Z = 198.953

| Datum 198.00 | | -3.00% 1 in -6 | | -3.00% | | -3.00% | | 1 in -6 -3.00% | |
|------------------|--|----------------|---------|---------|--|--------|---------|----------------|--|
| DESIGN HEIGHT | | -8.645 | 198.530 | 198.457 | | -7.000 | 198.592 | 198.560 | |
| EXISTING SURFACE | | -7.000 | 198.592 | 198.607 | | 6.000 | 198.569 | 198.776 | |
| OFFSETS | | 4.000 | 198.740 | 198.829 | | 7.384 | 198.488 | 198.535 | |

CHAINAGE 120.000

CONTROL LINE MC01
X = 656940.436
Y = 6896613.115
Z = 199.000

| Datum 198.00 | | -3.00% 1 in -6 | | -3.00% | | -3.00% | | 1 in -6 -3.00% | |
|------------------|--|----------------|---------|---------|--|--------|---------|----------------|--|
| DESIGN HEIGHT | | -8.675 | 198.696 | 198.696 | | -7.000 | 198.592 | 198.607 | |
| EXISTING SURFACE | | -8.500 | 198.694 | 198.608 | | 6.000 | 198.593 | 198.773 | |
| OFFSETS | | -7.000 | 198.677 | 198.653 | | 4.000 | 198.773 | 198.833 | |

CHAINAGE 100.000

CONTROL LINE MC01
X = 656943.325
Y = 6896632.905
Z = 199.010

| Datum 198.00 | | -3.00% 1 in -6 | | -3.00% | | -3.00% | | 1 in -6 -3.00% | |
|------------------|--|----------------|---------|---------|--|--------|---------|----------------|--|
| DESIGN HEIGHT | | -8.668 | 198.702 | 198.702 | | -7.000 | 198.685 | 198.663 | |
| EXISTING SURFACE | | -8.500 | 198.701 | 198.618 | | 6.000 | 198.665 | 198.773 | |
| OFFSETS | | -7.000 | 198.677 | 198.830 | | 4.000 | 198.785 | 198.880 | |

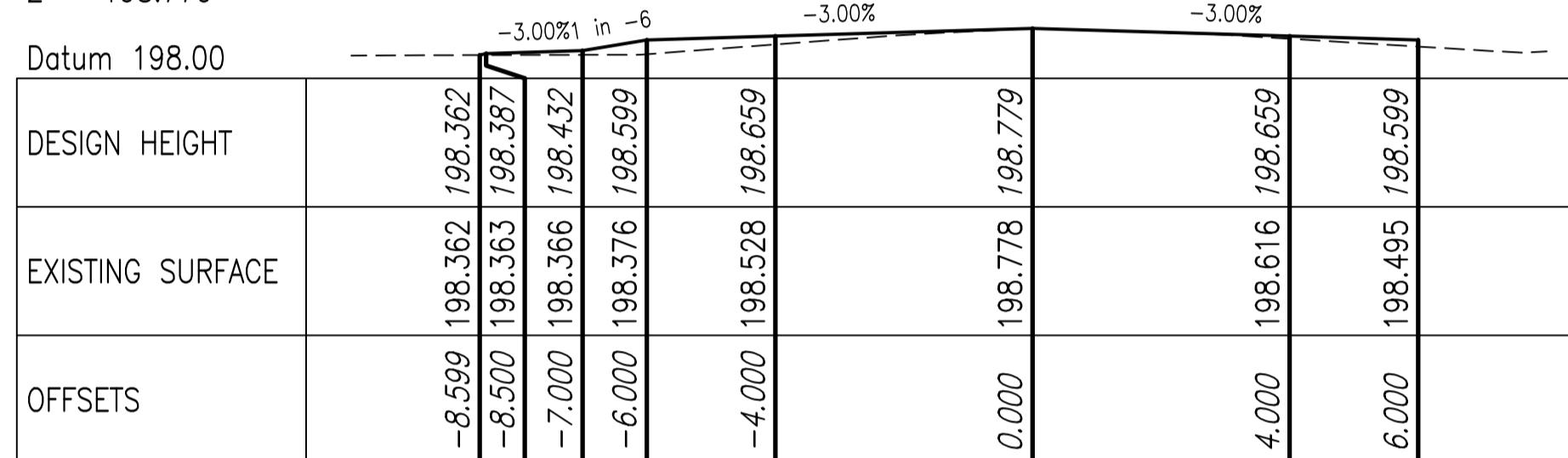
CHAINAGE 80.000

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|---|---------|---|------------------------|
| Scales: | | | |
| 0m | 5.0 | 10 | 15 20 |
| 0m | 0.4 | 0.8 | 1.2 1.6 |
| 1:500 AT A1 | | 1:40 AT A1 | |
| 4.000 | 198.783 | 198.890 | 7.428 198.620 198.582 |
| 6.000 | 198.675 | 198.830 | 8.928 198.722 198.537 |
| 7.126 | 198.614 | 198.642 | 9.354 198.750 198.750 |
| 8.626 | 198.726 | 198.597 | 10.787 198.821 198.821 |
| 8.930 | | | |

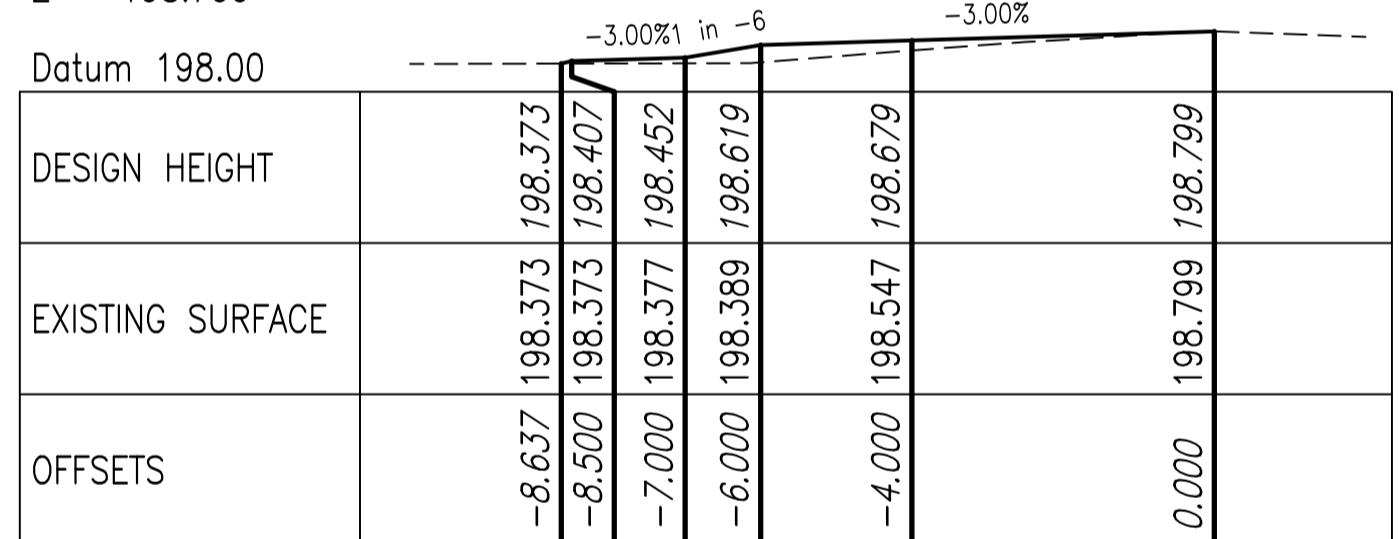
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Development Permit No.
RL127
and was issued on
24 September 2025
In accordance with the :-
Planning Act 2016
KATE SWEPSON
Consultant Town Planner

CONTROL LINE MC01
X = 656928.156
Y = 6896529.007
Z = 198.779



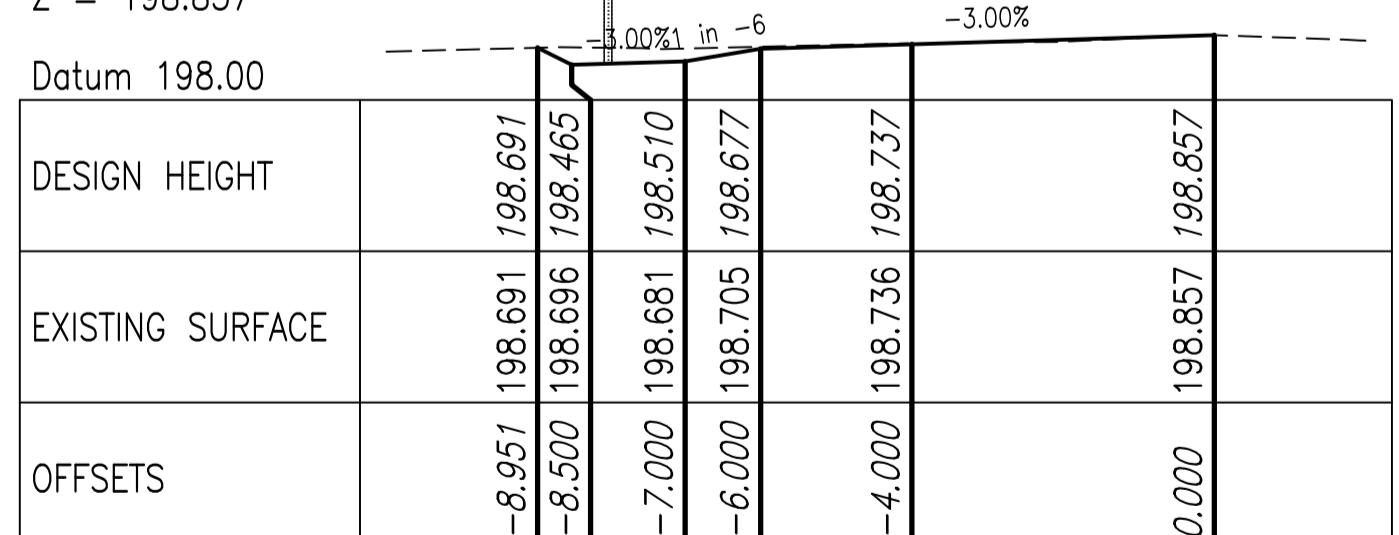
CHAINAGE 185.000

CONTROL LINE MC01
X = 656928.879
Y = 6896533.954
Z = 198.799



CHAINAGE 180.000

CONTROL LINE MC01
X = 656931.768
Y = 6896553.745
Z = 198.857



CHAINAGE 160.000

| | | | | | | |
|-----|-----|----------------------------|----------|----------|--|--|
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| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | |
| Rev | By | Description | Approved | Date | | |



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|---|--|-----|-----|---|-------------|-------------|--|
| Scales: | 0m | 1.0 | 2.0 | 3.0 | 4.0 | 1:100 AT A1 | |
| Drafter | GCC | | | | | | |
| Check | LOD | | | | | | |
| Approved | | | | | | | |
| Client | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC01 CROSS SECTIONS SHEET 2 OF 2 | | | | Drawing No. | 22.1003-021 | |
| | | | | | Sheet | 21 of 42 | |
| | | | | | Revision. | 6 | |
| | A1 ORIGINAL SIZE | | | | | | |

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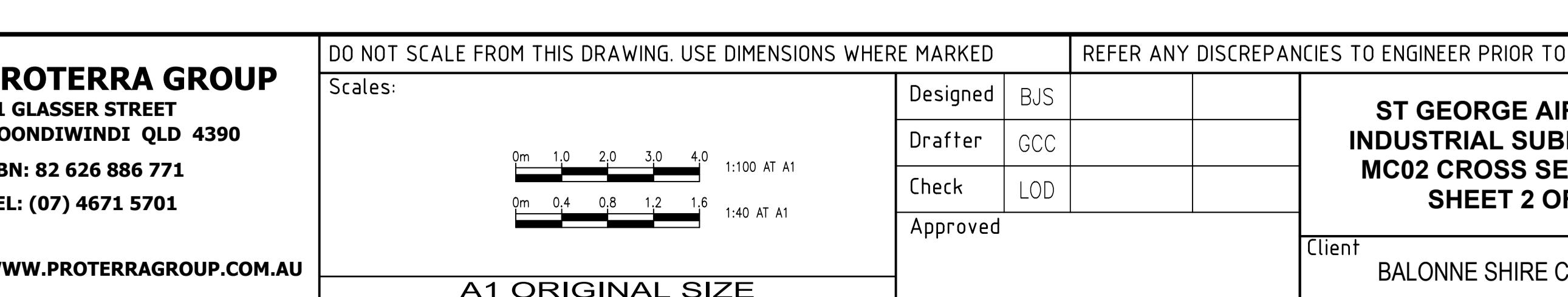
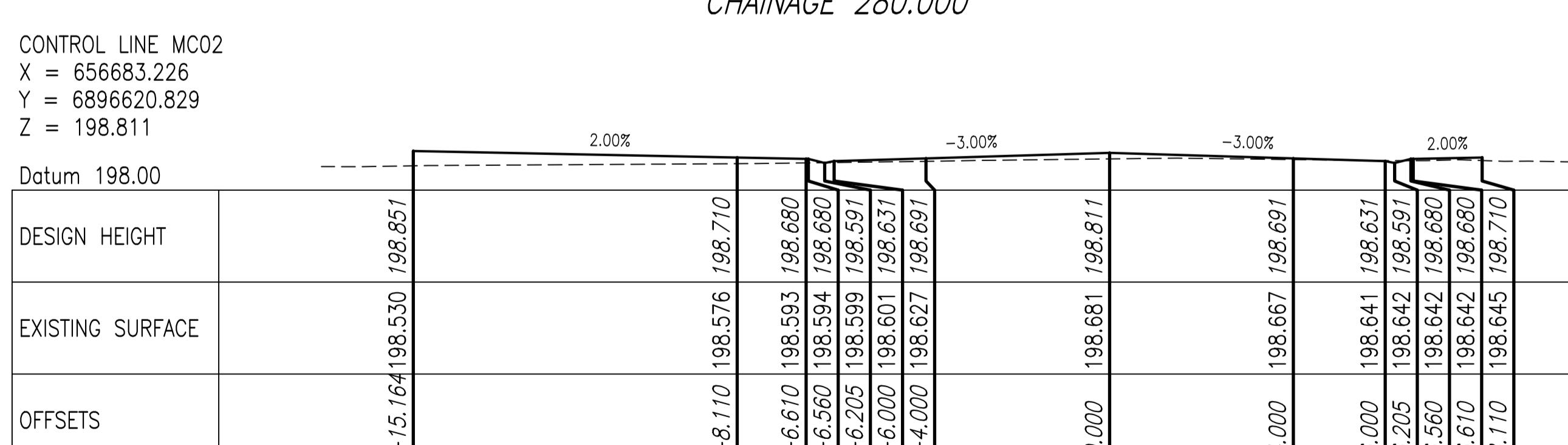
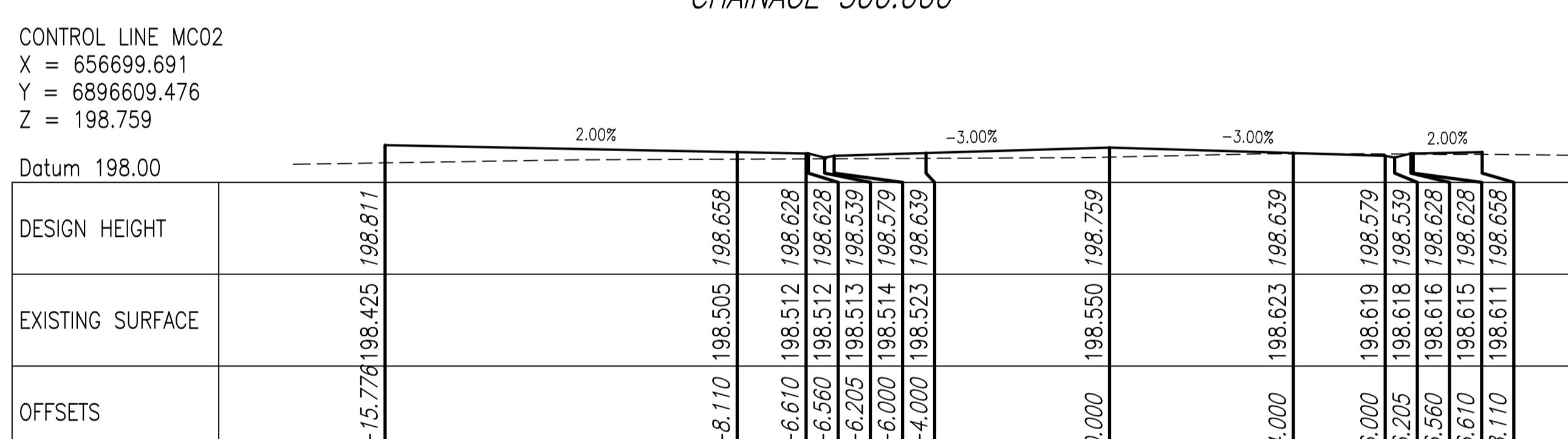
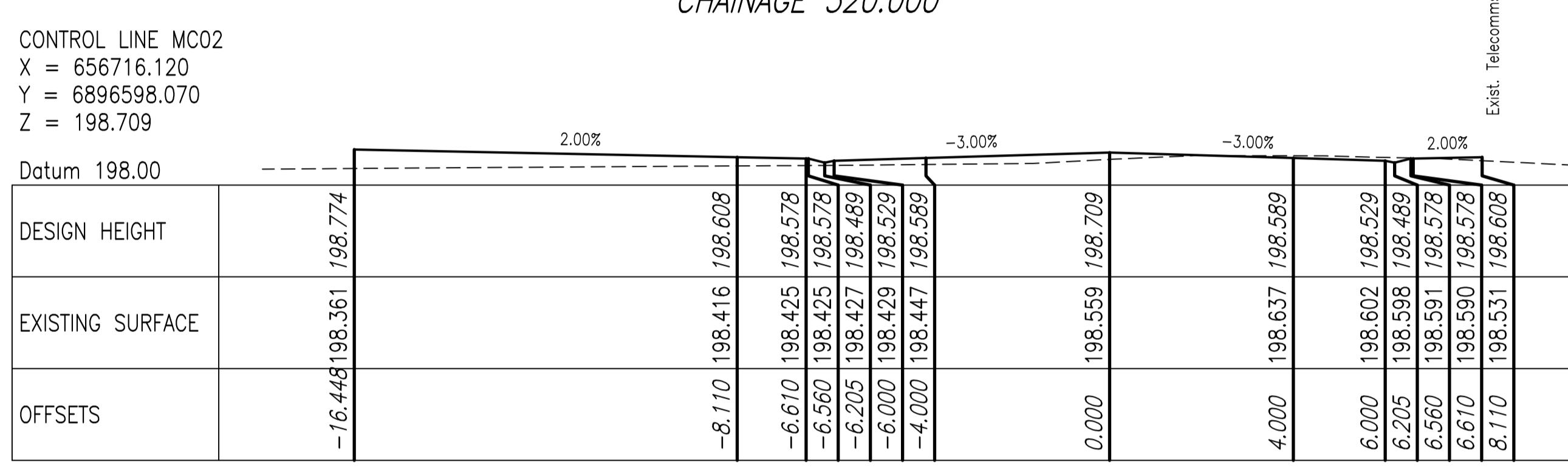
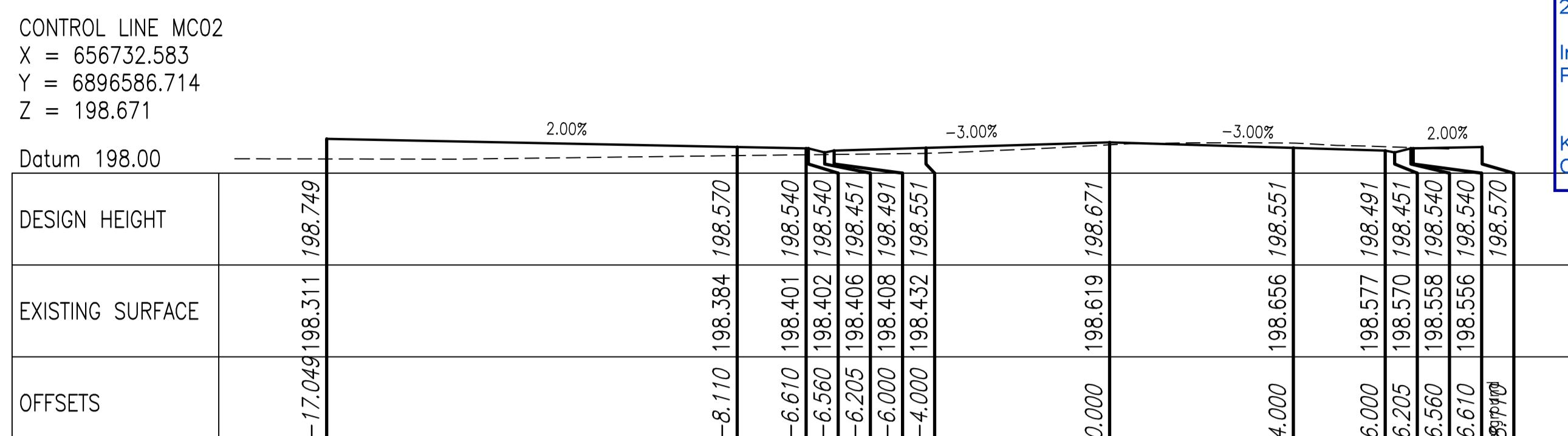
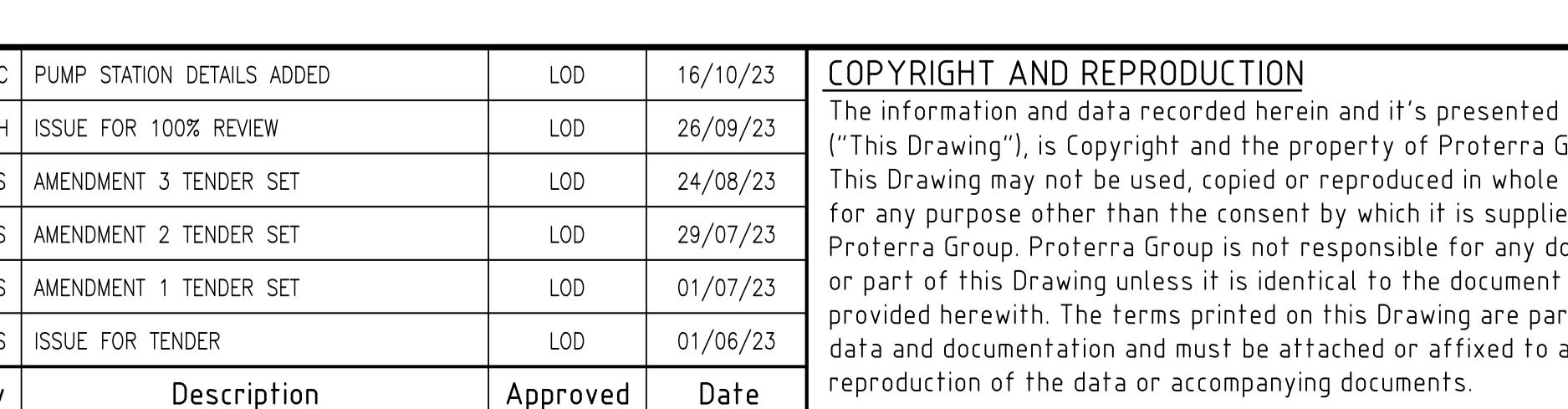
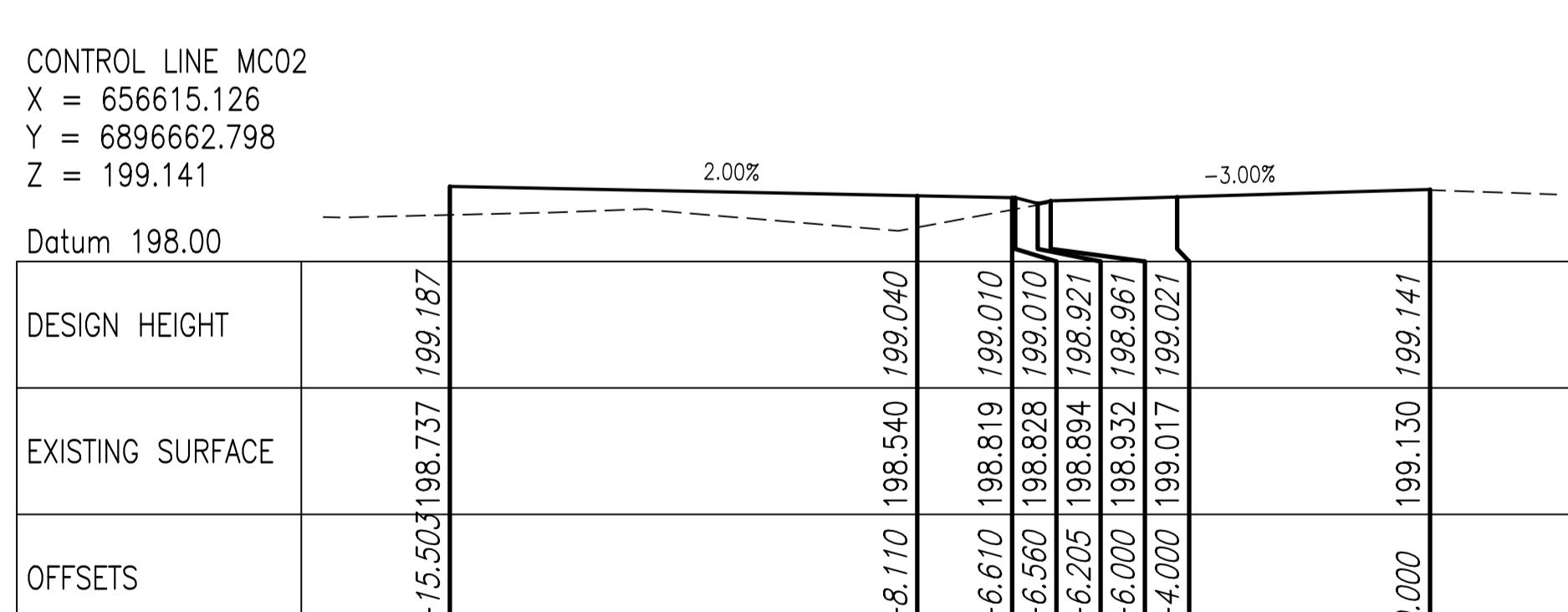
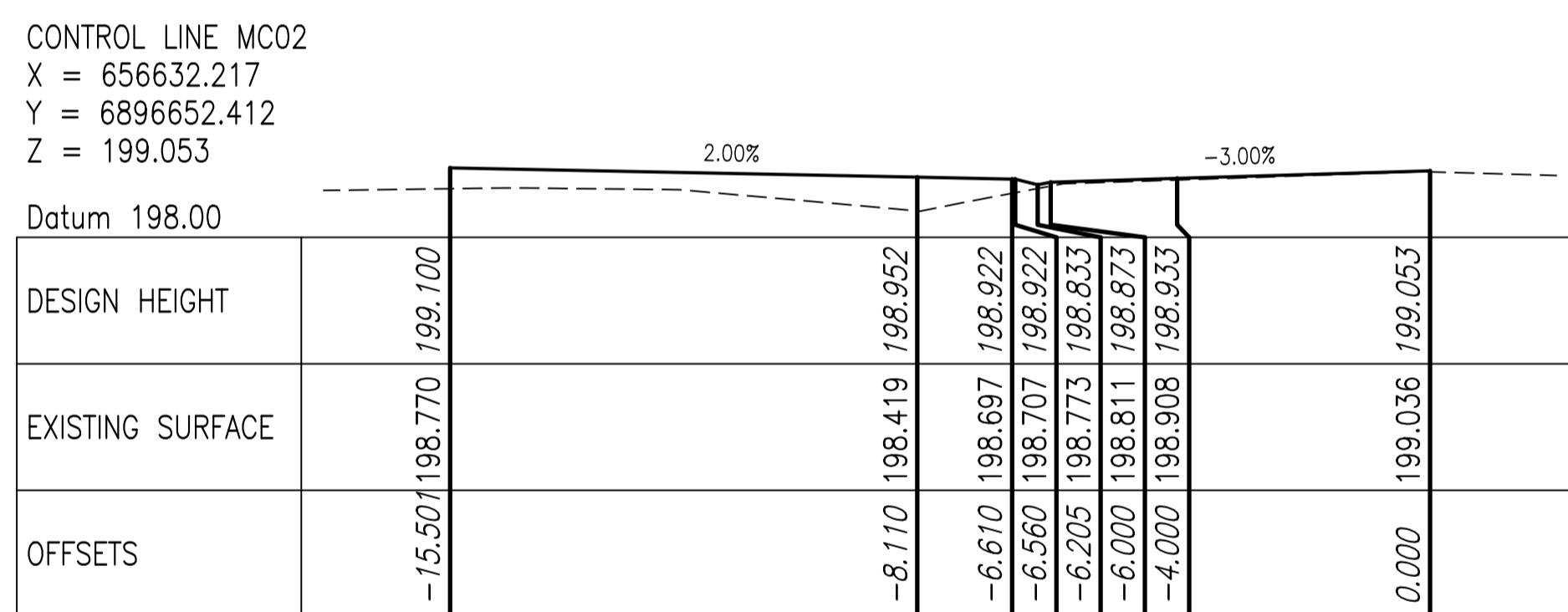
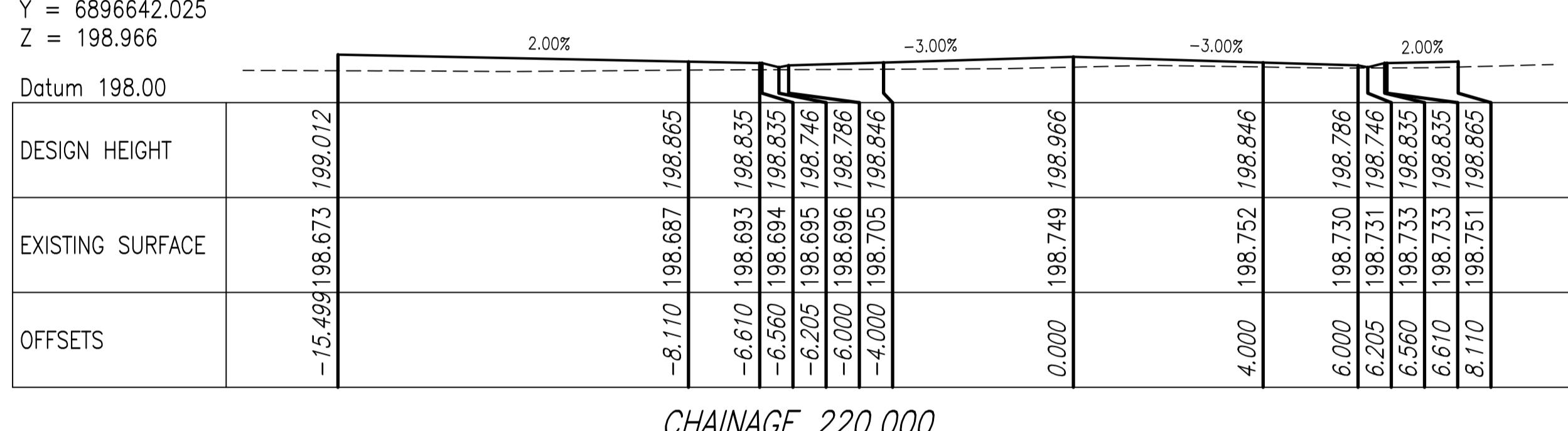
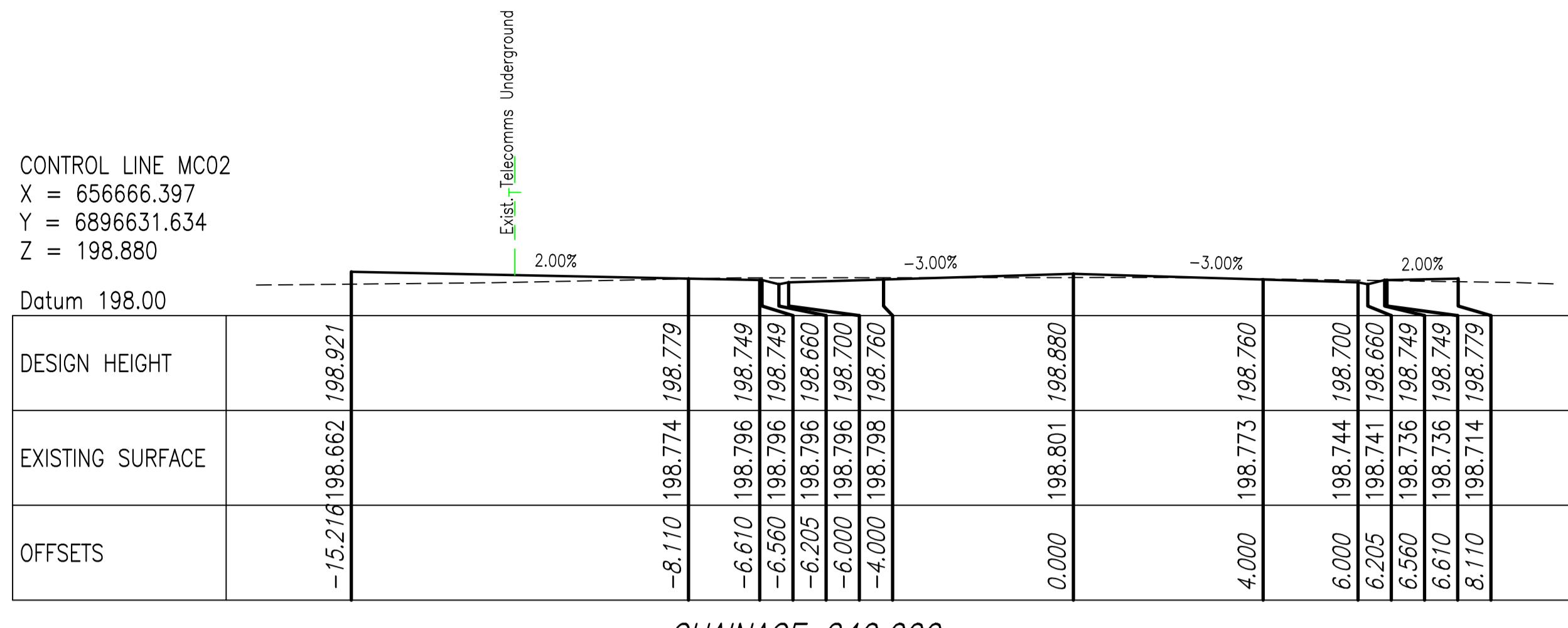
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Consultant Town Planner



CONTROL LINE MC02
X = 656809.173
Y = 6896566.446
Z = 198.718

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 400.000

CONTROL LINE MC02
X = 656789.384
Y = 6896569.343
Z = 198.698

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 380.000

CONTROL LINE MC02
X = 656769.600
Y = 6896572.274
Z = 198.678

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 360.000

CONTROL LINE MC02
X = 656750.367
Y = 6896577.637
Z = 198.662

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 340.000

| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
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| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
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| Rev | By | Description | Approved | Date |

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CONTROL LINE MC02
X = 656888.329
Y = 6896554.860
Z = 198.798

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 480.000

CONTROL LINE MC02
X = 656868.540
Y = 6896557.756
Z = 198.778

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 460.000

CONTROL LINE MC02
X = 656848.751
Y = 6896560.653
Z = 198.758

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 440.000

CONTROL LINE MC02
X = 656828.962
Y = 6896563.550
Z = 198.738

Datum 198.00

| DESIGN HEIGHT | |
|------------------|--|
| EXISTING SURFACE | |
| OFFSETS | |

CHAINAGE 420.000

CONTROL LINE MC02
X = 656800.138
Y = 6896553.444
Z = 198.718

Datum 198.00

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|---|-----|-----|-----|---|-------------|--|--|
| Scales: | | | | | | | |
| 0m | 1.0 | 2.0 | 3.0 | 4.0 | 1:100 AT A1 | | |
| 0m | 0.4 | 0.8 | 1.2 | 1.6 | 1:40 AT A1 | | |
| Approved | | | | | | | |
| A1 ORIGINAL SIZE | | | | | | | |
| Client | | | | Drawing No. | | | |
| BALONNE SHIRE COUNCIL | | | | 22.1003-024 | | | |
| Sheet 24 of 42 | | | | Revision. | | | |
| 6 | | | | 6 | | | |

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CONTROL LINE MC02
 X = 656914.699
 Y = 6896551.000
 Z = 198.825

Datum 198.00

| | | | | | | | |
|------------------|--|--|--|--|--|--|--|
| DESIGN HEIGHT | | | | | | | |
| EXISTING SURFACE | | | | | | | |
| OFFSETS | | | | | | | |

CHAINAGE 506.651

CONTROL LINE MC02
 X = 656908.118
 Y = 6896551.963
 Z = 198.818

Datum 198.00

| | | | | | | | |
|------------------|--|--|--|--|--|--|--|
| DESIGN HEIGHT | | | | | | | |
| EXISTING SURFACE | | | | | | | |
| OFFSETS | | | | | | | |

CHAINAGE 500.000

| | | | | | |
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| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | |
| Rev | By | Description | Approved | Date | |



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| | | | | | | | |
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| Scales: | 0m | 1.0 | 2.0 | 3.0 | 4.0 | 1:100 AT A1 | |
| Drafter | GCC | | | | | | |
| Check | LOD | | | | | | |
| Approved | | | | | | | |
| Client | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC02 CROSS SECTIONS SHEET 4 OF 4 | | | | Drawing No. | 22.1003-025 | |
| | | | | | Sheet | 25 of 42 | |
| | | | | | Revision. | 6 | |
| A1 ORIGINAL SIZE | | | | | | | |

CONTROL LINE MC03
X = 656596.007
Y = 6896604.207
Z = 198.779

| Datum 198.00 | | 2.00% | -3.00% | -3.00% | 2.00% |
|------------------|--|--------|---------|---------|--------|
| DESIGN HEIGHT | | -8.110 | 198.856 | 198.678 | -8.110 |
| EXISTING SURFACE | | -6.610 | 198.638 | 198.648 | -6.610 |
| OFFSETS | | -6.560 | 198.333 | 198.648 | -6.560 |

CHAINAGE 60.000

CONTROL LINE MC03
X = 656606.390
Y = 6896621.300
Z = 198.829

| Datum 198.00 | | 2.00% | -3.00% | -3.00% | 2.00% |
|------------------|--|--------|---------|---------|--------|
| DESIGN HEIGHT | | -8.110 | 198.734 | 198.728 | -8.110 |
| EXISTING SURFACE | | -6.610 | 198.752 | 198.698 | -6.610 |
| OFFSETS | | -6.560 | 198.753 | 198.698 | -6.560 |

CHAINAGE 40.000

CONTROL LINE MC03
X = 656616.774
Y = 6896638.393
Z = 198.880

| Datum 198.00 | | 2.00% | -3.00% | -3.00% | 2.00% |
|------------------|--|--------|---------|---------|--------|
| DESIGN HEIGHT | | -8.110 | 198.681 | 198.779 | -8.110 |
| EXISTING SURFACE | | -6.610 | 198.636 | 198.749 | -6.610 |
| OFFSETS | | -6.560 | 198.697 | 198.749 | -6.560 |

CHAINAGE 20.000

CONTROL LINE MC03
X = 656618.590
Y = 6896641.382
Z = 198.889

| Datum 198.00 | | 2.00% | -3.00% |
|------------------|--|--------|---------|
| DESIGN HEIGHT | | -8.110 | 198.716 |
| EXISTING SURFACE | | -6.610 | 198.723 |
| OFFSETS | | -6.560 | 198.721 |

CHAINAGE 16.503

CONTROL LINE MC03
X = 656591.698
Y = 6896597.115
Z = 198.758

| Datum 198.00 | | 2.00% | -3.00% |
|------------------|--|-------|---------|
| DESIGN HEIGHT | | 0.000 | 198.632 |
| EXISTING SURFACE | | 4.000 | 198.741 |
| OFFSETS | | 6.000 | 198.665 |

CHAINAGE 68.298

| | | | | | | |
|-----|-----|----------------------------|----------|----------|--|--|
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| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | |
| Rev | By | Description | Approved | Date | | |



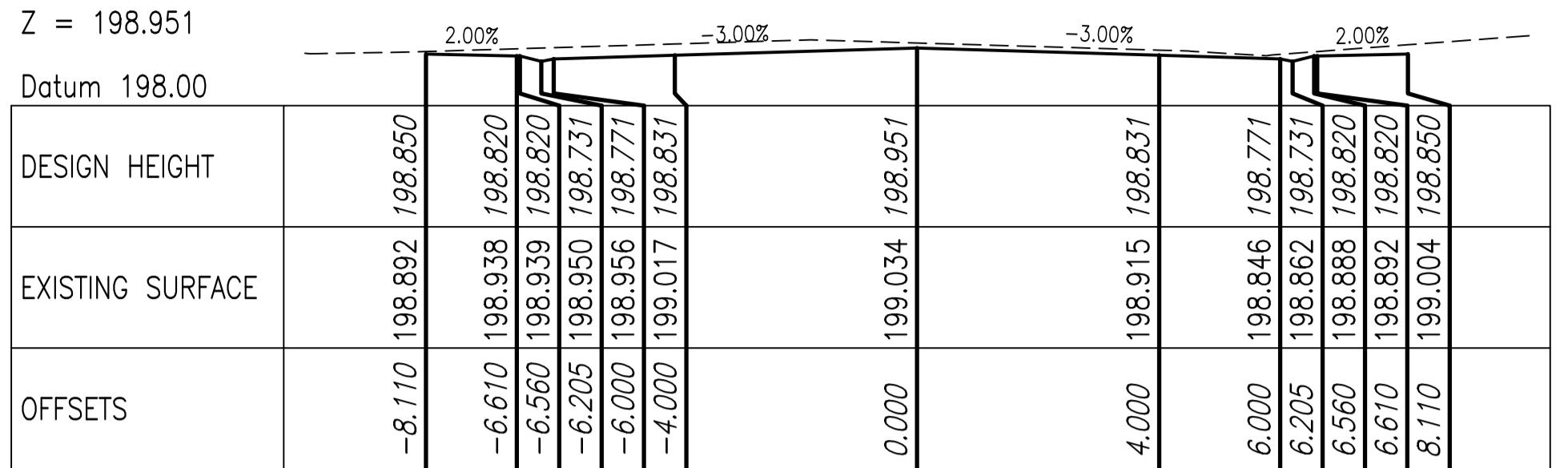
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|---|-----------------------|---------|-----|---|-------------|--|--|
| Scales: | | | | | | | |
| 0m | 1.0 | 2.0 | 3.0 | 4.0 | 1:100 AT A1 | | |
| 6.205 | 198.665 | 198.538 | | | | | |
| 6.560 | 198.652 | 198.627 | | | | | |
| 6.610 | 198.651 | 198.627 | | | | | |
| 8.110 | 198.599 | 198.657 | | | | | |
| A1 ORIGINAL SIZE | | | | | | | |
| Designed | BJS | | | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC03 CROSS SECTIONS | | | |
| Drafter | GCC | | | | | | |
| Check | LOD | | | | | | |
| Approved | | | | | | | |
| Client | BALONNE SHIRE COUNCIL | | | Drawing No. | 22.1003-026 | | |
| | | | | Sheet | 26 of 42 | | |
| Revision. | | | | | | | |
| 6 | | | | | | | |

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CONTROL LINE MC04
X = 656469.429
Y = 6896682.691
Z = 198.951

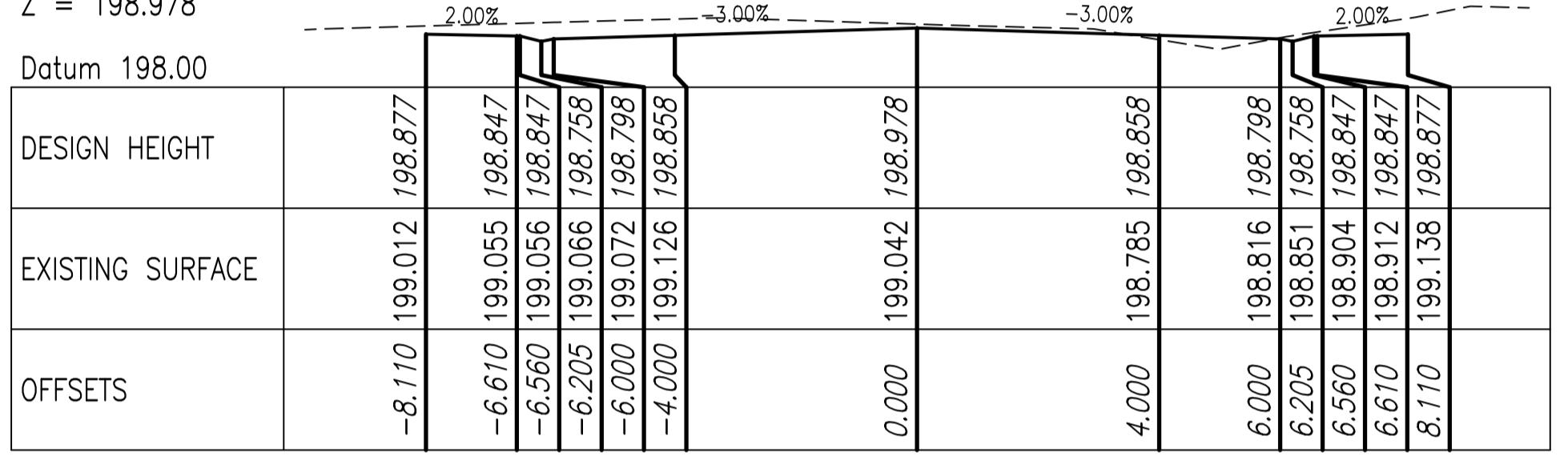
Datum 198.00



CHAINAGE 60.000

CONTROL LINE MC04
X = 656481.624
Y = 6896698.417
Z = 198.978

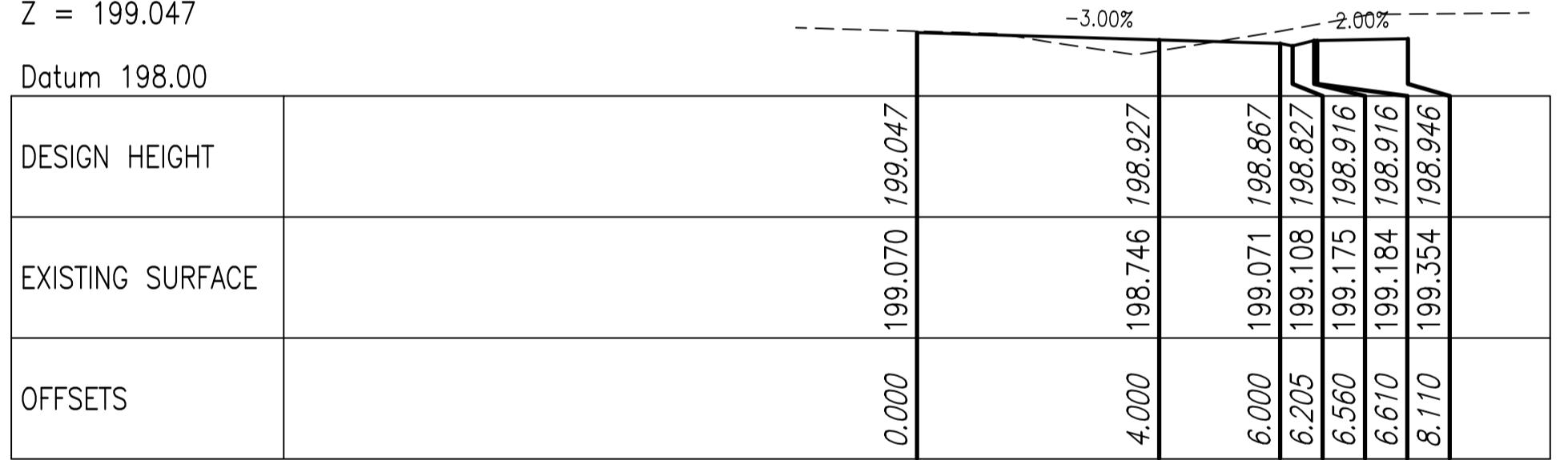
Datum 198 00



CHAINAGE 40.000

CONTROL LINE MC04
X = 656492.011
Y = 6896715.509
Z = 199.047

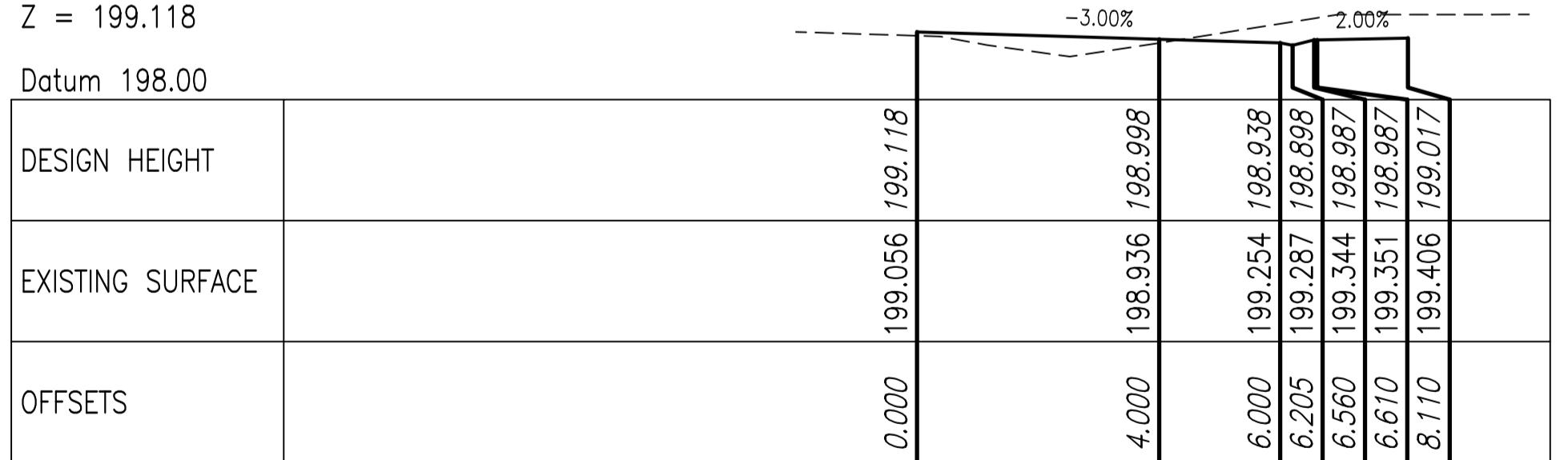
Datum 198.00



CHAINAGE 20.000

CONTROL LINE MC04
X = 656497.209
Y = 6896723.675
Z = 199.118

Page 108/20



CHAINAGE 10.316

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Scales:

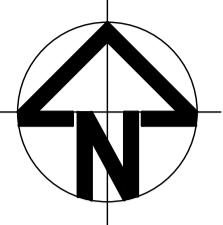
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0m 0.4 0.8 1.2 1.6 1:40 AT A1

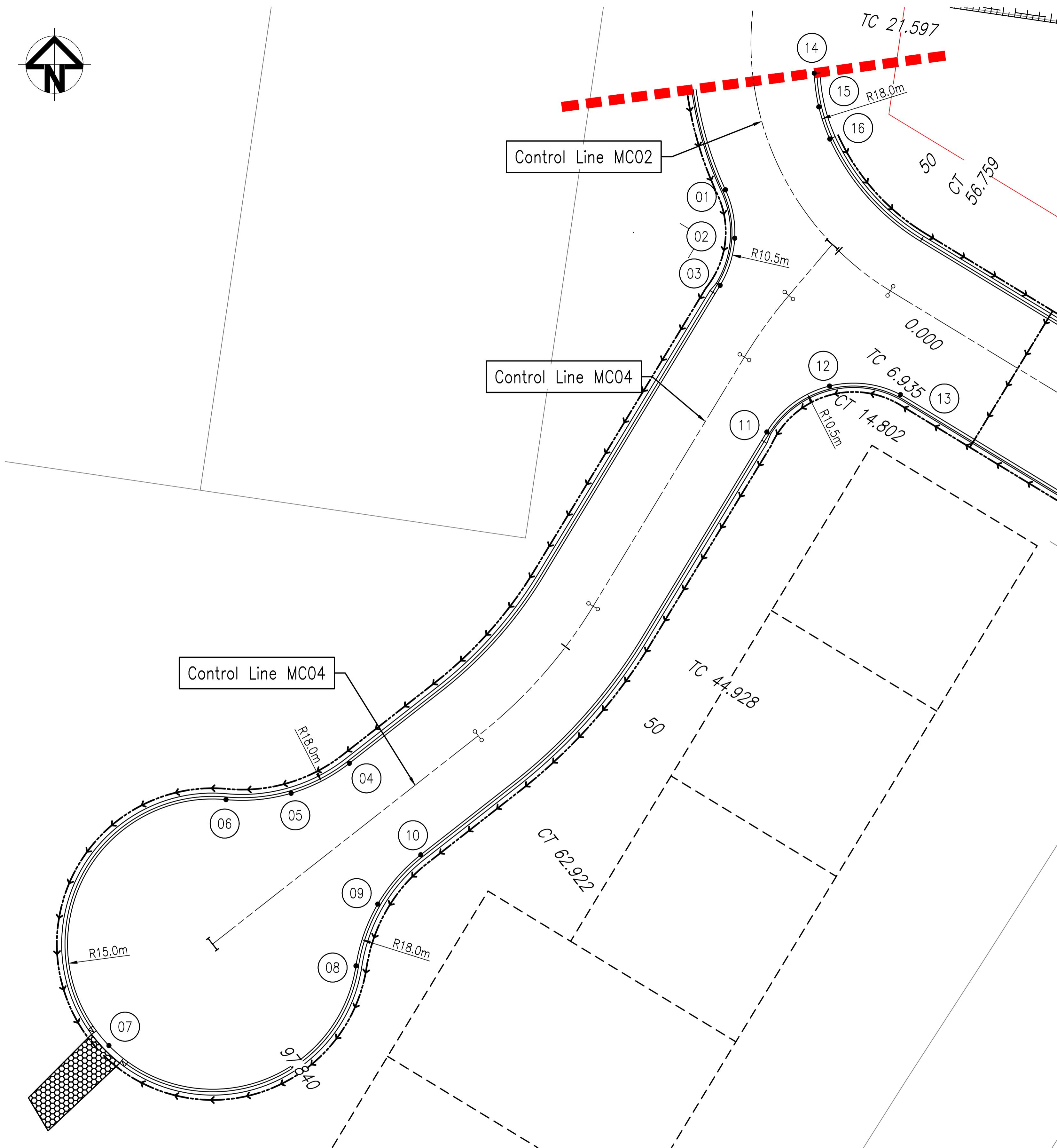
11. *What is the primary purpose of the following statement?*

| | | |
|---|--|----------------------------------|
| REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION | | |
| S | | |
| C | | |
| D | | |
| ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION MC04 CROSS SECTIONS | | Drawing No. 22.1003-02 |
| | | Sheet 27 of 42 |
| Client BALONNE SHIRE COUNCIL | | Revision. 6 |

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| ROADWORKS SETOUT DETAILS | | |
|--------------------------|------------|-------------|
| POINT | EASTING | NORTHING |
| 01 | 656492.747 | 6896737.254 |
| 02 | 656493.710 | 6896732.245 |
| 03 | 656492.199 | 6896727.372 |
| 04 | 656453.842 | 6896677.963 |
| 05 | 656447.823 | 6896674.869 |
| 06 | 656441.087 | 6896674.216 |
| 07 | 656428.979 | 6896548.791 |
| 08 | 656454.533 | 6896657.033 |
| 09 | 656456.786 | 6896663.414 |
| 10 | 656461.237 | 6896668.512 |
| 11 | 656497.030 | 6896712.216 |
| 12 | 656503.514 | 6896716.963 |
| 13 | 656510.850 | 6896716.074 |
| 14 | 656501.938 | 6896749.307 |
| 15 | 656502.406 | 6896745.824 |
| 16 | 656503.542 | 6896742.499 |

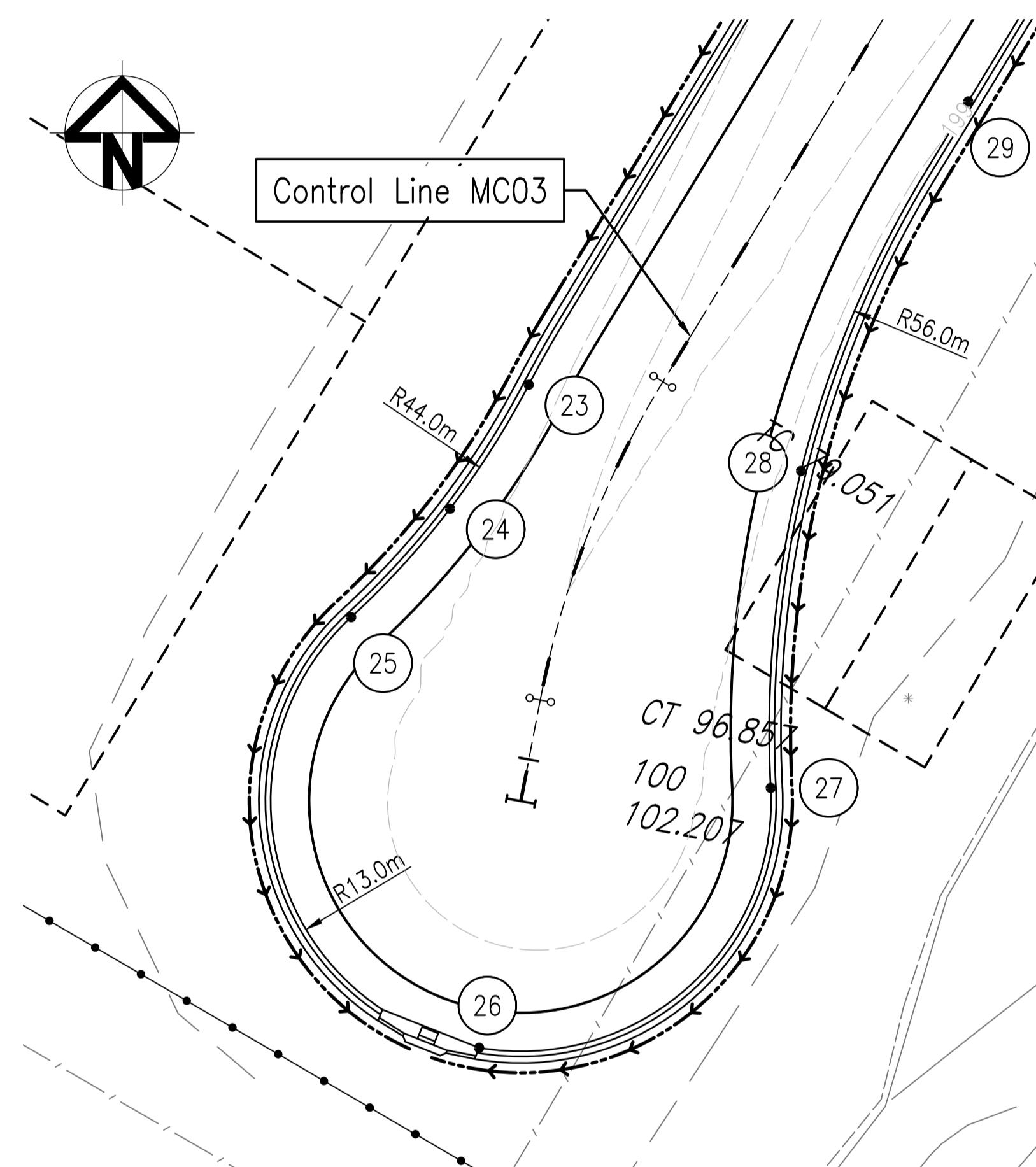
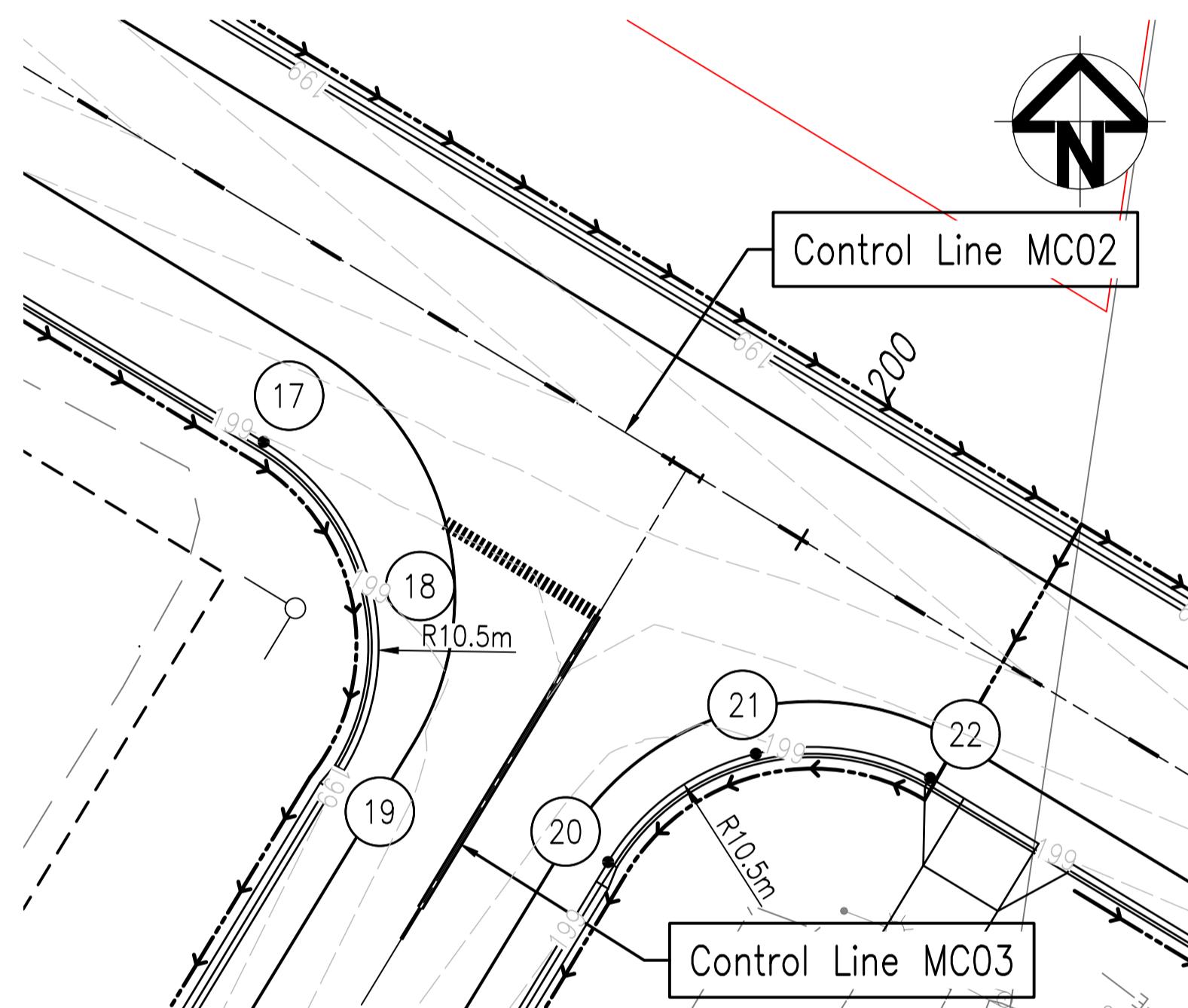
ROADWORKS DETAILS

SCALE 1:250

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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | | | | Scales: | 0m | 2.0 | 4.0 | 6.0 | 8.0 | 10.0 | 1:250 AT A1 | Designed | BJS | | | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS DETAILS PLAN SHEET 1 OF 3 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | | | | | | | | Drafter | GCC | | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | | | | | | | | Check | LOD | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | | | | | | | | Approved | | | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | | | | | | | | Client | BALONNE SHIRE COUNCIL | | | | |
| Rev | By | Description | Approved | Date | | | | | | | | | | | Revision. | 6 | | | | |

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| ROADWORKS SETOUT DETAILS | | |
|--------------------------|------------|-------------|
| POINT | EASTING | NORTHING |
| 17 | 656608.594 | 6896656.704 |
| 18 | 656613.342 | 6896650.221 |
| 19 | 656611.650 | 6896641.506 |
| 20 | 656623.718 | 6896638.267 |
| 21 | 656630.202 | 6896643.016 |
| 22 | 656637.851 | 6896641.960 |
| 23 | 656579.147 | 6896587.766 |
| 24 | 656575.061 | 6896581.330 |
| 25 | 656569.926 | 6896575.696 |
| 26 | 656576.569 | 6896553.330 |
| 27 | 656591.714 | 6896566.828 |
| 28 | 656593.292 | 6896583.293 |
| 29 | 656601.974 | 6896602.474 |

ROADWORKS DETAILS
SCALE 1:250

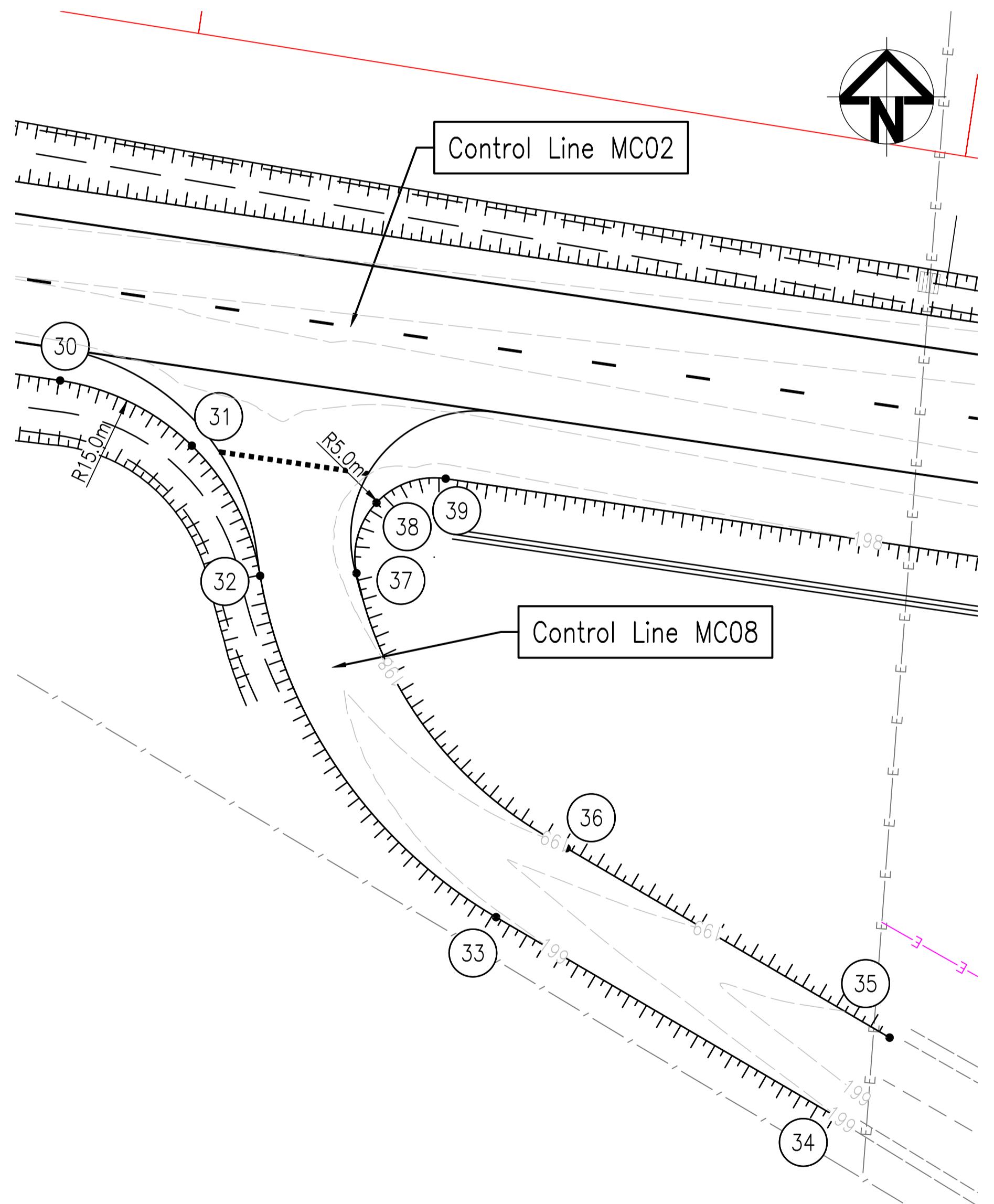
| | | | | | |
|-----|-----|----------------------------|----------|----------|--|
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| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | |
| Rev | By | Description | Approved | Date | |



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|---|--|----------|-----|--|--|--|--|
| Scales: | | Designed | BJS | | | | |
| 0m 2.0 4.0 6.0 8.0 10.0 | | Drafter | GCC | | | | |
| 1:250 AT A1 | | Check | LOD | | | | |
| Approved | | | | | | | |
| A1 ORIGINAL SIZE | | | | | | | |
| Client | | | | Drawing No. ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS DETAILS PLAN SHEET 2 OF 3 | | | |
| | | | | 22.1003-029 | | | |
| | | | | Sheet 29 of 42 | | | |
| | | | | Client | | | |
| | | | | BALONNE SHIRE COUNCIL | | | |
| | | | | Revision. 6 | | | |

BALONNE SHIRE COUNCIL
Planning Act 2016
This document comprises part of
Development Permit No.
RL127
and was issued on
24 September 2025
In accordance with the :-
Planning Act 2016
KATE SWEPSON
Consultant Town Planner



ROADWORKS DETAILS
SCALE 1:250

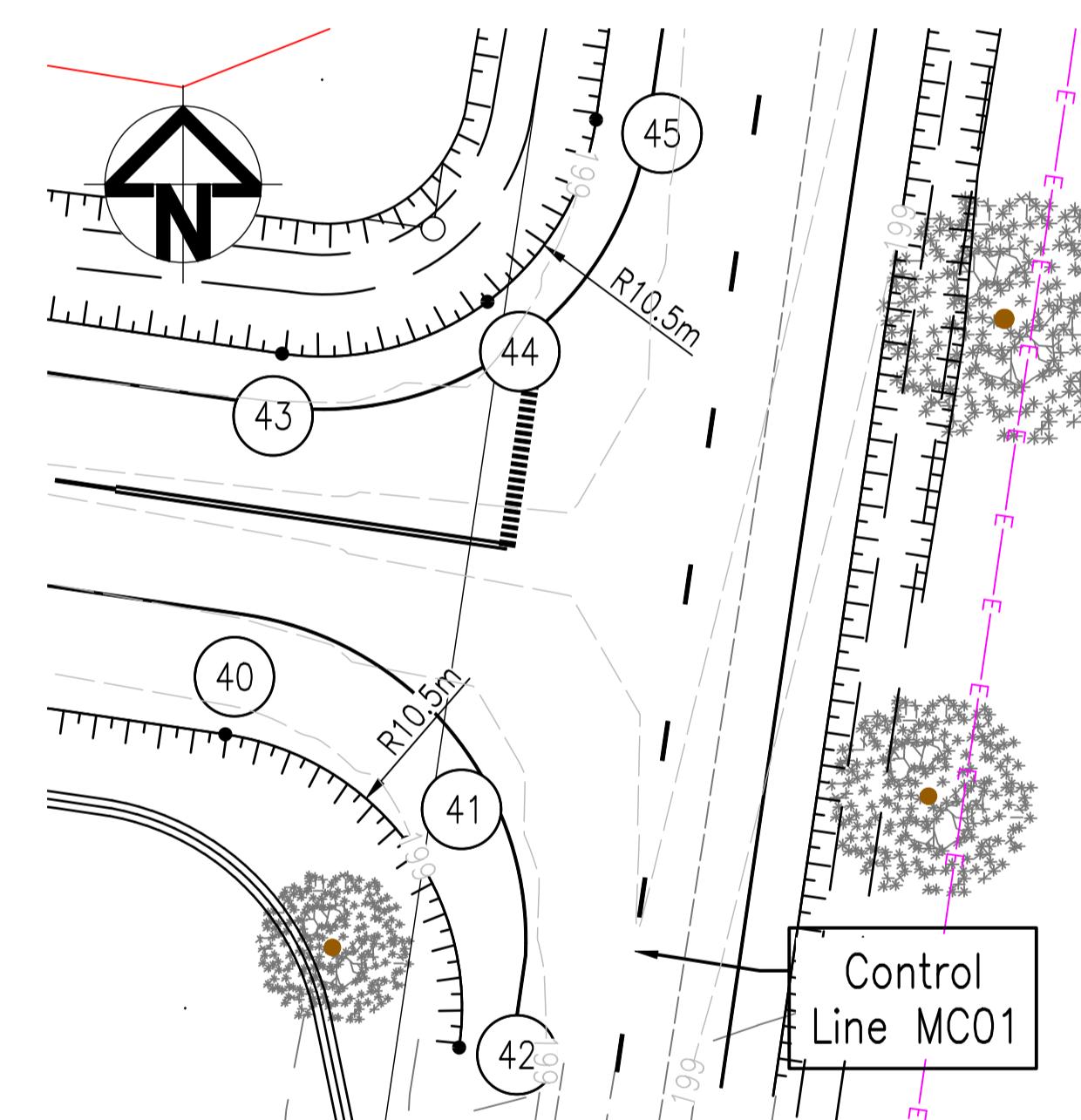
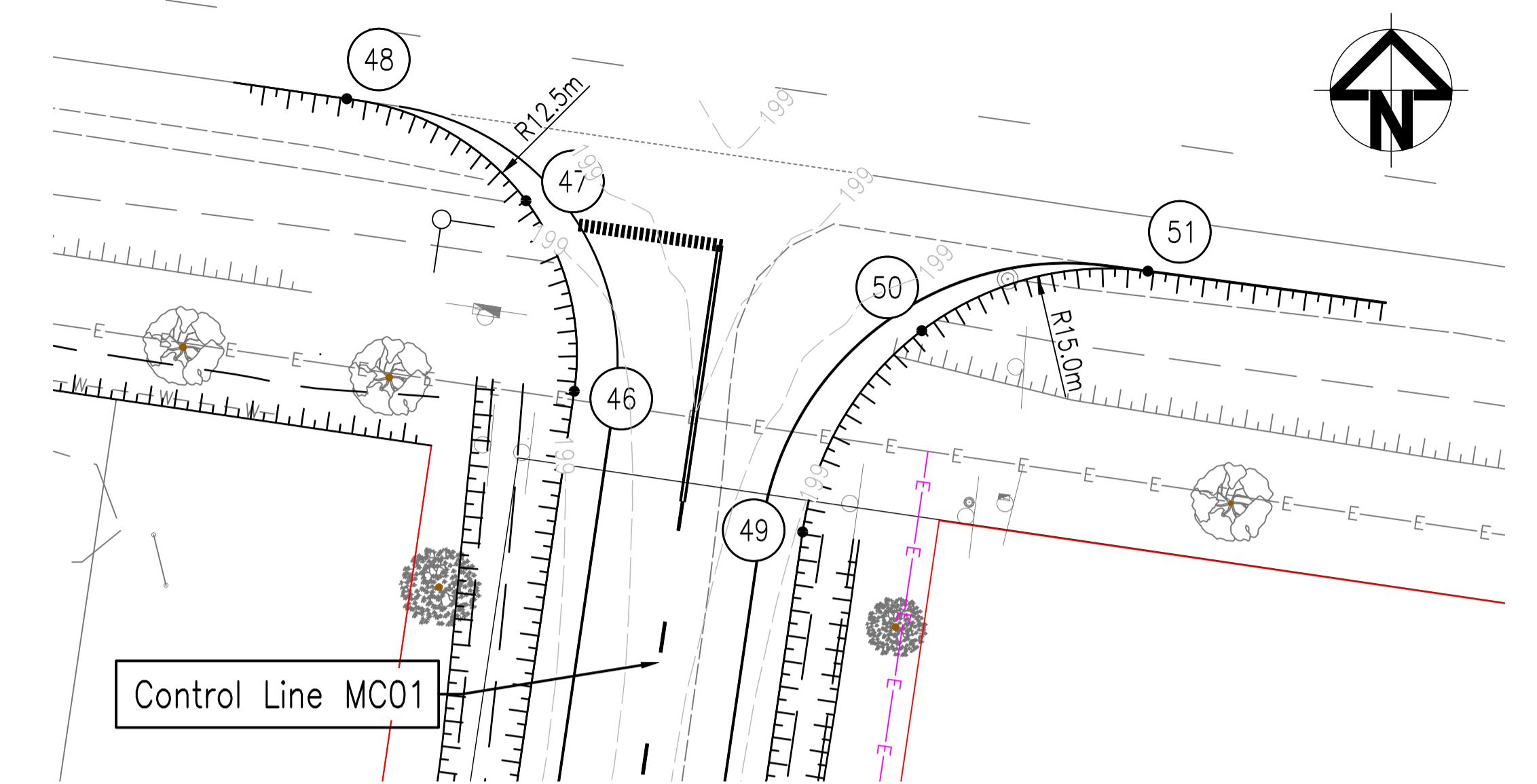
| | | | | |
|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
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| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
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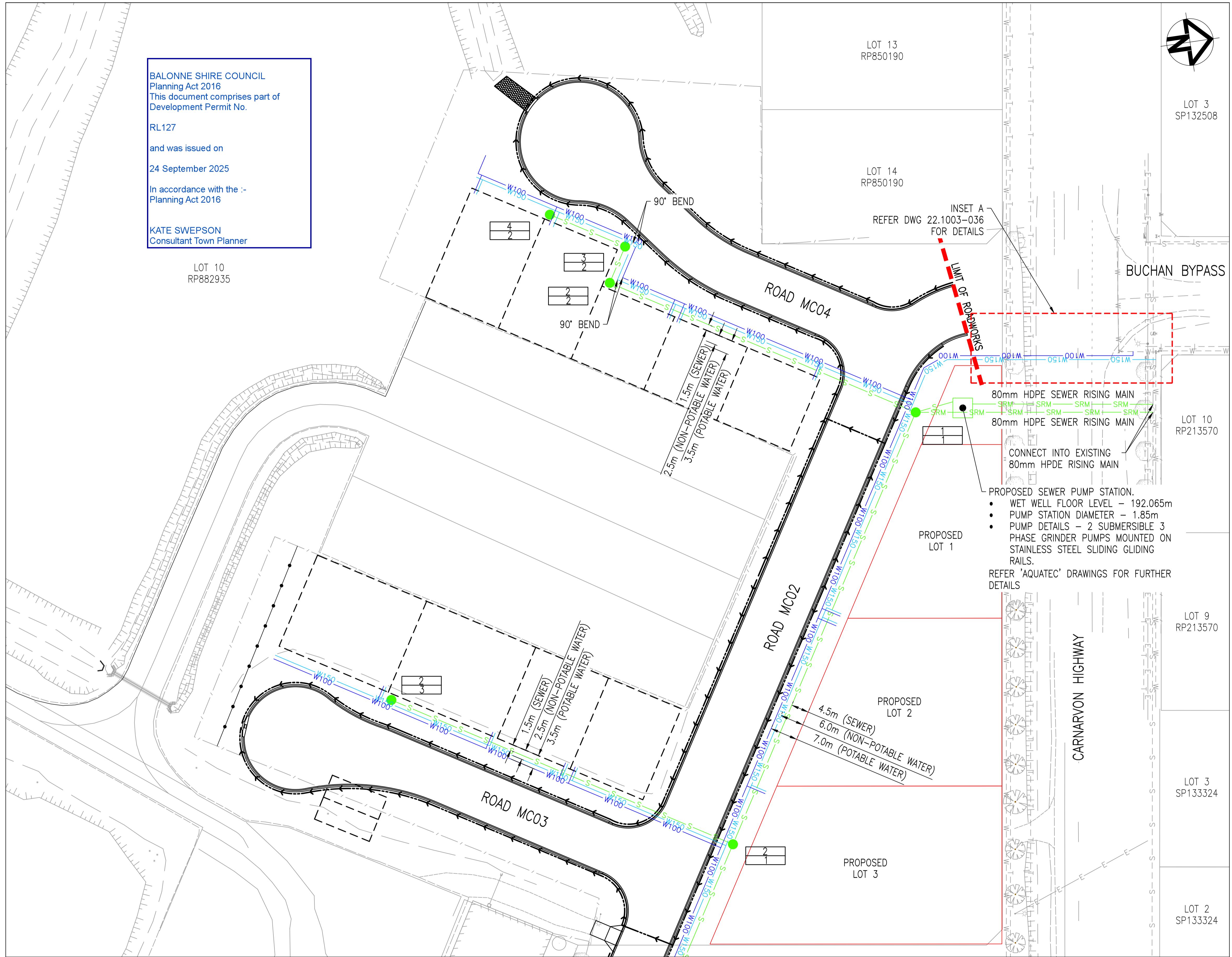


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|---|---|-----|-----|---|-----------------------|------|-------------|
| Scales: | 0m | 2.0 | 4.0 | 6.0 | 8.0 | 10.0 | 1:250 AT A1 |
| Designed | BJS | | | | | | |
| Drafter | GCC | | | | | | |
| Check | LOD | | | | | | |
| Approved | | | | | | | |
| Client | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION ROADWORKS DETAILS PLAN SHEET 3 OF 3 | | | Drawing No. | 22.1003-030 | | |
| | | | | Sheet | 30 of 42 | | |
| | | | | | BALONNE SHIRE COUNCIL | | |
| | | | | Revision. | 6 | | |



| POINT | EASTING | NORTHING |
|-------|------------|-------------|
| 30 | 656796.229 | 6896562.277 |
| 31 | 656804.534 | 6896558.169 |
| 32 | 656808.842 | 6896549.966 |
| 33 | 656823.715 | 6896528.455 |
| 34 | 656845.489 | 6896515.685 |
| 35 | 656848.524 | 6896520.861 |
| 36 | 656828.157 | 6896532.805 |
| 37 | 656814.916 | 6896550.131 |
| 38 | 656816.174 | 6896554.574 |
| 39 | 656820.536 | 6896556.091 |
| 40 | 656913.402 | 6896542.498 |
| 41 | 656920.294 | 6896538.392 |
| 42 | 656922.275 | 6896530.618 |
| 43 | 656915.557 | 6896556.938 |
| 44 | 656923.350 | 6896558.907 |
| 45 | 656927.467 | 6896565.811 |
| 46 | 656946.577 | 6896696.712 |
| 47 | 656944.225 | 6896705.995 |
| 48 | 656935.498 | 6896710.961 |
| 49 | 656957.704 | 6896689.860 |
| 50 | 656963.505 | 6896699.662 |
| 51 | 656974.520 | 6896702.563 |



JOINS SHEET 2
WATER AND SEWERAGE LAYOUT

SCALE 1:500

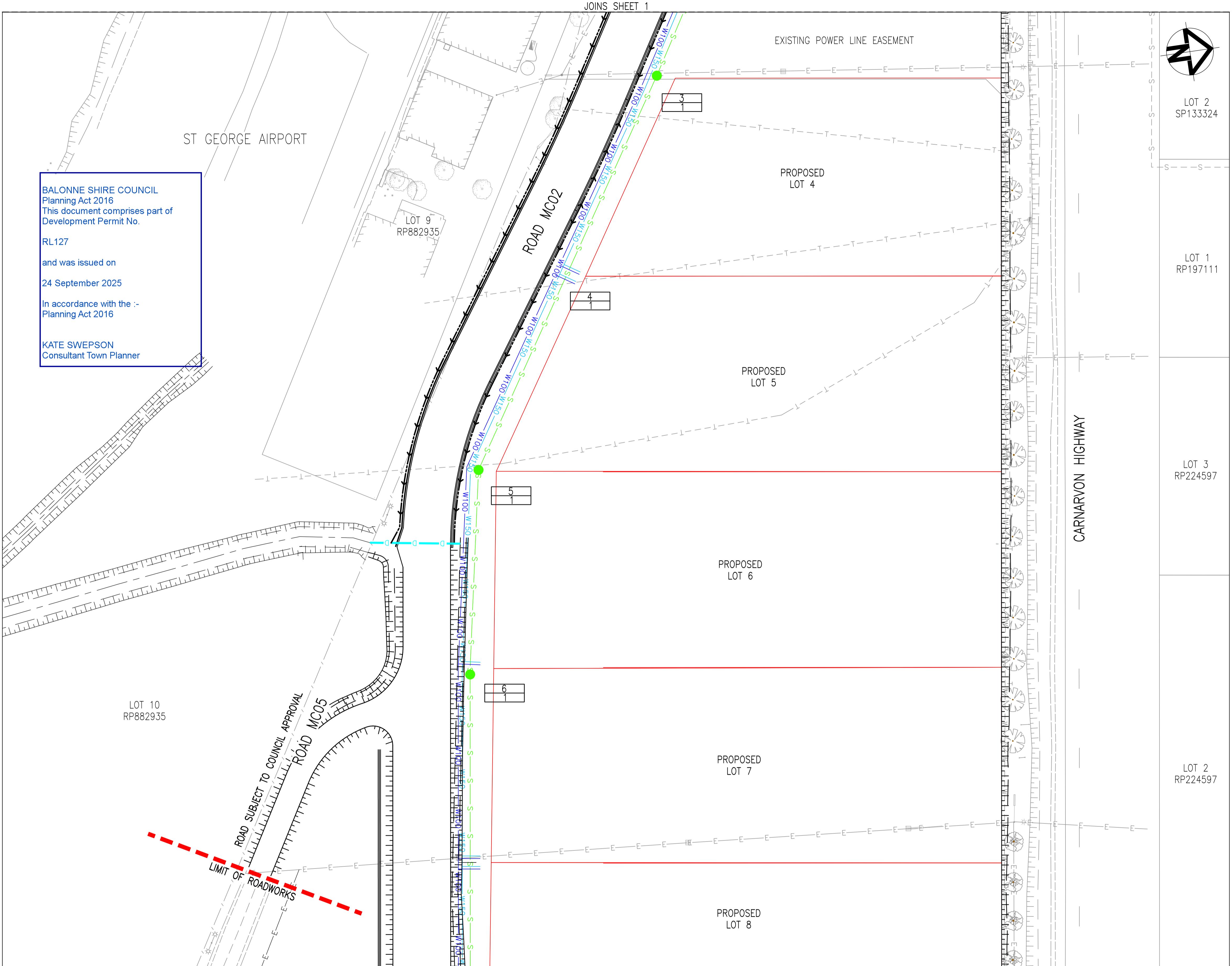
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|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |
| Rev | By | Description | Approved | Date |

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|---|--|-----|----|---|-------------|-------------|--|
| Scales: | 0m | 5.0 | 10 | 15 | 20 | 1:500 AT A1 | |
| Designed | BJS | | | | | | |
| Drafter | GCC | | | | | | |
| Check | LOD | | | | | | |
| Approved | | | | | | | |
| Client | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION WATER & SEWERAGE PLAN SHEET 1 OF 3 | | | Drawing No. | 22.1003-034 | | |
| | | | | Sheet | 34 of 42 | | |
| | | | | Revision. | 6 | | |



WATER LEGEND

| | |
|------|---|
| W100 | DN100 uPVC PN16 POTABLE WATER MAIN |
| W150 | DN150 uPVC PN16 RIVER WATER (NON POTABLE) |
| FH | FIRE HYDRANT, VALVE AND REDUCER |
| W | EXISTING WATER MAIN, VALVE AND HYDRANT |
| — | — |

SEWER LEGEND

| | |
|-----------------------|---|
| D—D—D | STORMWATER DRAINAGE LINE |
| S—S—S | SEWER MAIN AND MANHOLE (MH) |
| —SRM— | SEWER RISING MAIN |
| —S—O—S— | EXISTING SEWER MAIN AND MANHOLE |
| MH.6.57 1 ML04f | TYPE & INDICATIVE TOP OF MANHOLE LEVEL MANHOLE NUMBER LINE NUMBER |

WATER NOTES

- ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CURRENT WATER SERVICES ASSOCIATION OF AUSTRALIA STANDARDS.
- UNLESS SPECIFIED OTHERWISE ALL MATERIALS AND WORK SHALL COMPLY WITH THE RELEVANT AUSTRALIAN STANDARDS.
- FOR CONSTRUCTION OF PVC WATER RETICULATION SYSTEMS, REFER TO WSA 03-2011-3.1 "PART 2: CONSTRUCTION"
- THE CONSTRUCTION OF WATER SUPPLY RETICULATION WORK SHOWN ON THIS DRAWING MUST BE SUPERVISED BY AN ENGINEER WHO HAS RPEQ REGISTRATION.
- THE CONTRACTOR SHALL ENSURE THAT ALL RELEVANT SET-OUT PEGS ARE PLACED BY THE SURVEYOR BEFORE COMMENCING CONSTRUCTION.
- ADOPT THE EXISTING ROAD AC SURFACE AS THE PERMANENT LEVEL TO ENSURE MINIMUM COVER.
- COVER ON MAINS FROM PERMANENT LEVEL TO BE AS FOLLOWS:
 - DN100 PVC MAIN - MINIMUM 600mm TO FUTURE SURFACE, WHERE POSSIBLE
 - PVC PIPES SHALL BE SERIES 2, PN16 TO AS/NZS 1477
 - FITTINGS SHALL BE ELASTOMERIC SEAL JOINT TO AS/NZS 1477, U.N.O
 - PE AND PVC PIPE DEFLECTIONS AND CURVING TO BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND WSA03-2011.3.1, SECTION 15.2
- SCOURS SHALL DISCHARGE AS DETAILED, FITTING AND PLACEMENT OF CONCRETE TO SCOUR BENDS SHALL BE IN ACCORDANCE WITH WATER SERVICES ASSOCIATION OF AUSTRALIA (WSA 03-2011-3.1)
- VALVES AND HYDRANTS SHALL BE PLACED OPPOSITE REAL PROPERTY PEGS UNLESS NOTED OTHERWISE (TOLERANCE $\pm 0.2m$)
- WATER QUALITY TESTING IS TO BE PERFORMED IN ACCORDANCE WITH WATER SERVICES ASSOCIATION OF AUSTRALIA (WSA 03-2011-3.1)
- ALL PIPEWORK SHALL BE APPROVED BY THE SUPERINTENDENTS REPRESENTATIVE BEFORE BACKFILLING.
- MAINS SHALL NOT BE ACCEPTED 'ON MAINTENANCE' UNTIL THE 'AS CONSTRUCTED' DRAWINGS ARE APPROVED, THE MAINS ARE PASSED BY BALONNE SHIRE COUNCIL.

SEWER NOTES

- ALL WORK TO BE IN ACCORDANCE WITH THE WSA STANDARD DRAWINGS WHERE APPLICABLE AND THE REQUIREMENTS OF THE BALONNE SHIRE COUNCIL.
- WHERE APPROPRIATE, EXISTING MANHOLE LID LEVELS TO BE RAISED OR LOWERED TO ALIGN WITH ADJACENT FINISHED SURFACE LEVELS AS REQUIRED.
- ALL SEWER PIPES TO BE 150 DIA. (225 DIA. WHERE INDICATED) DWV uPVC SN8 RUBBER RING JOINTED TO AS/NZS 1260.
- ALL PROPERTY CONNECTIONS TO BE TERMINATED 500mm ABOVE THE FINISHED SURFACE LEVEL, CLEARLY MARKED AS SEWER AND SEALED WITH A GLUED CAP.
- ALL MANHOLE DROP TYPES TO BE IN ACCORDANCE WITH WSA STANDARD DRAWINGS.
- ALL WORK ASSOCIATED WITH CONNECTIONS TO LIVE SEWERS WILL BE CARRIED OUT BY BALONNE SHIRE COUNCIL AT THE CONTRACTORS EXPENSE.
- TRENCHING FOR SEWERS TO BE CARRIED OUT IN ACCORDANCE WITH BALONNE SHIRE COUNCIL REQUIREMENTS.
- THE CONTRACTOR MUST OBTAIN THE LOCATIONS OF ALL EXISTING SERVICES PRIOR TO ANY EXCAVATION. THE CONTRACTOR MUST CO-ORDINATE THE WORKS WITH THE RELEVANT AUTHORITIES AND MUST BE RESPONSIBLE FOR REINSTATING ANY EXISTING SERVICES WHICH BECOME UNCOVERED OR DAMAGED DURING THE CONSTRUCTION PERIOD.
- ANY ALTERATIONS REQUIRED TO EXISTING SERVICES TO BE CARRIED OUT AS DIRECTED BY THE BALONNE SHIRE COUNCIL ENGINEER AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR MUST MAINTAIN ACCURATE RECORDS OF LEVELS AND LOCATION OF ALL SERVICES TO FULLY COMPLY WITH BALONNE SHIRE COUNCIL REQUIREMENTS FOR AS-CONSTRUCTED INFORMATION AND MUST SUPPLY AS-CONSTRUCTED PLANS TO COUNCIL AT THE COMPLETION OF THE PROJECT.
- WHERE A SEWER MAIN CROSSES A DECLARED ROAD UNDER THE JURISDICTION OF THE DEPARTMENT OF TRANSPORT AND MAIN ROADS (DTMR), ANY CONDITIONS / REQUIREMENTS RELATING TO THE ROAD CROSSING MUST BE OBTAINED FROM THE DEPARTMENT OF TRANSPORT AND MAIN ROADS.
- ALL LEVELS AND DIMENSIONS ARE IN METRES TO AHD, EXCEPT PIPE SIZES WHICH ARE SHOWN IN MILLIMETRES.
- THE CONTRACTOR MUST VERIFY EXISTING LEVELS AT THE CONNECTION TO EXISTING SEWERS AND ENSURE MINIMUM FALL THROUGH THE NEW MANHOLE IS PROVIDED IN ACCORDANCE WITH THE RELEVANT STANDARD DRAWINGS.
- THE CONTRACTOR MUST CHECK ALL INFORMATION AND DIMENSIONS SHOWN ON THESE DRAWINGS, ON SITE, PRIOR TO COMMENCEMENT OF CONSTRUCTION.

JOINS SHEET 3
WATER AND SEWERAGE LAYOUT

SCALE 1:500

| | | | | |
|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |
| Rev | By | Description | Approved | Date |

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|---|-----|-----|-------------|
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| Scales: | 0m | 5.0 | 10 |
| | 15 | 20 | 1:500 AT A1 |
| Designed | BJS | | |
| Drafter | GCC | | |
| Check | LOD | | |
| Approved | | | |
| | | | |
| A1 ORIGINAL SIZE | | | |

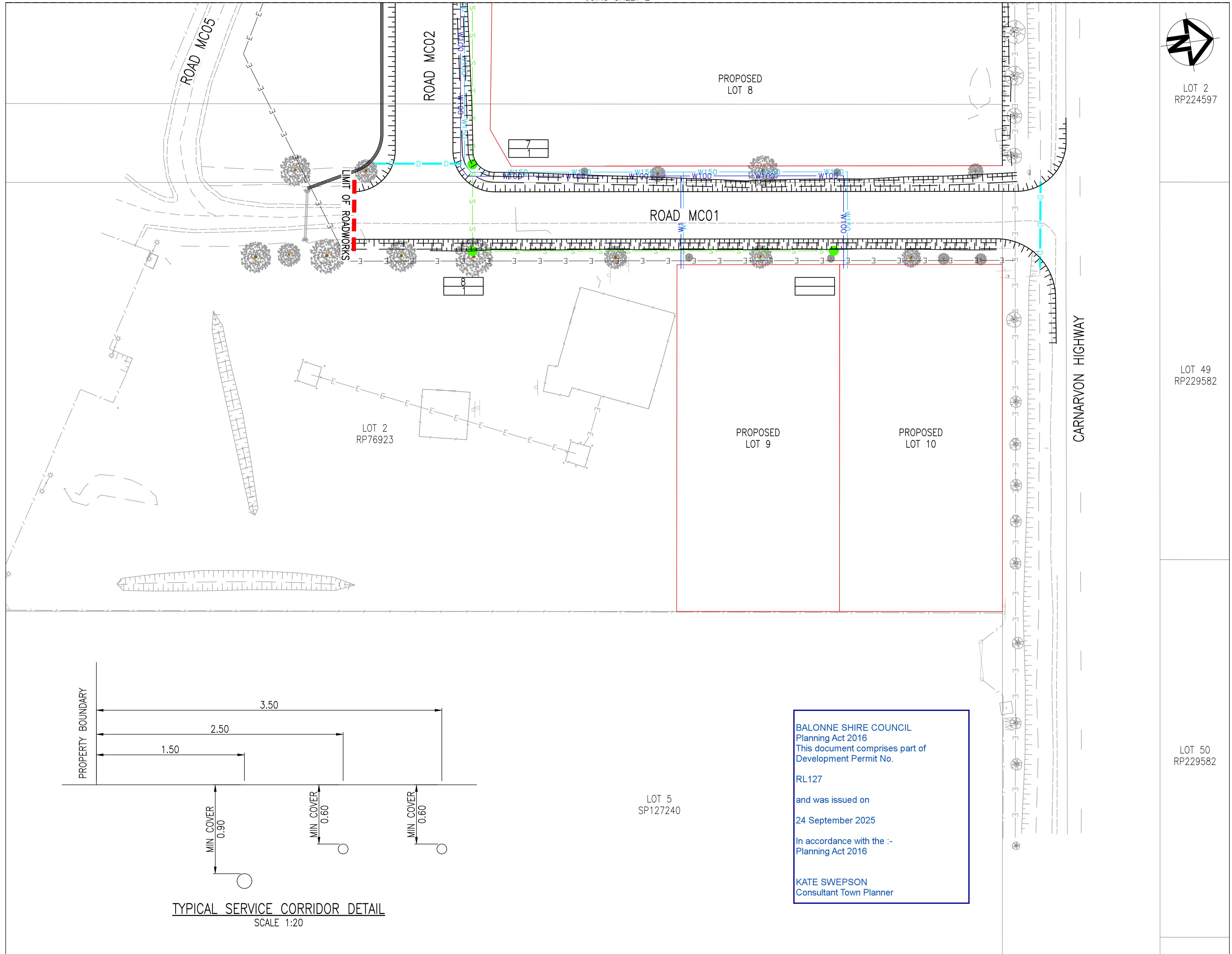
**ST GEORGE AIRPORT
INDUSTRIAL SUBDIVISION
WATER & SEWERAGE PLAN
SHEET 2 OF 3**

Drawing No.
22.1003-035

Sheet 35 of 42

Client
BALONNE SHIRE COUNCIL

Revision.
6



WATER LEGEND

| | |
|-----------------------|---|
| W100 | DN100 uPVC PN16 POTABLE WATER MAIN |
| W150 | DN150 uPVC PN16 RIVER WATER (NON POTABLE) |
| FH | FIRE HYDRANT, VALVE AND REDUCER |
| W | EXISTING WATER MAIN, VALVE AND HYDRANT |
| — — — W — — — W — — — | |
| D — D — D | STORMWATER DRAINAGE LINE |
| — S — S — | SEWER MAIN AND MANHOLE (MH) |
| — SRM — | SEWER RISING MAIN |
| — — S — — S — | EXISTING SEWER MAIN AND MANHOLE |
| MH.6.57 1 ML04f | TYPE & INDICATIVE TOP OF MANHOLE LEVEL MANHOLE NUMBER LINE NUMBER |

SEWER LEGEND

| | |
|-----------|-----------------------------|
| D — D — D | STORMWATER DRAINAGE LINE |
| — S — S — | SEWER MAIN AND MANHOLE (MH) |
| — SRM — | SEWER RISING MAIN |

| | |
|-----------------------|---|
| — — S — — S — | EXISTING SEWER MAIN AND MANHOLE |
| MH.6.57 1 ML04f | TYPE & INDICATIVE TOP OF MANHOLE LEVEL MANHOLE NUMBER LINE NUMBER |

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- ADOPT THE EXISTING ROAD AC SURFACE AS THE PERMANENT LEVEL TO ENSURE MINIMUM COVER.
- COVER ON MAINS FROM PERMANENT LEVEL TO BE AS FOLLOWS:
 - DN100 PVC MAIN - MINIMUM 600mm TO FUTURE SURFACE, WHERE POSSIBLE
 - PVC PIPES SHALL BE SERIES 2, PN16 TO AS/NZS 1477, U.N.O
 - FITTINGS SHALL BE ELASTOMERIC SEAL JOINT TO AS/NZS 1477, U.N.O
 - PE AND PVC PIPE DEFLECTIONS AND CURVING TO BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND WSA03-2011.3.1, SECTION 15.2
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- THE CONTRACTOR MUST CHECK ALL INFORMATION AND DIMENSIONS SHOWN ON THESE DRAWINGS, ON SITE, PRIOR TO COMMENCEMENT OF CONSTRUCTION.

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| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | | | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION WATER & SEWERAGE PLAN SHEET 3 OF 3 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | | | Drawing No. 22.1003-036 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | | Sheet 36 of 42 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | Client BALONNE SHIRE COUNCIL |
| Rev | By | Description | Approved | Date | | | | Revision. 6 |

| MAINTENANCE HOLE / SHAFT NAME | 9/1 | 8/1 | 7/1 | 6/1 | 5/1 | 4/1 | 3/1 | 2/1 | 1/1 |
|-------------------------------|-----|-----|-----|-----|-----|-----|---------|---------|-----|
| MH / MS COVER TYPE | | | | | | | | | |
| MH / MS TYPE | | | | | | | | | |
| DS MH / MS DROP TYPE | | | | | | | | | |
| LINE DEFLECTION & BRANCHES | 90° | | | 2° | | -1° | | 51° | |
| PROPERTY CONNECTION TYPE | | | | | | | LINE 03 | LINE 02 | |

BALONNE SHIRE COUNCIL
Planning Act 2016
This document comprises part of
Development Permit No.
RL127
and was issued on
24 September 2025
In accordance with the :-
Planning Act 2016
KATE SWEPSON
Consultant Town Planner

| | | | | | | | | | |
|--|----------------------|----------|----------|----------------------|----------------------|--------------------|----------------------|----------------------|---------|
| DATUM RL | 183.000 | | | | | | | | |
| PROPERTY DESCRIPTION | | | | | | | | | |
| PIPE SIZE (mm), CLASS | 150, PVC | 150, PVC | 150, PVC | 150, PVC | 300, PVC | 150, PVC | 150, PVC | 300, PVC | |
| GRADE (1 IN X) | | | | | | | | | |
| LENGTH | | | | | | | | | |
| EMBEDMENT TYPE | | | | | | | | | |
| FINISHED (& EXISTING) SURFACE LEVEL | 198.651 (198.651) | | | | | | | | |
| DEPTH OF INVERT BELOW FSL | 1.661 | 2.630 | 2.710 | 198.710 (198.710) | 198.446 (198.346) | 3.782 (198.329) | 4.707 (198.660) | 5.219 (198.659) | |
| INVERT LEVEL | 196.990 | 196.080 | 196.000 | 198.249 (198.771) | 195.180 | 195.210 | 194.240 (198.660) | 193.970 (198.659) | |
| CHAINAGE | 0.000 | 113.438 | 195.880 | 2.369 (198.771) | 228.887 | 280.558 | 389.809 | 438.280 | 557.057 |

LINE

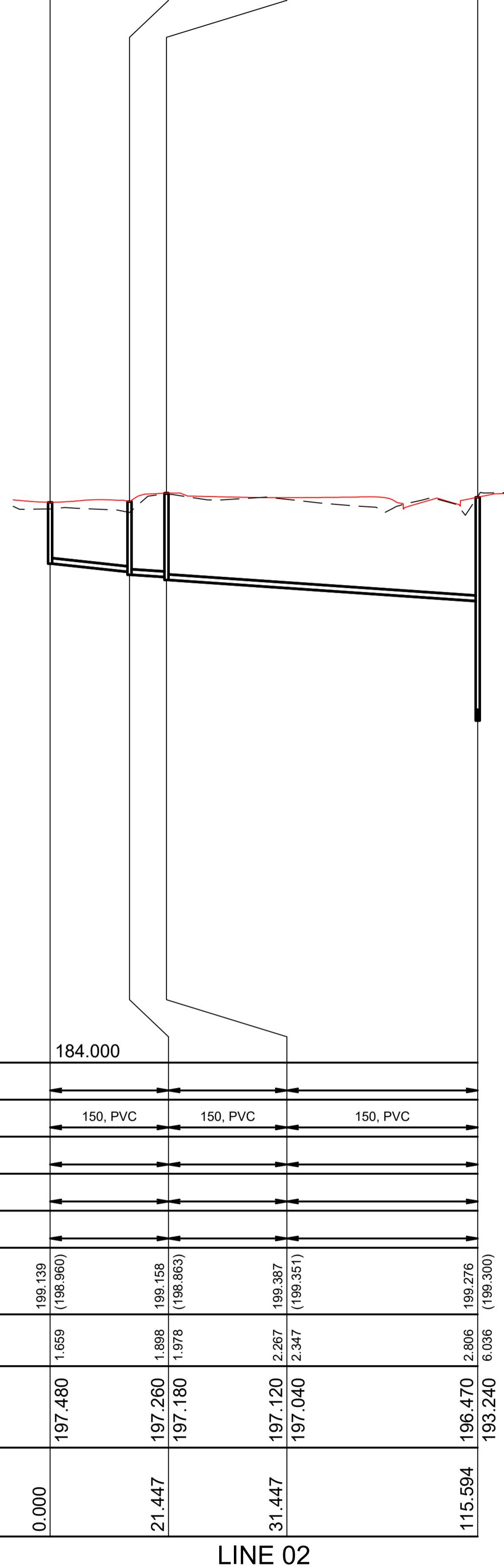
LINE 01

SEWERAGE LONGITUDINAL SECTION
SCALE 1:1000 HORIZONTAL, SCALE 1:100 VERTICAL

| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 | COPYRIGHT AND REPRODUCTION |  PROTERRA GROUP 31 GLASSER STREET GOONDIWINDI QLD 4390 ABN: 82 626 886 771 TEL: (07) 4671 5701 WWW.PROTERRAGROUP.COM.AU | DO NOT SCALE FROM THIS DRAWING. USE DIMENSIONS WHERE MARKED | | | REFER ANY DISCREPANCIES TO ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION | | |
|-----|-----|----------------------------|----------|----------|----------------------------|---|---|-----------------------|--|--|-----------------------------------|--|
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 | Scales: | Designed | BJS | | | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION SEWERAGE LONGSECTIONS SHEET 1 OF 2 | Drawing No. 22.1003-037 | |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 | | 0m 1.0 2.0 3.0 4.0 1:100 AT A1 | Drafter | GCC | | | | |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 | | 0m 10 20 30 40 1:1000 AT A1 | Check | LOD | | | | |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 | | | Approved | | | | | |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 | | | | | | | | |
| Rev | By | Description | Approved | Date | | A1 ORIGINAL SIZE | Client | BALONNE SHIRE COUNCIL | | Revision. | 6 | |

| MAINTENANCE HOLE / SHAFT NAME | 4/2 | 3/2 | 2/2 | 2/1 |
|-------------------------------|-----|-----|------|---------|
| MH / MS COVER TYPE | | | | |
| MH / MS TYPE | | | | |
| DS MH / MS DROP TYPE | | | | |
| LINE DEFLECTION & BRANCHES | | 90° | -90° | LINE 01 |
| PROPERTY CONNECTION TYPE | | | | |

110

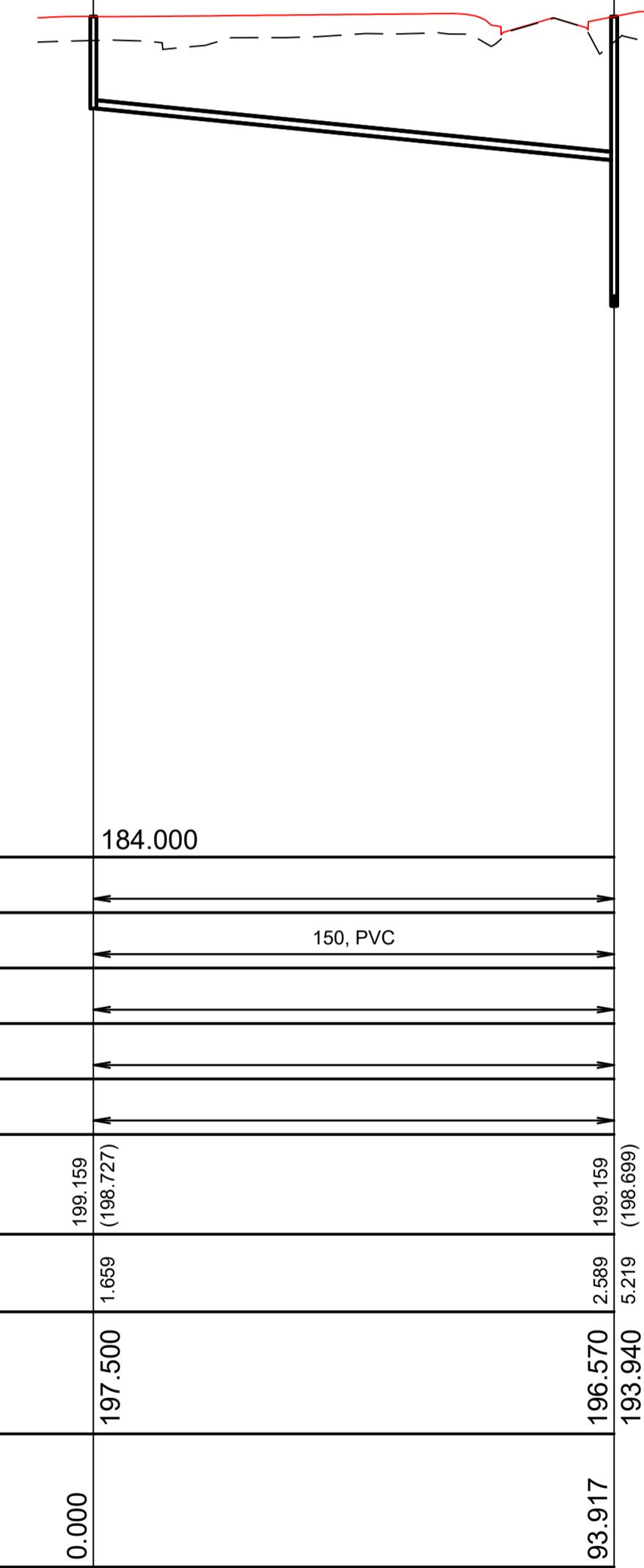


LINE

LINE 02

| MAINTENANCE HOLE / SHAFT NAME | 2/3 | 3/1 |
|-------------------------------|-----|-----|
| MH / MS COVER TYPE | | |
| MH / MS TYPE | | |
| DS MH / MS DROP TYPE | | |
| LINE DEFLECTION & BRANCHES | | |
| PROPERTY CONNECTION TYPE | | |

LINE 01

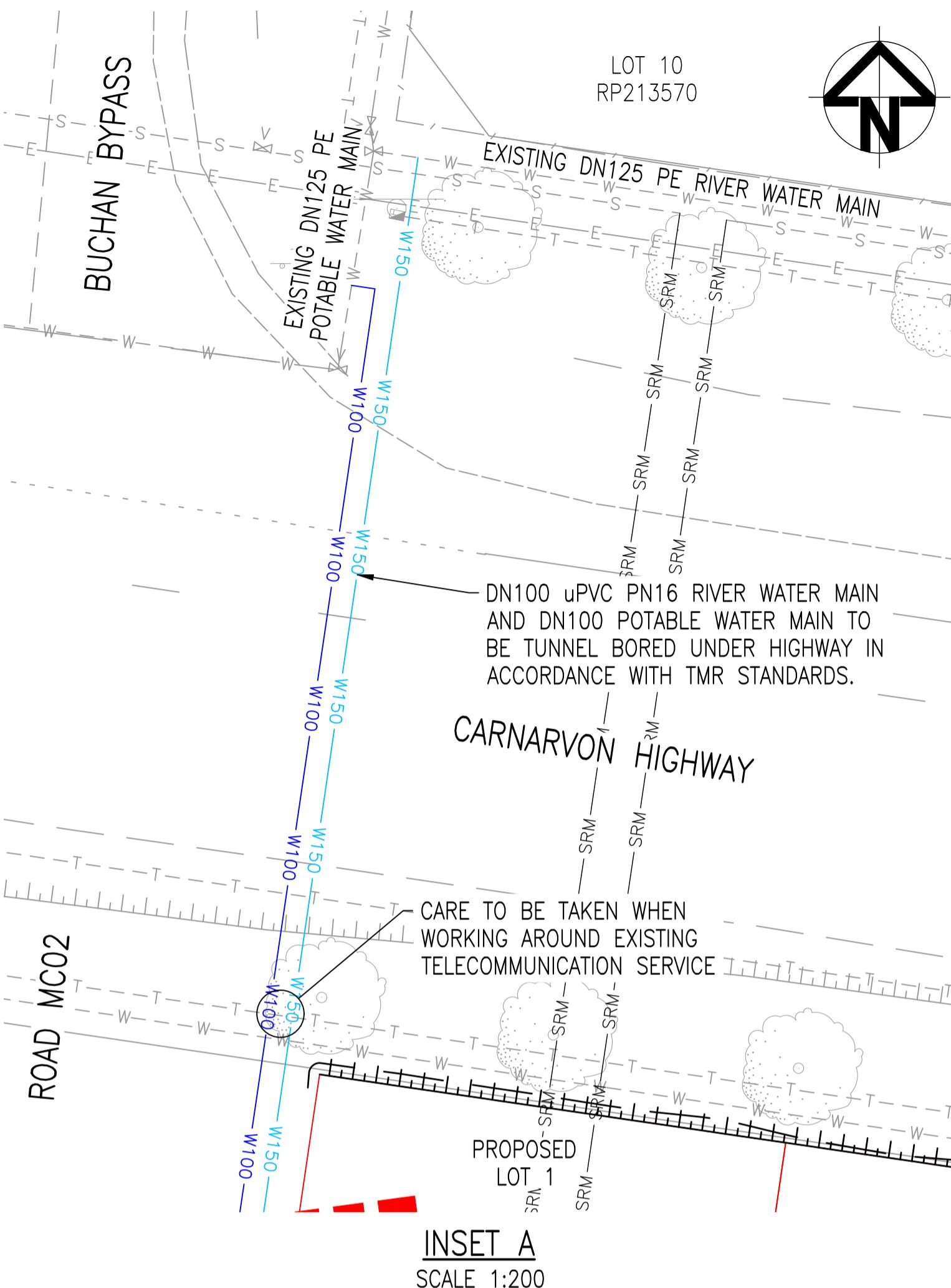


LINE

LINE 03

SEWERAGE LONGITUDINAL SECTION

SCALE 1:1000 HORIZONTAL, SCALE 1:100 VERTICAL



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and was issued on

24 September 2025

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KATE SWEPSO
Consultant Town Planner

NOTES

1. FOR SEWER LONGSECTIONS REFER TO
DRAWINGS 22.1003-039 & 40

| Rev | By | Description | Approved | Date |
|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |

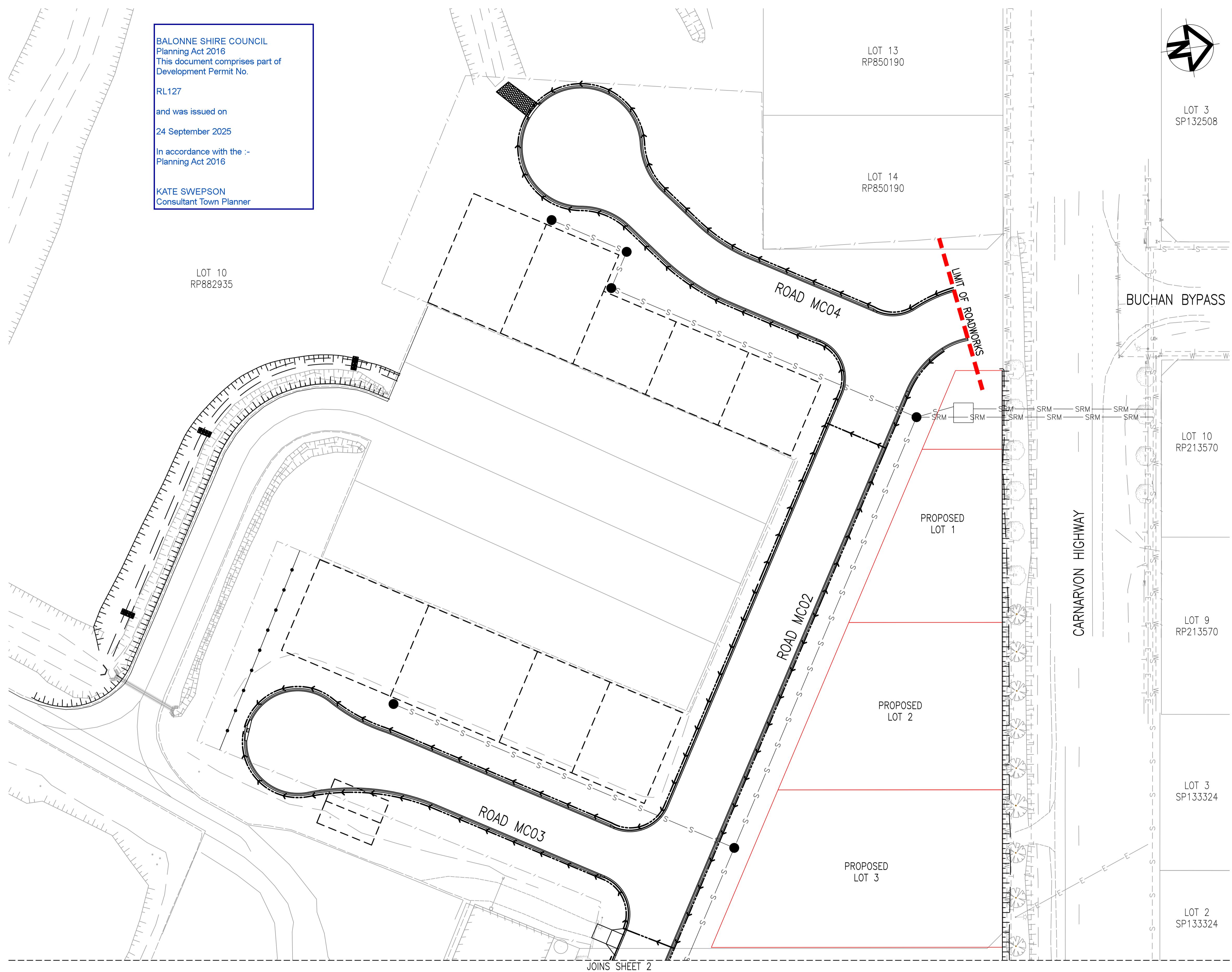


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31 GLASSER STREET
GOONDIWINDI QLD 4390
ABN: 82 626 886 771
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| Scales: | | Designed | BJS | | | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION WATER CONNECTION DETAILS | |
| 0m | 2.0 | 4.0 | 6.0 | 8.0 | 1:200 AT A1 | Drafter | GCC |
| | | | | | | Check | LOD |
| | | | | | | Approved | |
| A1 ORIGINAL SIZE | | | | Client BALONNE SHIRE COUNCIL | | Drawing No. 22.1003-03 | Sheet 39 of 42 |
| | | | | | | Revision. 6 | |

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JOINS SHEET 2
SOIL EROSION AND SEDIMENT CONTROL LAYOUT

SCALE 1:500

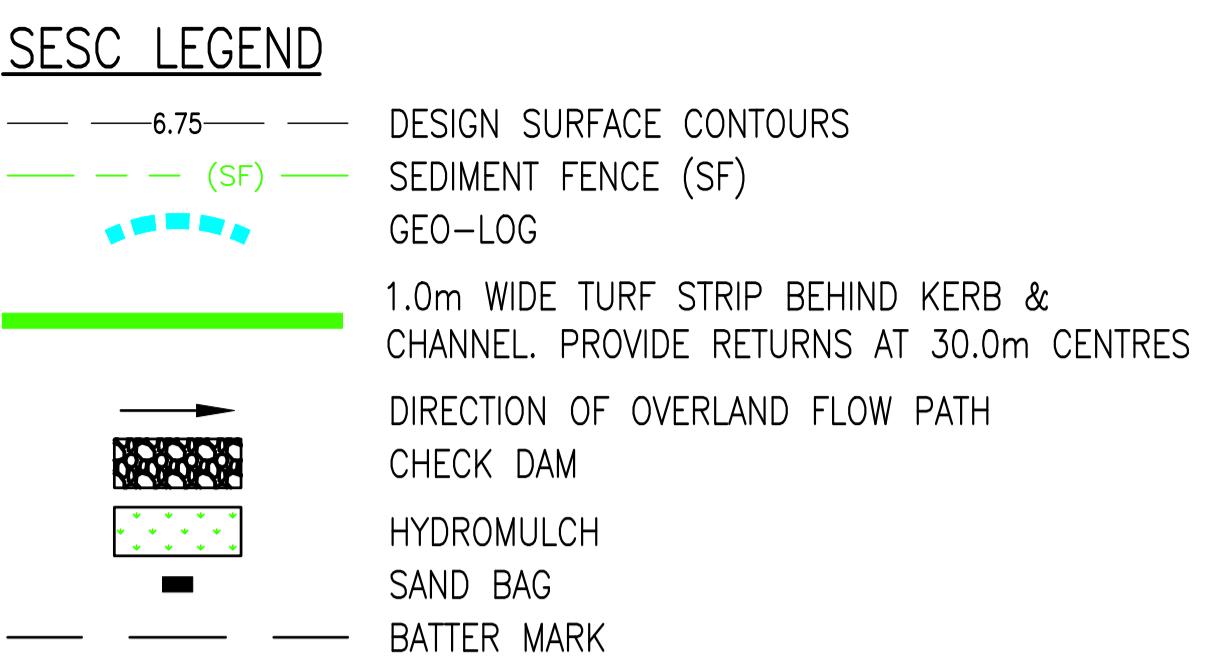
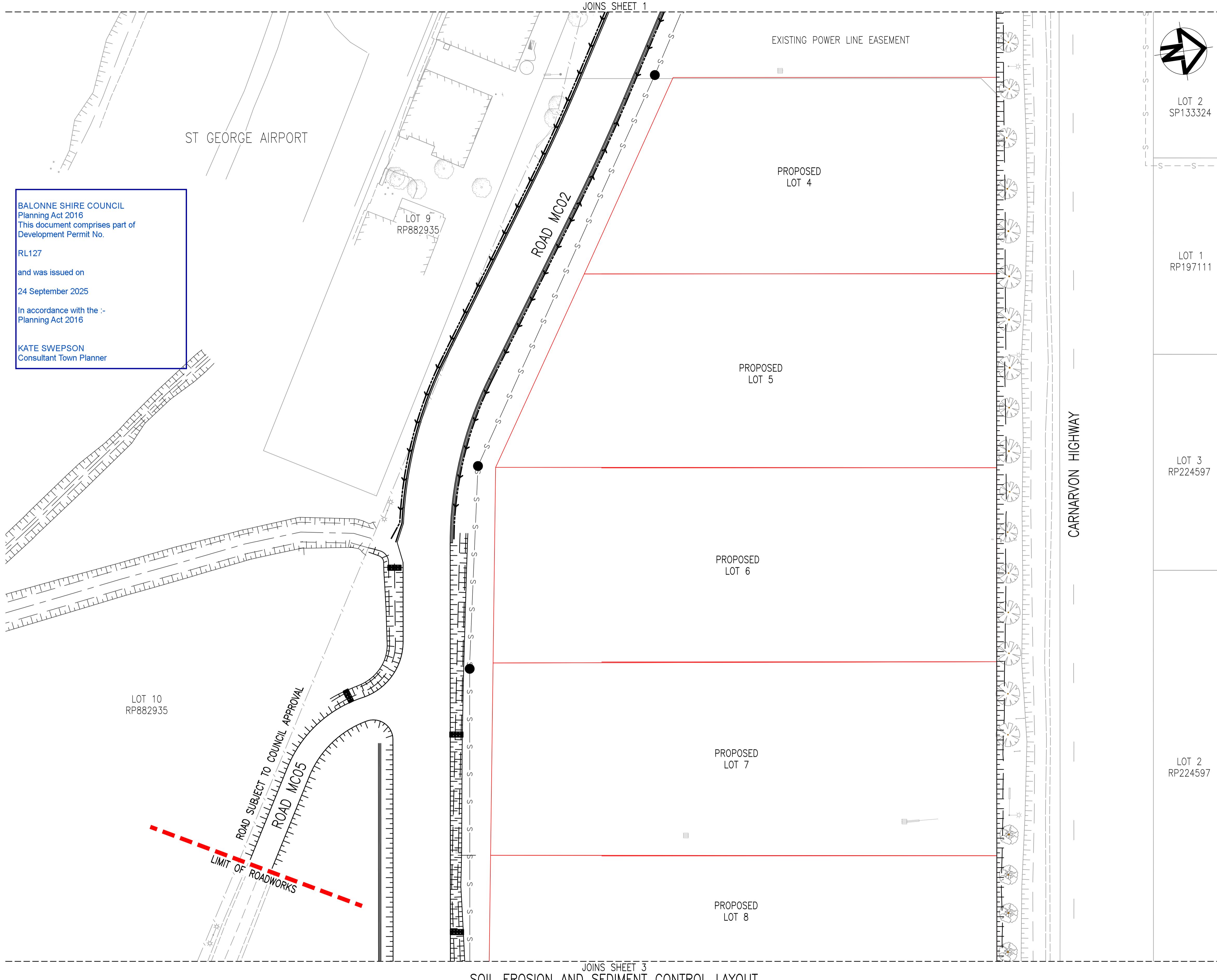
| | | | | |
|-----|-----|----------------------------|----------|----------|
| 6 | GCC | PUMP STATION DETAILS ADDED | LOD | 16/10/23 |
| 5 | CDH | ISSUE FOR 100% REVIEW | LOD | 26/09/23 |
| 4 | BJS | AMENDMENT 3 TENDER SET | LOD | 24/08/23 |
| 3 | BJS | AMENDMENT 2 TENDER SET | LOD | 29/07/23 |
| 2 | BJS | AMENDMENT 1 TENDER SET | LOD | 01/07/23 |
| 1 | BJS | ISSUE FOR TENDER | LOD | 01/06/23 |
| Rev | By | Description | Approved | Date |

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|---|---|-----|----|---|----|-------------|--|
| Scales: | 0m | 5.0 | 10 | 15 | 20 | 1:500 AT A1 | |
| Designed | BJS | | | | | | |
| Drafter | GCC | | | | | | |
| Check | LOD | | | | | | |
| Approved | | | | | | | |
| Client | ST GEORGE AIRPORT INDUSTRIAL SUBDIVISION EROSION & SEDIMENT CONTROL SHEET 1 OF 3 | | | Drawing No. 22.1003-040 | | | |
| | | | | Sheet 40 of 42 | | | |
| | | | | Client BALONNE SHIRE COUNCIL | | | |
| | | | | Revision. 6 | | | |



SESC NOTES

1. FOR SESC GENERAL NOTES, REFER TO DRG. NO 22.1003-032.

LOT 1
RP197111

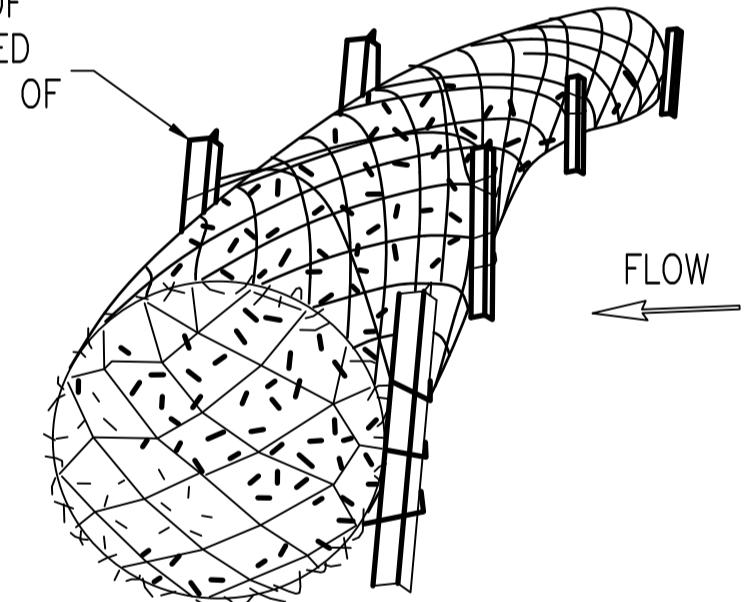
NOTE
ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURE MAY BE
REQUIRED DURING THE COURSE OF CONSTRUCTION. THE CONTRACTOR
SHALL IDENTIFY POTENTIAL PROBLEM AREAS AND TREAT THEM IN
ACCORDANCE WITH THE IECA (AUST.) AND BSC REQUIREMENTS.

LOT 3
RP224597

SANDBAGS ARE TO BE INSTALLED ACROSS THE
EXISTING ROAD PRIOR TO RAIN EVENT AND/OR
AT THE END OF EACH WORKING DAY

ALL DISTURBED AREAS OF SITE
INCLUDING ALLOTMENT FILL AREAS TO
BE TOPSOILED AND GRASS SEDED

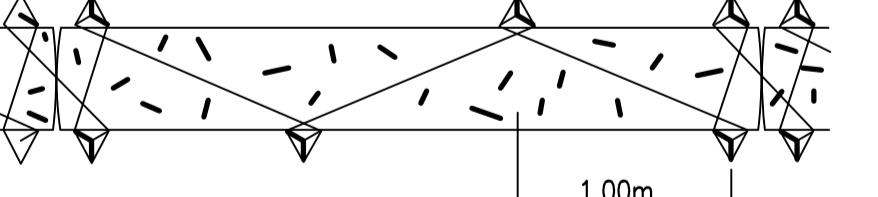
PROVIDE A MINIMUM OF
FOUR STAKES ATTACHED
TO DOWNSTREAM SIDE OF
GEO-LOG



LOT 2
RP224597

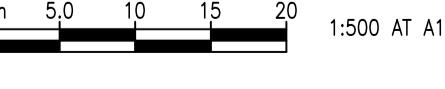
TYPICAL ANCHORAGE OF GEO-LOG

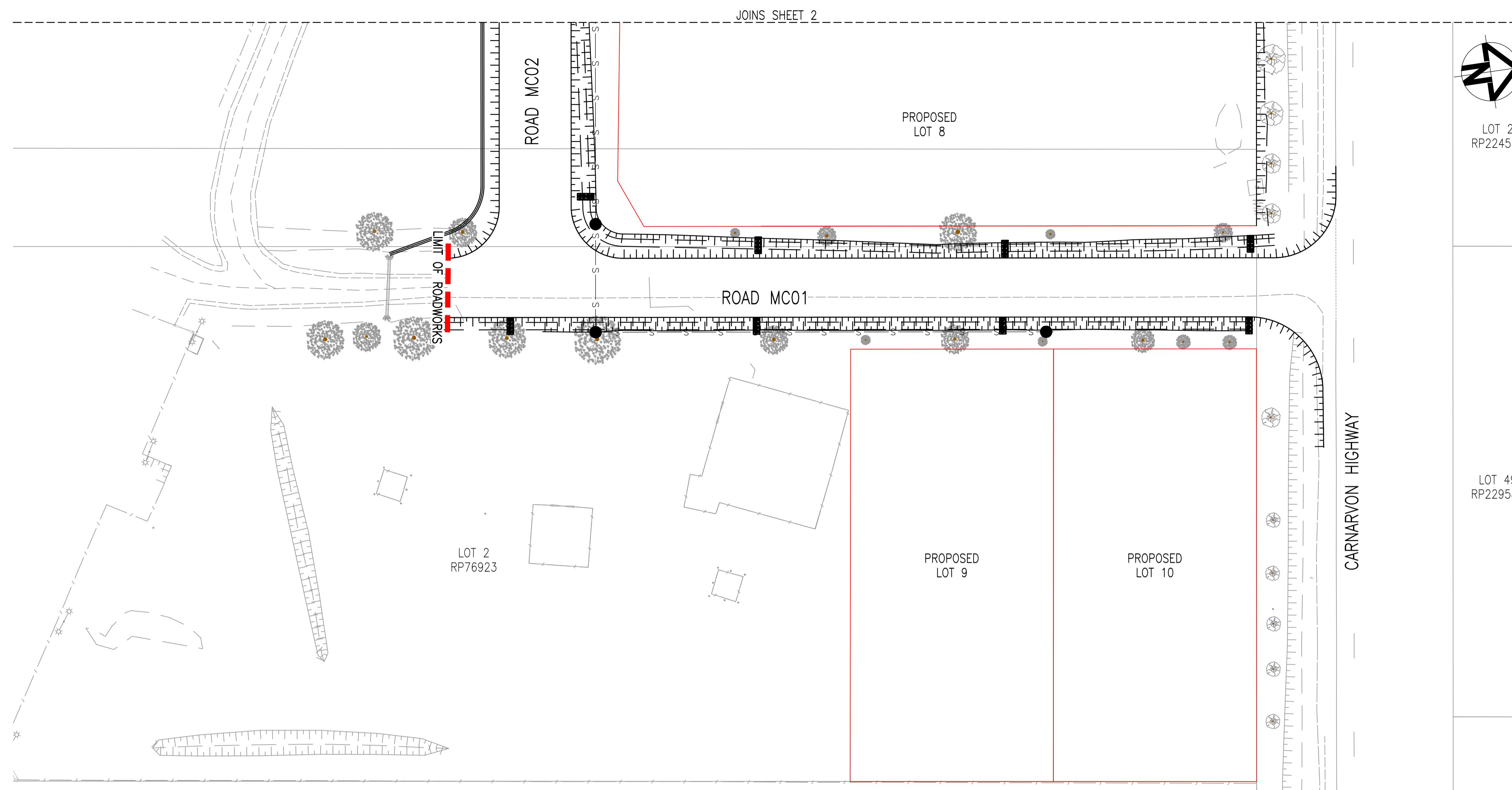
Geo Log to be installed in accordance with
IECA Sediment and Erosion Control Design Fact
Sheets - Instream Practices



STAKING METHOD

GEO-LOG DETAIL
Not To Scale

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| | | | | | | | | | Sheet 41 of 42 | |
| | | | | | | | | | Client BALONNE SHIRE COUNCIL Revision. | |



GENERAL LAYOUT

The diagram illustrates a temporary construction entry/exit sediment trap. It shows a cross-section of a slope leading down to a 'CONSTRUCTION SITE'. The trap is composed of several layers: a top layer of 'SUITABLE COARSE GRAVEL MATERIAL', followed by a 'CATTLE GRID SET 300mm ABOVE GROUND', and a bottom layer of 'GEOFABRIC BIDIM A34 OR APPROVED EQUIVALENT'. The total width of the trap is 10.0m MINIMUM. On the left, there is a 5.0m MINIMUM distance from the edge of the trap to the 'CONSTRUCTION SITE'. On the right, there is a 3.0m distance from the edge of the trap to the 'PROPERTY BOUNDARY'. A 'SITE ACCESS' arrow points to the right, indicating the direction of entry and exit. A note at the top right specifies that the 'SEDIMENT TRAP TO BE MINIMUM 3.0m WIDE'.

LOCATION TO BE DETERMINED ON SITE
ALTERNATE: 75mm ROCK RUMBLE PAD 20x3m
NTS

LOCATION TO BE DETERMINED ON SITE
ALTERNATE: 75mm ROCK RUMBLE PAD 20x3m
NTS



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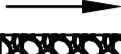
S: *REMOVED*

0m 5.0 10 15 20

0m 5.0 10 15 20 1:500 A

A1 ORIGINAL SIZE

SESC LEGEND

- 6.75 — DESIGN SURFACE CONTOURS
- (SF) — SEDIMENT FENCE (SF)
- GEO-LOG
- 1.0m WIDE TURF STRIP BEHIND KERB & CHANNEL. PROVIDE RETURNS AT 30.0m CENTRES
- DIRECTION OF OVERLAND FLOW PATH
-  CHECK DAM
-  HYDROMULCH
-  SAND BAG
- BATTER MARK

SESC NOTES

1. FOR SESC GENERAL NOTES, REFER TO DRG. NO 22.1003-032.

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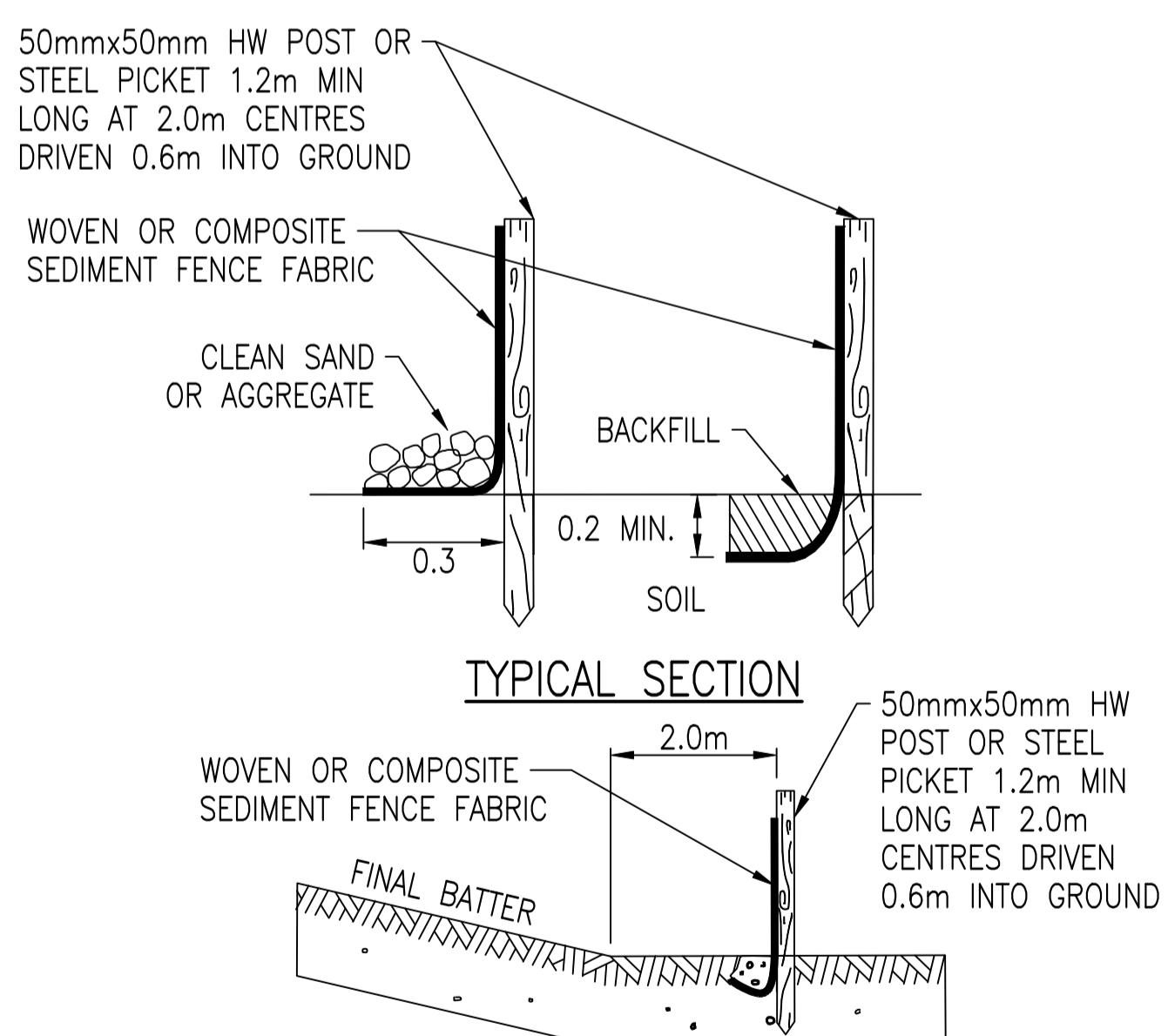
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Consultant Town Planner



LOCATION OF FENCE RELATIVE TO BASE OF SLOPE

| | | | | | | | | | | |
|-----|----|-------------------|----------|------|---|--|--|--|---|--|
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| 0 | 0 | rea | | | <p>PROTERRA GROUP 31 GLASSER STREET GOONDIWINDI QLD 4390 ABN: 82 626 886 771 TEL: (07) 4671 5701</p> | | <p>Approved</p> | | <p>Sheet 24 of 42</p> | |
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| Rev | By | Description | Approved | Date | | | A1 ORIGINAL SIZE | | | |